

Appendix H

Consultation Documentation

- Consultation Round 1 Summary Report (Winter 2013)
- Consultation Round 2 Summary Report (Winter 2015)
- Consultation Round 3 Summary Report (Spring 2015)
- Notices
- Aboriginal Consultation
- Comments Received Following Consultation Report #3
- Comments Received Following Notice of Commencement

Consultation Round 1 Summary Report (Winter 2013)

United Counties of Prescott and Russell in partnership with the
City of Ottawa

Consultation Round #1 Summary Report Ottawa Road 174 / Prescott-Russell County Road 17 Class Environmental Assessment Study

Prepared by:

AECOM
302 – 1150 Morrison Drive
Ottawa, ON, Canada K2H 8S9
www.aecom.com

613 820 8282 tel
613 820 8338 fax

In Association with:

Delcan Corporation
LRL Associates Ltd.
Houle Chevrier Engineering Ltd.
Golder Associates Ltd.

Project Number:

60270243

Date:

March, 2013



AECOM
302 – 1150 Morrison Drive
Ottawa, ON, Canada K2H 8S9
www.aecom.com

613 820 8282 tel
613 820 8338 fax

Statement of Qualifications and Limitations

The attached Report (the “Report”) has been prepared by AECOM Canada Ltd. (“Consultant”) for the benefit of the client (“Client”) in accordance with the agreement between Consultant and Client, including the scope of work detailed therein (the “Agreement”).

The information, data, recommendations and conclusions contained in the Report (collectively, the “Information”):

- is subject to the scope, schedule, and other constraints and limitations in the Agreement and the qualifications contained in the Report (the “Limitations”);
- represents Consultant’s professional judgement in light of the Limitations and industry standards for the preparation of similar reports;
- may be based on information provided to Consultant which has not been independently verified;
- has not been updated since the date of issuance of the Report and its accuracy is limited to the time period and circumstances in which it was collected, processed, made or issued;
- must be read as a whole and sections thereof should not be read out of such context;
- was prepared for the specific purposes described in the Report and the Agreement; and
- in the case of subsurface, environmental or geotechnical conditions, may be based on limited testing and on the assumption that such conditions are uniform and not variable either geographically or over time.

Consultant shall be entitled to rely upon the accuracy and completeness of information that was provided to it and has no obligation to update such information. Consultant accepts no responsibility for any events or circumstances that may have occurred since the date on which the Report was prepared and, in the case of subsurface, environmental or geotechnical conditions, is not responsible for any variability in such conditions, geographically or over time.

Consultant agrees that the Report represents its professional judgement as described above and that the Information has been prepared for the specific purpose and use described in the Report and the Agreement, but Consultant makes no other representations, or any guarantees or warranties whatsoever, whether express or implied, with respect to the Report, the Information or any part thereof.

Without in any way limiting the generality of the foregoing, any estimates or opinions regarding probable construction costs or construction schedule provided by Consultant represent Consultant’s professional judgement in light of its experience and the knowledge and information available to it at the time of preparation. Since Consultant has no control over market or economic conditions, prices for construction labour, equipment or materials or bidding procedures, Consultant, its directors, officers and employees are not able to, nor do they, make any representations, warranties or guarantees whatsoever, whether express or implied, with respect to such estimates or opinions, or their variance from actual construction costs or schedules, and accept no responsibility for any loss or damage arising therefrom or in any way related thereto. Persons relying on such estimates or opinions do so at their own risk.

Except (1) as agreed to in writing by Consultant and Client; (2) as required by-law; or (3) to the extent used by governmental reviewing agencies for the purpose of obtaining permits or approvals, the Report and the Information may be used and relied upon only by Client.

Consultant accepts no responsibility, and denies any liability whatsoever, to parties other than Client who may obtain access to the Report or the Information for any injury, loss or damage suffered by such parties arising from their use of, reliance upon, or decisions or actions based on the Report or any of the Information (“improper use of the Report”), except to the extent those parties have obtained the prior written consent of Consultant to use and rely upon the Report and the Information. Any injury, loss or damages arising from improper use of the Report shall be borne by the party making such use.

This Statement of Qualifications and Limitations is attached to and forms part of the Report and any use of the Report is subject to the terms hereof.

Marc R. Clermont, P. Eng.
Director of Public works
United Counties of Prescott and Russell
59 Court Street
L’Original, Ontario
K0B 1K0

Dear Mr. Clermont:

Project No: 60270243
**Regarding: Consultation Round #1 Summary Report
Ottawa Road 174 / Prescott-Russell County Road 17
Environmental Assessment Study**

The Consultation Round #1 Summary Report is attached for your review.

Sincerely,
AECOM Canada Ltd.

Original Signed by

Valerie McGirr, P. Eng.
Manager, Ottawa office
Valerie.mcgirr@aecom.com

VM:dc
Encl.
cc: file

Distribution List

# of Hard Copies	PDF Required	Association / Company Name
0	1	Marc Clermont, United Counties of Prescott and Russell
0	1	Angela Taylor, City of Ottawa

Revision Log

Revision #	Revised By	Date	Issue / Revision Description
0	DC/KR	February 2013	Draft for review

AECOM Signatures

Report Prepared By:

Original Signed by

Danielle Chartrand
Environmental Planner, AECOM

Report Prepared By:

Original Signed by

Kelly Roberts, B.Sc. (Hons)
Manager of Environmental Planning, Delcan

Report Reviewed By:

Original Signed by

Valerie McGirr, P. Eng
Manager, Ottawa office, AECOM

Table of Contents

Statement of Qualifications and Limitations
Letter of Transmittal
Distribution List

	page
1.	INTRODUCTION1
2.	PROJECT WEBSITE.....2
3.	CONSULTATION GROUPS2
3.1	Agency Consultation Group (ACG) 2
3.2	Business Consultation Group (BCG)..... 5
3.3	Public Consultation Group (PCG)..... 8
3.4	First Nations Consultation..... 11
4.	PUBLIC OPEN HOUSES11
4.1	Public Open House in Cumberland 13
4.2	Public Open House in Orléans 16
4.3	Public Open House in Rockland 17
4.4	Summary of Comments Submitted..... 18
4.4.1	Summary of Comments Submitted at POH in Cumberland 19
4.4.2	Summary of Comments Submitted at POH in Orléans 20
4.4.3	Summary of Comments Submitted at POH in Rockland..... 20
4.4.4	Comments Submitted after the POH Meetings 21
5.	CONCLUSION.....21

List of Figures

Figure 1.	Study Area..... 1
-----------	-------------------

List of Tables

Table 1.	Contacts for Agency Consultation Group..... 3
Table 2.	Summary of Discussion at Agency Consultation Group Meeting 4
Table 3.	Contacts for Business Consultation Group 6
Table 4.	Summary of Discussion at Business Consultation Group Meeting..... 6
Table 5.	Contacts for Public Consultation Group 9
Table 6.	Summary of Discussion at Public Consultation Group Meeting 9
Table 7.	Summary of Discussion at Public Open House in Cumberland..... 13
Table 8.	Summary of Discussion at Public Open House in Orléans..... 16
Table 9.	Summary of Discussion at Public Open House in Rockland 18
Table 10.	Summary of Comments Submitted at Public Open House in Cumberland 19
Table 11.	Summary of Comments Submitted at Public Open House in Orléans 20
Table 12.	Summary of Comments Submitted at Public Open House in Rockland 20

Table 13. Summary of Comments Submitted after the Public Open House Meetings 21

Annexes

- Annex D-1. Invitation letter to Agency Consultation Group Meeting #1
Presentation for Agency Consultation Group Meeting #1
- Annex D-2. Invitation letter to Business Consultation Group Meeting #1
Presentation for Business Consultation Group Meeting #1
- Annex D-3. Invitation letter to Public Consultation Group Meeting #1
Presentation for Public Consultation Group Meeting #1
- Annex D-4. Invitation letter to First Nations for Agency Consultation Group Meeting #1
- Annex D-5. Notice of Round #1 of Public Open Houses (English and French)
Presentation for Public Open House #1 (English and French)
Display Material for Public Open House #1 (English and French)
- Annex D-6. Newsletter (English and French)
- Annex D-7. Comment-Questionnaire
Comments Submitted at and after the Public Open House Meetings (excluding personal information)

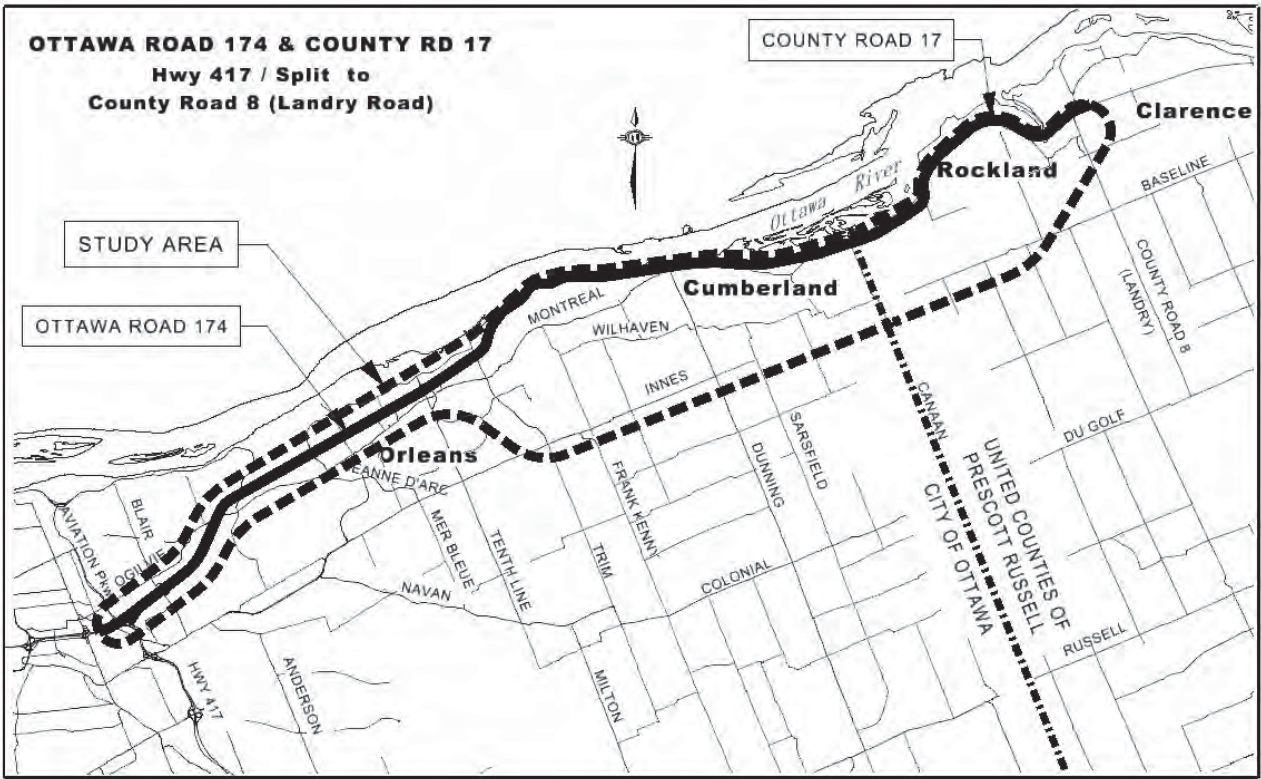
1. INTRODUCTION

The United Counties of Prescott and Russell (UCPR) in partnership with the City of Ottawa are undertaking a Class Environmental Assessment (EA) study for the Ottawa Road (OR) 174 and Prescott-Russell County Road (CR) 17 corridor from Highway 417 to CR 8 (Landry Road). **Figure 1** illustrates the project study area. This study will consider improvements to the existing OR 174 and CR 17 corridor, improvements to other existing roads and the construction of new roads in the study area illustrated below. This study is being carried out in accordance with the requirements for a Schedule ‘C’ project under the Municipal Class Environmental Assessment (2007, 2011).

The EA process will involve developing, assessing, and evaluating alternatives, which will result in a recommended plan to be presented to the Committees/Council of UCPR and City of Ottawa.

Three rounds of Public Open Houses will be held during the course of the study to review and discuss the project with the Study Team and to solicit feedback. The purpose of this report is to present an overview of the first round of consultation and to document responses.

Figure 1. Study Area



2. PROJECT WEBSITE

Project information was posted on the United Counties of Prescott and Russell (UCPR) and the City of Ottawa web sites at:

www.prescott-russell.on.ca/en/public-works/major-projets
or
www.ottawa.ca/hwy174and17study

Public Open House meeting notices, display material presented at the meetings and project updates will be available online throughout the study. Contact information is also provided to allow the public to comment throughout the study process.

3. CONSULTATION GROUPS

Three separate Consultation Groups were formed to represent various agency, business and public stakeholders, which will provide direct input and feedback during the course of the study.

3.1 Agency Consultation Group (ACG)

An Agency Consultation Group (ACG) was established for this project to address technical issues; comment on the special studies required to fully assess the various alternatives; and help ensure that the UCPR and City of Ottawa are following procedures, legislation and appropriate policies. Members include federal, provincial, UCPR, City of Ottawa and other municipal representatives.

Prior to the first round of Public Open Houses, a separate meeting was held with the ACG to obtain input. This meeting was held on:

Monday, January 28, 2013
9:00 a.m. to 12:00 p.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West, Ottawa

The members of the study team in attendance included:

United Counties of Prescott and Russell	
Marc R. Clermont	Director of Public Works
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Vanessa Skelton	Traffic Engineer, AECOM
Kelly Roberts	Environmental Planner, Delcan

Copies of the invitation letter and presentation are provided in **Appendix A** of this report.

Notices were emailed or mailed to agencies and utilities listed in **Table 1**.

Table 1. Contacts for Agency Consultation Group

Contacts - ACG	
United Counties of Prescott and Russell:	City of Clarence-Rockland:
• Accessibility Advisory Committee	• Community Services (Recreation)
• Agricultural Advisory Committee	• Fire Department
• Economic Development and Tourism	• Physical Services
• Emergency Services	• Planning
• Paramedic Services	Ottawa Catholic School Board
• Planning & Forestry	Catholic District School Board of Eastern Ontario
• Public Works	Upper Canada District School Board
City of Ottawa:	Ottawa Student Transportation Authority
• Acquisition, Real Estate Partnership & Development Office	Student Transportation for Eastern Ontario
• Emergency & Protective Services	Conseil des écoles catholiques du Centre-Est
• Planning & Growth Management	Conseil des écoles publiques de l'Est de l'Ontario
• Construction Services	Conseil scolaire de district catholique de l'Est ontarien
• Design, Review and Implementation	Consortium de transport scolaire de l'Est
• Development Review - East Unit	Ministry of Infrastructure
• Development Review - Outer Core Unit	Ministry of Natural Resources
• Development Review - Rural East Unit	Ministry of Tourism, Culture and Sport
• Environmental Services	Ministry of Transportation
• Infrastructure Assessment - Transportation Assets	OPP
• Infrastructure Assessment - Water Resources Assets	Rideau Valley Conservation Authority
• Land Use & Natural Systems	South Nation Conservation Authority
• Municipal Design & Construction East, Infrastructure Services	Transport Action Canada
• OC Transpo	National Capital Commission
• Planning and Infrastructure	National Research Council
• Ottawa Police Services	Public Works and Government Services Canada
• Rail Planning, Rail Implementation Office	RCMP
• Right of Way info & Approvals	Transport Canada
• Road Safety, Traffic Investigation & Surveys	Bell Aliant
• Rural Affairs Office	Enbridge Gas Distribution Inc.
• Special Operations	Hydro One Networks Inc.
• Traffic Operations	Hydro Ottawa
• Traffic, Safety & Mobility	Rogers Communications Inc.
• Transit Priority, Transit Services	Videotron
• Transportation Strategic Planning	
• Water Distribution	

The ACG presentation dealt with the following topics:

- Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process

- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps

A total of 29 people attended the Agency Consultation Group meeting including 8 people by teleconference. A summary of the discussion is presented in **Table 2**.

Table 2. Summary of Discussion at Agency Consultation Group Meeting

Summary of Discussion - ACG
<ul style="list-style-type: none">• How do you take into account location of Interprovincial Bridge (IPB)? <i>The selection of a preferred route for the IPB will occur after our needs assessment work. Therefore the interprovincial bridge EA team will be responsible for assessing the transportation impacts associated with their routes. The impacts will depend on the origins and destination of traffic using the interprovincial bridge.</i>
<ul style="list-style-type: none">• Are you talking to the IPB team? <i>We have made contact with the team. Once they decide on a route, we will have a better idea on the scope of discussions needed.</i>
<ul style="list-style-type: none">• A decrease in congestion along the OR 174-CR17 could reduce use of transit from Clarence Rockland or in Ottawa. <i>Widening will not eliminate all congestion. To support CR Transpo, the municipality is considering park and ride and car pool lots in Clarence Rockland. By 2031 the East Transitway should be built beside OR174. We will protect the corridor for transitway.</i>
<ul style="list-style-type: none">• Will HOV lanes be considered? <i>HOV lanes will be considered. HOV lanes for OR 174 were examined as part of the MTO Preliminary Design Study in the early 1990's (easterly limit was Champlain Street). In that study HOV lanes were shown in the median.</i>
<ul style="list-style-type: none">• What is the timing of construction? <i>Design for the widening from Blair to Jeanne d'Arc is in the 10 year plan for 2016-2017 and construction would follow in 2018. Will consider the affects of having the construction of the widened OR174 occur at the same time as OLRT construction work.</i>
<ul style="list-style-type: none">• There are many structures on 174 and some may require replacement at the time work is done. <i>We can co-ordinate with the maintenance work when the details of the project become available.</i>
<ul style="list-style-type: none">• What is the “life” of the EA? <i>After 10 years, it is necessary to re-visit the existing conditions and assumptions to determine if they remain valid if construction has not commenced. An addendum must be prepared to document changes.</i>
<ul style="list-style-type: none">• Life cycle costs and affordability should be looked at to compare alternatives. <i>This will be considered this request.</i>
<ul style="list-style-type: none">• Will you consider the relocation of employment centres? <i>As part of the EA study, moving locations of employment areas is not considered. This is an OP review issue. There is a move to increase transit-oriented development around the OLRT.</i>
<ul style="list-style-type: none">• Will lanes be freed up for car use with transitway? <i>If the existing bus-on-shoulder lanes are no longer needed once the Transitway is constructed, they can be used as part of the roadway.</i>
<ul style="list-style-type: none">• If we know the schedule ahead of time we can better plan infrastructure improvements. <i>ACG will be used to keep city staff informed of the study schedule</i>
<ul style="list-style-type: none">• Consider access to water and the barrier effect of the existing roadway along the Ottawa River. Study provides an opportunity to consider a parallel recreational corridor. Encourage cycling. Consider ultimate land use and intensification, including suburban development in Clarence-Rockland. <i>This is recognized in our text and will be included in the impact assessment.</i>
<ul style="list-style-type: none">• Need to consider the design speed through village areas and urban design features to help drivers recognize that they are entering a village (setbacks, roadside environment, urbanized feel). Slow down traffic through communities. Connectivity across the 174 is an issue in Cumberland Village.

Summary of Discussion - ACG
<ul style="list-style-type: none">• Will car pool lots be considered? <i>Clarence-Rockland is looking to connect with retailers in Rockland to arrange for shared parking for park and ride purposes where available. The City is also looking at this in the TMP update.</i>
<ul style="list-style-type: none">• TMP update is looking at peak periods. Perhaps LOS 'D' as objective is not realistic. <i>We will not achieve LOS 'D' even though it was identified as our objective to be consistent with the 2008 TMP. We will still have congestion.</i>
<ul style="list-style-type: none">• What do Cumberland Village residents want?• Polarized group with different concerns. Residents on 174 don't want widening. <i>This study may address some of their issues, connectivity and access to waterfront (additional factors will still be an issue through Cumberland Village).</i>
<ul style="list-style-type: none">• Updates to Cycling Plan and Pedestrian Plan are underway. The City will use existing studies to feed into the updates and would like to include riverfront pathways in both Plans. Will have multi-use pathway on either side of Trim Road but no connection to Petrie Island. Would like to plan for this eventually. There are habitat issues at the creek crossings along OR 174.
<ul style="list-style-type: none">• Clarence-Rockland is looking to develop Rockland waterfront so widening of the existing roadway is not the best solution from a planning perspective. A new route to the south of Rockland would be a win-win.

3.2 Business Consultation Group (BCG)

A Business Consultation Group (BCG) was established for this project to provide input to the study and to advise and comment on local issues and concerns. The BCG is not an approving body. Members may include businesses, business associations, institutions and developers with interest in the area

Prior to the first round of Public Open Houses a separate meeting was held with the BCG to obtain input. This meeting was held on:

Wednesday, January 30, 2013
2:00 p.m. to 4:00 p.m.
Cumberland Lions Hall, Maple Hall
2552 Old Montreal Road, Cumberland

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Vanessa Skelton	Traffic Engineer, AECOM
Kelly Roberts	Environmental Planner, Delcan

Copies of the invitation letter and presentation are provided in **Appendix B** of this report.

Letters of notification were mailed or emailed to individuals listed in **Table 3**.

Table 3. Contacts for Business Consultation Group

Contacts - BCG	
AVW - TELAV Audio Visual Solutions	Bernard Sanscartier Construction ltd.
Brigil	C.H. Clément Construction
Mr. Gas Limited	Clarence-Rockland Construction Association
Traversiers Bourbonnais (Ferry)	Devcore Group
Swedco	DRB Development
IBI Group	Regional Development
Novatech Engineering Consultants	Taggart
Orléans Chamber of Commerce	Tamarack Homes, Cardinal Creek
Ottawa Chamber of Commerce	Spacebuilders Ottawa Ltd.
Clarence-Rockland Chamber of Commerce	

The BCG presentation dealt with the following topics:

- Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps

A total of 5 people attended the Business Consultation Group meeting held on January 30, 2013. A summary of the discussion is summarized in **Table 4**.

Table 4. Summary of Discussion at Business Consultation Group Meeting

Summary of Discussion - BCG
<ul style="list-style-type: none">• When were the traffic counts taken? The daily traffic volume is estimated at 22,000 vehicles west of Rockland. <i>The bar graph shows one way peak hour volumes. The bars for 2012 were taken in 2011 and 2012. The road is operating at capacity with about 1100 vehicles per hour in one direction.</i>
<ul style="list-style-type: none">• Is the Interprovincial Bridge included in the traffic analysis? <i>The selection of a preferred route for the IPB will occur after our needs assessment work. Therefore the interprovincial bridge EA team will be responsible for assessing the transportation impacts associated with their routes. The impacts will depend on the origins and destination of traffic using the interprovincial bridge.</i>
<ul style="list-style-type: none">• Is the OLRT taken into account? <i>Yes. The traffic analysis assumes the achievement of the transit ridership associated with completion of the rapid transit facility.</i>
<ul style="list-style-type: none">• Was Clarence-Rockland transit included? Do the buses use 174? <i>We had existing data but no projections. We assumed that the existing transit ridership would continue. The CRT buses use 174 and the Transitway.</i>
<ul style="list-style-type: none">• Will conversion from BRT to LRT change the results? No.
<ul style="list-style-type: none">• Is the Highway 50 work included? Would it affect the routes that trucks take? <i>Most increase in the traffic volumes in the TRANS traffic model are related to commuter traffic in the developing areas. Truck usage is usually spread out during the day and has less impact on the peak. During the peak the percentage of trucks is low (perhaps 2-3%).</i>

Summary of Discussion - BCG
<ul style="list-style-type: none">• Why was LOS ‘D’ selected? <i>This is to be consistent with the City of Ottawa 2008 TMP.</i>
<ul style="list-style-type: none">• What about adding a bus-only lane or HOV? <i>We will be examined vehicle occupancy data as part of an assessment for HOV lanes.</i>
<ul style="list-style-type: none">• Recent concern in Clarence-Rockland is reduced ridership on the transit service. Some people are driving their cars to the Trim Road Park and Ride and getting on the bus there. Decreasing the congestion could further decrease transit ridership. <i>Widening will not eliminate all congestion. To support CR Transpo, the municipality is considering park and ride and car pool lots in Clarence Rockland. By 2031 the East Transitway should be built beside OR174. We will protect the corridor for Transitway.</i>
<ul style="list-style-type: none">• How many fatalities have there been in the years of data? <i>There were 3 from the split to Trim, 5 from Trim to Canaan Road and 2 from Canaan Road to Landry Road.</i>
<ul style="list-style-type: none">• Collisions seem to have reduced due to congestion (slow speeds during the peak). <i>Sometimes this can lead to concerns during in “shoulder” peak times as drivers take chances.</i>
<ul style="list-style-type: none">• Will the bus lane be maintained on 174 during OLRT construction? <i>Yes – there will be good service maintained for OC Transpo.</i>
<ul style="list-style-type: none">• Are you looking at eliminating signals, such as at Trim Road? <i>Yes. At Trim Road an interchange is being considered. At other locations, such as in Rockland, signals are expected to remain at the intersections; however intersection improvements will be examined with respect to system operation and safety.</i>
<ul style="list-style-type: none">• Some drivers are using Canaan Road to Baseline Road as an alternative route to CR 17. The City of Clarence-Rockland is re-building Baseline Road.
<ul style="list-style-type: none">• Will natural features be considered in the assessment of the impacts to widening? <i>Yes.</i>
<ul style="list-style-type: none">• Will the study look at a one-way pair and new routes? <i>New routes and other ways to solve the problem are being examined.</i>
<ul style="list-style-type: none">• Is this area affected by First Nations land claims? <i>Yes. A large portion of eastern Ontario is part of the Algonquin's land claim. We will keep the First Nations communities informed and involved throughout the study.</i>
<ul style="list-style-type: none">• What is the daily capacity of the road? <i>As an example, the traffic volume on Highway 7 to Carleton Place was about 16,500 before twinning of the highway.</i>
<ul style="list-style-type: none">• Will widening threaten the viability of businesses? <i>Widening could increase the traffic passing in front of a business. Access could also be changed or restricted. It is important to understand localized impacts in order to answer this question.</i>
<ul style="list-style-type: none">• The ferry service should be prominent on the transportation existing conditions maps. It is an efficient service. <i>Will be added.</i>
<ul style="list-style-type: none">• Do the future projections reflect the expansion of Wendover? <i>Yes. Data concerning Wendover development was provided and is included in the traffic projections.</i>
<ul style="list-style-type: none">• Flexible hours have helped reduce peak hour volumes after being introduced by the federal government. <i>Yes – this is part of the TDM measures currently in place.</i>
<ul style="list-style-type: none">• When is the widening of Trim Road to occur? Will it serve as a link from a new route or Innes to the 174? <i>Trim Road construction is underway now. An appropriate connection has not been determined. Trim Road will be considered. The affect of the Cardinal Creek Village development will also be considered.</i>
<ul style="list-style-type: none">• Is there road capacity pressure from east of Rockland? <i>No. There is residual capacity east of Edwards Street even considering the planned development. Much of the development in Clarence-Rockland is to the south of the existing highway, though there is also some development to the northeast. Congestion is expected on the west side of Rockland.</i>
<ul style="list-style-type: none">• Are there plans for Park and Ride lots in Rockland? <i>Clarence-Rockland is discussing shared parking use with businesses. The City of Ottawa is looking at Park and Ride lots in their OP update at this time.</i>

Summary of Discussion - BCG
<ul style="list-style-type: none">• Notice of the POHs has been sent to the Clarence-Rockland Chamber of Commerce members.
<ul style="list-style-type: none">• Would like to see the elimination of intersections where possible for safety reasons (such as Trim Road, Cameron Street and Canaan Road). There will be a new connection to CR 17 at Caron Street. <p><i>The safety assessment identified specific intersections where there is potential for improvements. Note that there will also be a new intersection on 174 as part of the new Cardinal Creek Village development.</i></p>
<ul style="list-style-type: none">• Where will a new route connect to 174? <p><i>This is to be determined in the next phase of the project.</i></p>
<ul style="list-style-type: none">• What is the time frame for the Transitway extension? <p><i>At present the East Transitway is expected to be extended to Trim Road by 2031. The TMP is being updated now and it may provide further information on this when complete. There are 3 corridors examined for the East Transitway from Place D-Orléans to Trim. Corridors that don't require the hydro line to be buried are preferred. The 174-17 EA Study will consider the work done for the feasibility study for the East Transitway. A corridor will be protected.</i></p>
<ul style="list-style-type: none">• Will the EA indicate property requirements? <p><i>Yes. Property impacts will be shown. We will consider the terrain when deciding on ROW.</i></p>
<ul style="list-style-type: none">• Is the old CPR rail line in the Study Area? <p><i>No, it is on the northeast side of Mer Bleue heading southeasterly towards Montreal.</i></p>

3.3 Public Consultation Group (PCG)

A Public Consultation Group (PCG) was established for this project to provide input to the study and to advise and comment on local issues and concerns. The PCG is not an approving body. Members may include community associations and resident groups, Advisory Committees and advocacy groups.

Prior to the first round of Public Open Houses a separate meeting was held with the PCG to obtain input. This meeting was held on:

Wednesday, January 30, 2013
6:30 p.m. to 8:30 p.m.
Cumberland Lions Hall, Maple Hall
2552 Old Montreal Road, Cumberland

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Vanessa Skelton	Traffic Engineer, AECOM
Kelly Roberts	Environmental Planner, Delcan

Copies of the invitation letter and presentation are provided in **Appendix C** of this report.

Letters of notification were mailed or emailed to community groups and interested parties listed in **Table 5**.

Table 5. Contacts for Public Consultation Group

Contacts - PCG	
Beacon Hill Community Association	Carson Grove and Rockcliffe Mews Residents Association
Blackburn Community Association	City of Ottawa, Cumberland Heritage Village Museum Board
Bradley Estate Community Association	Citizens for Safe Cycling
Cardinal Creek Community Association	Cycle Vision
Chapel Hill South Community Association	Ecology Ottawa
Cumberland Village Community Association	Federation of Citizens Association of Ottawa-Carleton
Fallingbrook Community Association	Greenspace Alliance of Canada's Capital
Navan Community Association	Heritage Ottawa
Sandpiper Cove Community Association	Ottawa Riverkeeper
Rothwell Heights Property Owners Association	Save the Parkway
Village Association (Chatelaine Village, Queenswood Village, Riverwalk)	

The PCG presentation dealt with the following topics:

- Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps

A total of 8 people attended the Public Consultation Group meeting held on January 30, 2013. A summary of the discussion is summarized in **Table 6**.

Table 6. Summary of Discussion at Public Consultation Group Meeting

Summary of Discussion - PCG
<ul style="list-style-type: none">• Lots of traffic is from east of Canaan Road.
<ul style="list-style-type: none">• For the 2031 projects, do you consider the travel speed on each route and diversion of traffic to the faster route? <p><i>Yes. The computer model that is used (TRANS) considers the overall arterial road network in the National Capital Region and so vehicles are assigned to the network with consideration for the system of roads. The traffic volume projections for 2031 (green bars on the graph) take into account the completion of roadworks and transit projects noted in the 2008 Transportation Master Plan of the City of Ottawa for 2031. These include the extension of the Blackburn Hamlet Bypass and the Cumberland Transitway.</i></p>
<ul style="list-style-type: none">• Does the interprovincial bridge factor into the assessment? <p><i>This is not included as the preferred route has not been selected. Volumes east of the Greenbelt will not be affected.</i></p>
<ul style="list-style-type: none">• How do the studies for Trim Road, 174-17 and the interprovincial bridge fit together? <p><i>These studies are not being done in isolation. We have included the Trim Road results in our work. The NCC who are responsible for the interprovincial bridge study sit on our Agency Consultation Group so that the results of both studies can be shared at the appropriate time.</i></p>
<ul style="list-style-type: none">• Who is on the ACG? <p><i>Affected municipalities, federal departments and provincial agencies have staff representatives on the ACG. Examples include conservation authorities, municipal staff, NCC, Ministry of Culture, Tourism and Sport, MTO.</i></p>

Summary of Discussion - PCG
<div><div>• What is the design speed?</div><div>Usually the design speed is 10 to 20 km/h over the posted speed. The speeds to be used on 174-17 have not been reviewed.</div></div>
<div><div>• Were collisions on Innes looked at?</div><div>No. The 174-17 is the corridor under examination. The Study Area is the area considered for physical improvements to address the problems on 174-17.</div></div>
<div><div>• What about East Lane where the last fatality occurred? Why isn't it identified as a location with potential for safety improvement?</div><div>The analysis compared each mid block location and intersection with other similar locations to estimate the "expected" number of collisions for each location. While the fatality did not make this location join the list, it is also useful to have your anecdotal evidence in our safety review.</div><div>There will be a new intersection on 174 at the new Cardinal Creek Village development.</div></div>
<div><div>• Where are the fish Species at Risk?</div><div>Mostly in the Ottawa River though the American eel has been making its way into Green's Creek. As of this summer, the habitat of these species will also be protected.</div></div>
<div><div>• Does the Village of Cumberland have a heritage designation?</div><div>No, not for the village as a whole.</div></div>
<div><div>• Add the ferry system to the transportation map.</div></div>
<div><div>• What is Baseline Road classified as?</div><div>Collector (to be confirmed).</div></div>
<div><div>• What were the dates of the travel time studies?</div><div>The studies east of Trim Road were done in the fall of 2012 after the sinkhole was repaired. West of Trim Road the surveys were done in May 2011 by the City of Ottawa. Other travel runs were done. We have used the longest travel times out of the complete sample as illustrations.</div></div>
<div><div>• Rockland drivers leave early.</div><div>Yes we adjusted the count times to capture the peak from Rockland, which starts before 7 am (typically the start time for traffic counts).</div></div>
<div><div>• Does TDM include moving jobs?</div><div>No that would be a planning issue.</div></div>
<div><div>• Congestion charges could be considered to reduce travel demand.</div></div>
<div><div>• Suggest roundabouts instead of signals.</div><div>This will be considered later in the study.</div></div>
<div><div>• Passing lanes just make drivers drive faster.</div></div>
<div><div>• Does widening mean an additional lane in each direction?</div><div>Yes.</div></div>
<div><div>• At City Hall on January 29, planning goals included nurturing/encouraging rural villages. Will this be part of the EA Study?</div><div>Yes.</div></div>
<div><div>• We feel that a widened road would devastate Cumberland Village.</div></div>
<div><div>• Opinions on growth may be different.</div><div>The OP is looking at this. Growth in a "rural way" within the legal boundaries of the village.</div><div>We understand the community's interest in improved pedestrian access to the waterfront across 174 and that the 174 shod not act as a barrier.</div><div>The 1992 study did show a service road through the village. This is not anticipated this time.</div></div>
<div><div>• Are noise impacts to be studied?</div><div>Yes. Note that it is the rear yard (or outdoor living space) that is considered in the noise analysis.</div></div>
<div><div>• What are the concerns with building along the river?</div><div>Road construction adjacent to a watercourse is possible but over the river is more difficult. Spills and salt runoff is an issue close to the watercourse.</div></div>
<div><div>• Runoff is an issue going from 2 to 4 lanes.</div><div>Stormwater management will be required.</div></div>
<div><div>• Did you look at new routes versus widening, such as on 416?</div><div>Yes. We will continue to do this in the next phase with the alternative solutions carried forward.</div></div>

Summary of Discussion - PCG
<div><div>• If the section from Trim Road to Rockland is a new route, we would like the existing 174-17 to be a parkway with bikepaths along the river.</div></div>
<div><div>• We would like to discourage any more entrances onto 174-17.</div></div>
<div><div>• There is MTO/City land next to 1891 that is designated as part of the Algonquin settlement.</div></div>
<div><div>• Will you consider that the planned growth is to the south of the 174-17 in the evaluation? In the future everyone would have to travel through the community to reach a widened 174-17 if that is selected.</div><div>Yes the location of growth will be considered.</div></div>
<div><div>• Can Trim Road be the connector from Innes or a new alignment to 174?</div><div>This will be examined.</div></div>
<div><div>• The ferry system is a major contributor of traffic to Cumberland Village.</div></div>
<div><div>• At the Liveable Ottawa Presentation at City Hall, there was a discussion on the use of peak period rather than peak hour for analysis. Did the team use the peak period for travel demand or the peak hour as this methodology may affect the need to widen?</div><div>Peak hour was used. The TMP needs approval from Council prior to implementing the peak period methodology.</div></div>
<div><div>• Widening the road will decrease congestion, thus discouraging transit use. This is an important consideration for the City going forward.</div></div>

3.4 First Nations Consultation

First Nations were invited to attend the Agency Consultation Group meeting on January 28, 2013 or to contact the study team to discuss an alternative consultation format that better suited their needs. Invitation letters were mailed to:

- Algonquins of Ontario Consultation Office
- Kitigan Zibi Anishinabeg First Nation
- Métis Nation of Ontario
- Quebec Métis Nation

A copy of each letter is included in **Appendix D** of this report. No response was received from any First Nation group to date. Follow-up will be undertaken by the study team to ensure their input is received for this study.

4. PUBLIC OPEN HOUSES

One of the key objectives of the environmental planning process is to provide the public, interested parties and affected agencies with opportunities for meaningful input. To help ensure this objective is met, comprehensive public and agency notification of the first round of Public Open Houses (POHs) was undertaken.

Newspaper notices were placed in the following local newspapers:

- Ottawa Sun: Friday, January 25 and February 1, 2013
- Le Droit: Friday, January 25 and February 1, 2013
- Journal Vision Newspaper: Thursday, January 31 and February 7, 2013

In addition, the notice was placed on the project web sites at:

www.prescott-russell.on.ca/en/public-works/major-projects
and
www.ottawa.ca/hwy174and17study

The notice was also mailed or emailed to individuals identified on the study contact list. A copy of the English and French notice is provided in **Appendix E**.

Due to the size of the study area and the number of communities affected, the first round of consultation included Public Open House meetings at three different locations on separate nights. The information presented, presentation format and handouts were the same at each meeting. The POH meetings were held:

Tuesday, February 5, 2013 (Cumberland) Cumberland Lions Club, Maple Hall 2552 Old Montreal Road, Cumberland 6:30 to 9pm, presentation at 7:00	Wednesday, February 6, 2013 (Orléans) Sir Wilfrid Laurier Secondary School, Cafeteria 1515 Tenth Line Road, Orléans 6:30 to 9pm, presentation at 7:00
Thursday, February 7, 2013 (Rockland) City Hall, Council boardroom 1560 Laurier Street, Rockland 6:30 to 9pm, presentation at 7:00	

At the Public Open House meetings there was an opportunity to:

- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Review and comment on the proposed alternative solutions and the evaluation of alternative solutions; and
- Ask questions and discuss the project with members of the Study Team.

The POHs provided an opportunity for members of the public to view the display material and to discuss the project with representatives from the United Counties of Prescott and Russell, City of Ottawa and consultant team. Attendees were encouraged to provide written comments.

A presentation was given during the POHs that followed the format of the display material. The presentation and the displays presented at the Public Open House are provided in **Appendix E** and dealt with the following topics:

- Welcome and Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps
- Thank you for attending

A newsletter was prepared for this study and copies were handed out to the public at each POH meeting. A copy of the newsletter in English and French is provided in **Appendix F**.

In total 168 people signed the registration sheets at the 3 locations for the Public Open House meetings. A total of 38 comments were submitted up to the final submission date of February 22, 2013. These included those handed in at the POH meetings and those received by email or mail following the meetings. A copy of these comments,

excluding personal information, is provided in **Appendix G** of this report. The following sections detail the attendance at each location and comments received for each Public Open House meeting.

4.1 Public Open House in Cumberland

The first POH meeting was held:

Tuesday, February 5, 2013
Cumberland Lions Club, Maple Hall
2552 Old Montreal Road
6:30 p.m. to 9:00 p.m., presentation at 7:00 p.m.

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Louis Prévost	Director of Planning
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Patrick Déoux	Lead Project Planner, AECOM
Kelly Roberts	Lead Environmental Planner, Delcan
Danielle Chartrand	Environmental Planner, AECOM
Joelle Doubrough	Environmental Planner, Delcan
Mark Baker	Transportation Engineer, Delcan
Yvon Simoneau	Senior Engineer, LRL

A total of 93 people signed the registration sheet at the Public Open House meeting held in Cumberland. However, it was estimated that approximately 120 people actually attended this meeting. A total of 15 comments were submitted at the Cumberland POH meeting. These comments are summarized in **Section 6.4** of this report.

The study team gave a presentation on the display material presented at the Public Open House meeting. A question and answer period followed the presentation and discussions are summarized below in **Table 7**.

Table 7. Summary of Discussion at Public Open House in Cumberland

Summary of Discussion – POH in Cumberland
<ul style="list-style-type: none">• Whether an alternate route or expansion of existing 174 is selected, property expropriation will be involved. How do you decide what is acceptable? What are the policies? <i>The solution will identify what is needed. At this time we do not know what will be required. The City will proceed in a ‘willing seller/willing buyer’ basis. Property owners that may be impacted will be kept informed of potential property impacts. Best efforts will be made to ensure fairness to both parties.</i>
<ul style="list-style-type: none">• Is expropriation (i.e. number and cost) a factor that will influence the chosen solution? <i>Absolutely. The amount of property, homes, strips of land etc. will all be considered. The fewer number of residents impacted the better.</i>
<ul style="list-style-type: none">• The village (Cumberland) will be decimated if the 174 is widened. There are driveways etc. along the road. You will need a minimum of 6 to 8 lanes to accommodate needs, what about all of the driveways. <i>Premature to say that 6 to 8 lanes will be needed. The Alternate Designs phase of the study will determine requirements and what is required.</i>

Summary of Discussion – POH in Cumberland
<ul style="list-style-type: none">It seems like the right hand doesn’t know what the left hand is doing. They need to be talking and cooperating. What about the vision for a village core in Cumberland? If you widen through the village everyone would be forced to leave and it would go against the village core vision that has been in progress for years. <i>We are aware of the Vision for Cumberland Village, and it will be considered when choosing the final solution. The Cumberland Village Community Association is very active in this Study and is providing input on the Public Consultation Group.</i>
<ul style="list-style-type: none">What about group commuting time and safety? It would be a mistake to widen the 174. There are lots of driveways. If the plan is to widen but then requires lights, this will just slow things down. What is the point? There is nothing on Innes and you can do it without affecting people. <i>Traffic considerations will be evaluated during upcoming phases. At this point it is premature to comment on final traffic design.</i>
<ul style="list-style-type: none">Going back to land expropriation: I recommend that you expand the study area to the south. You may find a more feasible solution. Suggest going south of Baseline. <i>South of Baseline is not within our study area at this time but it is not impossible to look at solutions further south. Please make sure to provide comments with suggestions.</i>
<ul style="list-style-type: none">Transit solutions into Ottawa seem to be lost. There used to be better bus service, and now there's not and that seems detrimental. Alternate mode of transportation should be considered rather than cars.
<ul style="list-style-type: none">Written letter read aloud. Some points include: Why not put in a route south of Rockland? Highway widening options have been explored since the 1980's. What about archaeological resources? Is it more cost effective to expropriate land south of Innes? How much land will be expropriated? What are the costs of moving utilities (i.e. hydro, water, sewer etc)? Will utilities go underground? How would access lanes affect existing land owners? What about shaking due to traffic?
<ul style="list-style-type: none">Great study and nice to see such a large meeting in the hall. I think that everyone (all in attendance) agree that the solution is to build south – there is nothing there but Innes Rd. What about transit? Trim is full and it has been expanded. Following the sinkhole, traffic on the 174 was a nightmare. There is no other option than to go around during construction if you construct here. Go south.
<ul style="list-style-type: none">Would it not be more cost effective to purchase now vacant land south rather than have to expropriate properties later along the highway?
<ul style="list-style-type: none">Widening roads will never be enough. All we get is lip service from the mayor with regards to transit etc. <i>All solutions will incorporate public transit. For example in 2031 approximately 43% of people crossing the Green's Creek screenline will be via transit. We still need an additional solution. We are using the existing Transportation Master Plan that incorporates an aggressive transit strategy.</i>
<ul style="list-style-type: none">Would like to see more transit. There is only one bus from Cumberland Village and that’s not enough. Whatever was there was taken way. Want to see the 174 go south with additional transit provided in Cumberland Village <i>The City is looking at improving transit that we have now. There is currently great transit service from Orléans to downtown.</i>
<ul style="list-style-type: none">Do I understand you correctly? There is no ‘ring road’ (by-pass) proposed around the City? I think a ring road is the best solution. Politicians should be exploring this. Pass on this information. <i>This is outside of our study area, which is from the Split to Rockland, not the entire City. A ring road is something that would be explored in a document like the Transportation Master Plan (TMP) for the whole city – not just this study area. The City is in the process of updating the current TMP. Check the City's website for more information.</i>
<ul style="list-style-type: none">Would like to see this section of road designated as a scenic route/drive, not just highway. Would like new road to be further south and not widening of highway through the village. The split is a funnel and it’s a terrible mess. Widening won’t solve the funnel problem at the split. <i>We are carrying forward solutions for further evaluation, these include widening the existing as well as alternative routes.</i>
<ul style="list-style-type: none">It seems that going south with access to other routes makes more sense. What about the Cardinal Creek Village (CCV)? Their plan has just been approved. Their plan does not accommodate certain links to the 174. I have seen the plans and there is no direct route. Is there any sort of interaction between the EA study team and the CCV planning committee? <i>We have held discussions with the CCV team. The City has been involved in the CCV process and is working with the developers. CCV process is more advanced than the 174 EA. They have received approval to go ahead. The CCV has limited our options but there are still other options to consider. Want to look at other options than just widening.</i>
<ul style="list-style-type: none">The City should be buying vacant land now with respect to the Cardinal Creek so we can still have that route alternative. Once the development is built it’s too late.

Summary of Discussion – POH in Cumberland
<ul style="list-style-type: none">Where would the link to Trim Road be with respect to CCV? Another main street? Connect at Wilhaven? <i>That will be looked at in more detail in future phases of the EA. New routes etc.</i>
<ul style="list-style-type: none">Alternative solutions were examined in the past. Innes Road was examined in the 1990’s then the City put in big box stores. All of these studies are a waste of money and a waste of effort.
<ul style="list-style-type: none">In favour of the Innes option. Logical connections with Trim Road and new connections can be accommodated. Or are developments pushing so that widening is the only option?
<ul style="list-style-type: none">What about an alternative connecting with the connection to the south at Walkley? A Walkley connection has been talked about in the past. Find a way to accommodate not just funnel more traffic to the 174. What about the Hunt Club/Walkley. Can you expand the study area limits? Don’t eliminate other options by just looking at widening.
<ul style="list-style-type: none">What about conservation of the Ottawa River? When do you look at the river and creeks and talk with the Rideau Valley Conservation Authority (RVCA)? <i>RVCA and South Nation Conservation (SNC) are part of the Agency Consultation Group. We met with them last Monday and they will be involved throughout the process.</i>
<ul style="list-style-type: none">A couple of routes identified to the south. How do you make it work? How do they connect to the 174? <i>Carrying forward multiple alternative solutions. At this time an actual location for the connections has not been identified. They will continue to be evaluated IF appropriate connections can be found. Not sure right now. The next step is to look at the options. Early screening has ruled out certain solutions with major impacts, such as widening Old Montreal Road through the Village of Cumberland, right outside of the hall. We will be looking at alternative solutions including new corridors.</i>
<ul style="list-style-type: none">What about Cumberland Transitway? When is that happening? To improve mass transportation via transit, why not direct people to those areas? <i>Frank Kenny to Blair Station will not happen soon. The existing TMP is good to the year 2031. The Cumberland Transitway was identified in the TMP for implementation closer to 2031. Our study identifies those transit users by making use of the existing TMP projections. Orléans already has great transit use and it is anticipated to improve.</i>
<ul style="list-style-type: none">The access issues are east of Cumberlrand (i.e. Rockland). <i>Rockland does have transit (CR Transpo). The route has pick-ups in Rockland then takes the 174 into Ottawa where the buses use the Transitway.</i>
<ul style="list-style-type: none">The traffic on the 174 has been bumper to bumper for years. Where are all of these people coming from? Why are you bringing people closer to the river, there is no new development along the river? If the development is occurring to the south shouldn’t the new road be built south? <i>Innes Road to the south is being explored as an option.</i>
<ul style="list-style-type: none">Have Cumberland parking lots been considered? <i>Looking at that. Partnership is important. The last study was completed in the 1990's. At that time the Ministry of Transportation (MTO) downloaded the road to the municipalities and they did not have funding to implement the study.</i>
<ul style="list-style-type: none">Is funding in place for work to happen to this road? Does the funding have a shelf life? What are the terms of the funding? Does it apply only to widening of the 174 but not if an alternate route is chosen? <i>Funding was committed in 2007-2008. We are not familiar with the terms of the funding agreement. The City Project Manager is working closely with the Mayor's office which is working closely with the MPP and MP. They are aware of the details of the study and that the study is considering options other than widening. They indicate that funding is still available.</i>
<ul style="list-style-type: none">Our input doesn’t seem to count. Questions are phrased to get the answers you want. Cumberland is an existing community yet you are willing to affect us but CCV is a new development not yet built and there is more consideration for that community. Traffic lights on a highway will not help speed up traffic.
<ul style="list-style-type: none">Added lanes and unrestricted development (i.e. CCV) just create a funnel effect with more people. The biggest problem will continue to be at the split where it is already a bottleneck. You need to look at the entire picture, development in the south, different routes etc. I have an existing noise barrier but all it does is block the sight. What about the Transitway? Where is it in relation to all of this? What will more vehicles per hour do to property values? Everything seems to be pointing toward the widening of the 174. The NCC may have to make exceptions/accommodations within the Greenbelt. Transit is not the only answer. It is time for the politicians to start a dialogue. Impacts on property value and quality of life need to be considered.

Summary of Discussion – POH in Cumberland
<ul style="list-style-type: none">The City is always talking about intensification. Taxpayers in Cumberland are paying for new services within the City, for example, the LRT, Transit etc. Cumberland taxpayers are always investing in new infrastructure for the City. Where is the transit in Cumberland? What about Rockland? Why do we have to pay for them to use infrastructure? What about Rockland providing transit for their own people. <i>It is too early to comment on changes to Rockland transit, but always trying to improve.</i>
<ul style="list-style-type: none">For anyone interested there is a CR Transpo presentation tomorrow (Wed, Feb 6th) at the Rockland Council Meeting.

4.2 Public Open House in Orléans

The second POH meeting was held:

Wednesday, February 6, 2013
Sir Wilfrid Laurier Secondary School, Cafeteria
1515 Tenth Line Road, Orléans
6:30 p.m. to 9:00 p.m., presentation at 7:00 p.m.

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Patrick Déoux	Lead Project Planner, AECOM
Vanessa Skelton	Traffic Engineer, AECOM
Kelly Roberts	Lead Environmental Planner, Delcan
Danielle Chartrand	Environmental Planner, AECOM
Joelle Doubrough	Environmental Planner, Delcan
Yvon Simoneau	Senior Engineer, LRL

A total of 28 people signed the registration sheet at the Public Open House meeting held in Orléans. A total of 2 comments were submitted at the Orléans POH meeting. A summary of these comments is provided in **Section 6.4** of this report.

The study team gave a presentation on the display material presented at the Public Open House meeting. A question and answer period followed the presentation and discussions are summarized below in **Table 8**.

Table 8. Summary of Discussion at Public Open House in Orléans

Summary of Discussion – POH in Orléans
<ul style="list-style-type: none">Great presentation. 2 comments. You didn't mention the most important thing, impact assessment, primarily as it will impact people. Human health and safety will be impacted. The study area is an urban centre but some areas are less urban. Noise in some areas is beyond healthy levels already. <i>We will be looking at noise and health impacts through various criteria.</i>

Summary of Discussion – POH in Orléans
<ul style="list-style-type: none">What about other projects? You didn't present how it blends and works with other projects in the area. What about negotiations with the Greenbelt, noise barriers, interprovincial bridges etc? I agree that growth is in Orléans and Rockland. What % of traffic is from the growth areas south of Orléans and Rockland? Why widen the corridor where it is now, when the growth and traffic is from South and East. I think that all of this should be presented and explored.
<ul style="list-style-type: none">Concerned about sound barriers. There is a high noise level in existing conditions. The house shakes when bus/trucks go by and there is only a chain link fence. Will noise barriers be installed? <i>A noise study will occur as part of the EA. Not sure if a City of Ottawa noise retrofit study is currently underway.</i>
<ul style="list-style-type: none">Barriers are planned by the City. People's homes do shake under existing conditions. Risk to public safety and legal aspects for City and consultant. Very serious for residents of the corridor. <i>Noise and vibration will be evaluated during the next phases for specific alternatives.</i>
<ul style="list-style-type: none">What about land expropriation? Between the Split and Trim? <i>Premature to say if land expropriation would be required and where. The next phase will look at where to widen the road – for example, can it be expanded toward the centre of the road to reduce land requirements.</i>
<ul style="list-style-type: none">Why are you even looking at the impact of traffic? Buses and trucks make the same noise here as they do on the west end of the Queensway. Sound barriers on the Queensway for noise. <i>Our noise study will look at 2031/future conditions and where noise could be. This noise study will be independent of any retrofits that the City does to address existing conditions.</i>
<ul style="list-style-type: none">Cumberland resident who spoke at last night's meeting came to reiterate her points for the politicians: i.e. ring road needed around the City of Ottawa as growth is south, impacts to access, wells, septic, noise, vibration to adjacent land owners, cost and impacts to local residents in the Cumberland area. Buy vacant land now to build new road in future.
<ul style="list-style-type: none">Councillor Monette indicated that the City tried to give the Road (OR174) back to the province in the past, but they would not accept it.
<ul style="list-style-type: none">You're projecting traffic to 2031, the interprovincial bridge is going in – but where? Where is that growth going to be? Is everything being captured in your study? <i>The preferred/selected interprovincial bridge corridor has not been identified so we have been unable to include it in our model. The transportation impacts will be a part of the EA for the Interprovincial Bridge because of the timing of the studies. We cannot make an assumption about where it will be, but even at the most easterly corridor being considered, it will still be located within the Greenbelt. The NCC has completed a joint study with the City – they have reached an agreement that there will not be any new transportation corridors allowed within this section of the Greenbelt. Mer Bleue is an internationally recognized wetland and as such no new corridors will go through.</i>
<ul style="list-style-type: none">In 1984 I was on Gloucester City Council. At that time the City was begging for Anderson Road for Rockcliffe Parkway connections. Anderson Road DOES go right through Mer Bleue Bog. <i>Yes, due to new policy, no new roads will be built through this area.</i>
<ul style="list-style-type: none">This project is joint venture between 2 proponents (Prescott Russell and Ottawa). What happens if the co-proponents don't agree? <i>They hired a consultant to provide advice and expertise. An agreement identifies the limits of the co-proponency related to the funding received from Prescott-Russell. There is a legal agreement between the City and Prescott-Russell that outlines the processes that will be taken. The County has hired project managers to guide the process, and there is a steering committee in place with members from each co-proponent that will help to resolve any critical issues that can't be taken care of by the study team.</i>

4.3 Public Open House in Rockland

The third POH meeting was held:

Thursday, February 7, 2013
City Hall, Council Chambers
1560 Laurier Street, Rockland
6:30 p.m. to 9:00 p.m., presentation at 7:00 p.m.

The members of the project team in attendance included:

United Counties of Prescott and Russell

Marc R. Clermont	Director of Public Works
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Patrick Déoux	Lead Project Planner, AECOM
Vanessa Skelton	Traffic Engineer, AECOM
Kelly Roberts	Lead Environmental Planner, Delcan
Danielle Chartrand	Environmental Planner, AECOM
Mylene Corbeil	Administrative Assistant, Delcan
Yvon Simoneau	Senior Engineer, LRL

A total of 47 people signed the registration sheet at the Public Open House meeting held on February 7, 2013 in Rockland. A total of 2 comments were submitted at the Rockland POH meeting. A summary of these comments is provided in **Section 6.4** of this report.

The study team gave a presentation on the display material presented at the Public Open House meeting. A question and answer period followed the presentation and discussions are summarized below in **Table 9**.

Table 9. Summary of Discussion at Public Open House in Rockland

Summary of Discussion – POH in Rockland
<ul style="list-style-type: none">Traffic counts have increased over the years on Wilhaven due to congestion on Highway 174. Have the traffic volumes on these roads been factored into the study? People are taking alternate routes to avoid travelling on Highway 174/17. <i>Traffic counts on various local roads were examined.</i>
<ul style="list-style-type: none">Has the location for the new road been determined? <i>Route location has not been determined yet; this will be examined during the next phase.</i>
<ul style="list-style-type: none">Where exactly south of Wilhaven is the new road being considered? Residents of Rockland will not want to travel further south. <i>Options considered will be between Wilhaven and Innes, no further south.</i>
<ul style="list-style-type: none">Will traffic lights be eliminated? They just slow down traffic and cause congestion?
<ul style="list-style-type: none">What days were traffic counts taken? Traffic on Thursday and Friday is much lower than the rest of the week. <i>Traffic counts are usually undertaken by the City on Tuesday, Wednesday and Thursdays as the traffic department is aware of lower traffic counts on Monday and Fridays. Traffic counts examined in this study were taken in November 2011 by the City of Ottawa and in Rockland in May 2011 and 2012.</i>
<ul style="list-style-type: none">Financing for this project was announced in 2010 but the study ends in 2015 and then another 6-7 years before it is built. Other solutions should be implemented in the interim. Consider implementing a local OC Transpo service to Hwy 174/Canaan intersection, which could be used as a pick-up/drop-off area for commuters. The bus could easily turn around at this location.Consider adding a third HOV lane which would not need an EA as no widening is required.

4.4 Summary of Comments Submitted

Comment-Questionnaire sheets were handed out at each public open house meeting to solicit public input. A copy of the Comment-Questionnaire is provided in **Appendix G** of this report. The outline was as follows:

About You

Where do you live? (Street Name)
What specific interest do you have in this study?

Primary Issues

Do you have any comments on the background information presented tonight?

Alternative Solutions

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Consultation Groups

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Consultation Groups

Do you feel that the information presented at this Open House has given you a better understanding of the study?

General Comments

Do you feel that the information presented at this Open House has given you a better understanding of the study? If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

4.4.1 Summary of Comments Submitted at POH in Cumberland

A total of 15 comments were submitted at the Cumberland POH meeting on February 5, 2013. These comments are summarized in **Table 10** and copies of the submissions, excluding personal information, are provided in **Appendix G** of this report.

Table 10. Summary of Comments Submitted at Public Open House in Cumberland

Description of Comments	Comment Sheet #	Number of Comments Received
● Request to participate on consultation group	1, 4, 5, 8, 13	5

Description of Comments	Comment Sheet #	Number of Comments Received
• Widening would add more traffic / congestion onto Highway 174	2, 6, 9, 10, 13, 15	6
• Impact on community in Cumberland	2, 6, 9, 11, 12, 15	6
• Examine alternatives to work together rather than separately. Should start at Trim Road.	2,	1
• Costs including expropriation should be identified	2, 15,	2
• Add to mailing list	3,	1
• Property impacts are issues	4, 6, 10, 12, 15,	5
• Build new road south of Ottawa Road 174 / ring road where development is occurring	4, 7, 12, 15	4
• Better transit options	5, 13	2
• Opposed to widening Ottawa Road 174	6, 9, 10,	3
• Ensure Cardinal Creek development does not interfere with linking 174 to Innes corridor	8	1
• Perception is that decision is already made to widen Ottawa Road 174	8	1
• New route would provide an additional route for travellers	9	1
• Supports widening Ottawa Road 174	14	1
• Noise & vibration, property access, wells & septic, archaeology/heritage are issues	15	1

4.4.2 Summary of Comments Submitted at POH in Orléans

A total of 2 comments were submitted at the Cumberland POH meeting on February 6, 2013. These comments are summarized in **Table 11** and copies of the submissions, excluding personal information, are provided in **Appendix G** of this report.

Table 11. Summary of Comments Submitted at Public Open House in Orléans

Description of Comments	Comment Sheet #	Number of Comments Received
• Noise & Vibration are issues	16	1
• Support new route south of Wilhaven	17	1

4.4.3 Summary of Comments Submitted at POH in Rockland

A total of 2 comments were submitted at the Rockland POH meeting on February 7, 2013. These comments are summarized in **Table 12** and copies of the submissions, excluding personal information, are provided in **Appendix G** of this report.

Table 12. Summary of Comments Submitted at Public Open House in Rockland

Description of Comments	Comment Sheet #	Number of Comments Received
• Noise & Vibration are issues	18	1
• Increased traffic on Landry is a concern	18	1
• Consider both short term and long term solutions	18	1
• Consider lane reductions during construction of the LRT	19	1

4.4.4 Comments Submitted after the POH Meetings

An additional 19 comments were received by mail or email following the public open house meetings. Only comments received by February 22, 2013 were included in this report. Any comments received after this submission date were still considered in this study and included in the Environmental Study Report documentation.

A summary of these comments is provided in **Table 13** and copies of the submissions, excluding personal information, are provided in **Appendix G** of this report.

Table 13. Summary of Comments Submitted after the Public Open House Meetings

Description of Comments	Comment Sheet #	Number of Comments Received
• Request to participate on consultation group	20, 21, 24, 30, 38	5
• Build new road south of Ottawa Road 174 / ring road where development is occurring	20, 21, 22, 23, 26, 27, 28, 30, 33, 34, 35, 36, 38	13
• Opposed to widening Ottawa Road 174	20, 22, 36	3
• Concern about impact on community in Cumberland	22, 30, 33, 38	4
• Concerned about property impacts	22, 25, 27, 28, 29, 34	6
• Costs / Funding are issues	23, 26, 30	3
• Supports widening Ottawa Road 174	23, 37	2
• Supports this study to improve traffic conditions	24	1
• Perception is that decision is already made to widen Ottawa Road 174	26, 30, 33	3
• Traffic from Cumberland Ferry is an issue	26	1
• Consider both short term and long term solutions	29, 32	2
• Widening would add more traffic / congestion onto Highway 174	30, 33	2
• Provide GO Transit for commuters between Ottawa and Hawkesbury	31	1
• Provide 3-lane highway by adding ½ lane on each side of existing highway and incorporate a reversible lane	32	1
• Provide better transit options	32, 36	2
• Concern about impacts on wildlife, need to provide safe crossing access under roadway and access to river	36	1

5. CONCLUSION

The overall agreement at all public open house meeting locations was that something needs to be done to address the current traffic conditions/congestion on Ottawa Road 174 / County Road 17.

Many residents voiced concern about property impacts from increased noise and vibration levels due to increased traffic volume and widening of the highway. Cost was also a concern for many.

The residents of Cumberland opposed the widening of OR 174 indicating the residents along the highway and the community would be severely impacted. The consensus appeared to be that a new route should be built further south as the growth and development is occurring south. In addition, residents also requested better transit options for Cumberland.

Annex D-1

- Invitation letter to Agency Consultation Group Meeting #1
- Presentation for Agency Consultation Group Meeting #1

Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study

We invite you to the first ACG meeting to be held on:

Monday, January 28, 2013
9:00 a.m. to 12:00 p.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West
Ottawa

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013 6:30 p.m. to 9:00 p.m. Cumberland Lions Club Maple Hall 2552 Old Montreal Road Cumberland	Wednesday, February 6, 2013 6:30 p.m. to 9:00 p.m. Sir Wilfrid Laurier Secondary School, Cafeteria 1515 Tenth Line Road Orléans	Thursday, February 7, 2013 6:30 p.m. to 9:00 p.m. City Hall Council boardroom 1560 Laurier Street Rockland
--	--	---

The purpose of the first ACG meeting (and POH #1) is to:

- Introduce the project and the role of the ACG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at:
www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email:
Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

Invitation à la première réunion du Groupe de consultation d’organismes (GCO)
Étude d’évaluation environnementale de portée générale – Route 174 d’Ottawa et route 17 du
comté de Prescott-Russell

Nous vous invitons à la première réunion du GCO, qui aura lieu le :

Lundi 28 janvier 2013
De 9 h 00 à 12 h 00
l’hôtel de ville, Salle Honeywell
110, avenue Laurier Ouest
Ottawa

Si vous n’êtes pas en mesure d’assister à cette réunion, vous souhaiterez peut-être vous joindre à nous pour la première série de séances portes ouvertes, qui se dérouleront aux dates et endroits suivants :

Mardi 5 février 2013 De 18 h 30 à 21 h Club Lions de Cumberland Salle Maple 2552, chemin Old Montreal Cumberland	Mercredi 6 février 2013 De 18 h 30 à 21 h École secondaire Sir Wilfrid Laurier, Cafétéria 1515, chemin Tenth Line Orléans	Jeudi 7 février 2013 De 18 h 30 à 21 h Hôtel de ville Salle du Conseil 1560, rue Laurier Rockland
---	--	--

Les objectifs de la première réunion du GCO et de la première série de séances portes ouvertes sont les suivants :

- Présenter le projet et le rôle du GCO;
- Constater la nécessité du projet et les conditions actuelles;
- Prendre connaissance des critères d’évaluation proposés;
- Déterminer les solutions de rechange;
- Examiner l’évaluation des solutions de rechange.

Vous trouverez des renseignements au sujet de l’étude, y compris les avis, sur le site Web de la Ville d’Ottawa, au www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure ou au www.ottawa.ca/etuderoute174et17.

Veuillez donner confirmation de votre participation à Danielle Chartrand par téléphone, au 613-820-8282, poste 238, ou par courriel, à l’adresse Danielle.chartrand@aecom.com. Pour obtenir de plus amples renseignements sur le projet, veuillez communiquer avec moi ou avec :

Angela Taylor, ing. Ingénieure principale de projet Ville d’Ottawa Tél. : 613-580-2424, poste 15210 Courriel : Angela.Taylor@ottawa.ca	Marc R. Clermont, ing. Directeur des Travaux publics Comtés unis de Prescott et Russell Tél. : 613-675-4661, poste 3100 Courriel : MClermont@prescott-russell.on.ca
---	---

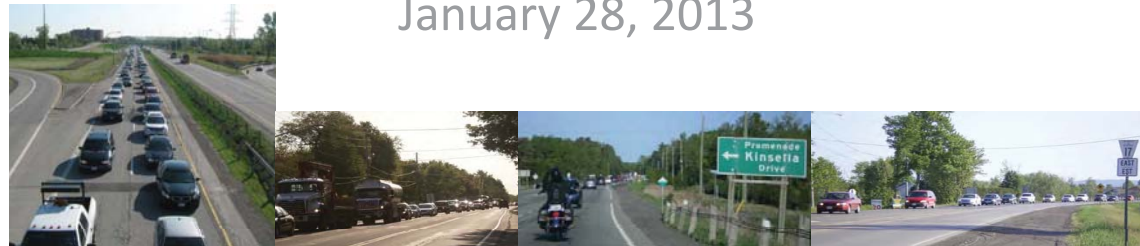
Recevez nos salutations distinguées.
AECOM Canada Ltd.

Valerie McGirr, ing.
Chargée de projet de firme de conseils
Courriel : Valerie.mcgirr@aecom.com
Tél. : 613-820-8282, poste 243

AECOM
1150, promenade Morrison, pièce 302, Ottawa (Ontario) K2H 8S9
Tél. : 613-820-8282; téléc. : 613-820-8338
www.aecom.com

Agency Consultation Group Meeting

January 28, 2013



Introductions



Agenda

- Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps



Co-Proponents

- Prescott Russell
 - Marc Clermont
 - Dave McAvoy/Phil Pawluik
- City of Ottawa
 - Angela Taylor



Study Team

- AECOM
 - Valerie McGirr
 - Vanessa Skelton
- Delcan
 - Kelly Roberts
- Study Team
 - Houle Chevrier, LRL, Golder

Role and Responsibilities

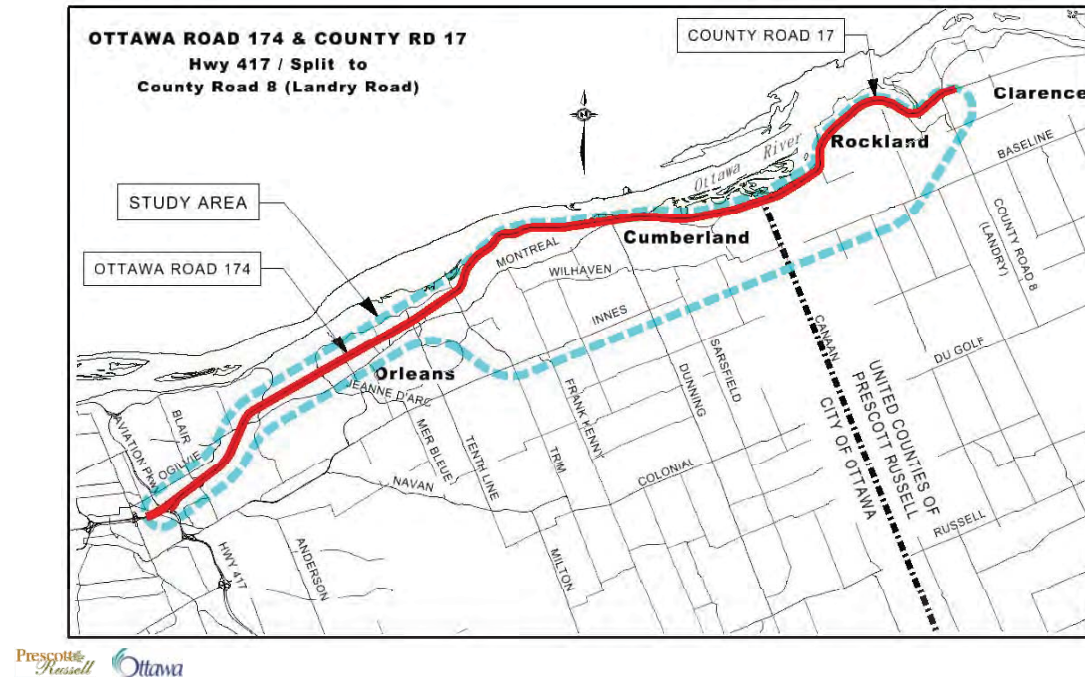
- ACG Role is to:
 - address technical issues
 - comment on the special studies required to fully assess the various alternatives
 - Provide guidance to the City and Counties regarding following procedures, legislation and appropriate policies
 - Disseminate information and collect comments from your agency/group
- Federal, provincial and municipal agencies are invited
- Three (3) meetings in total

Agency Consultation Group

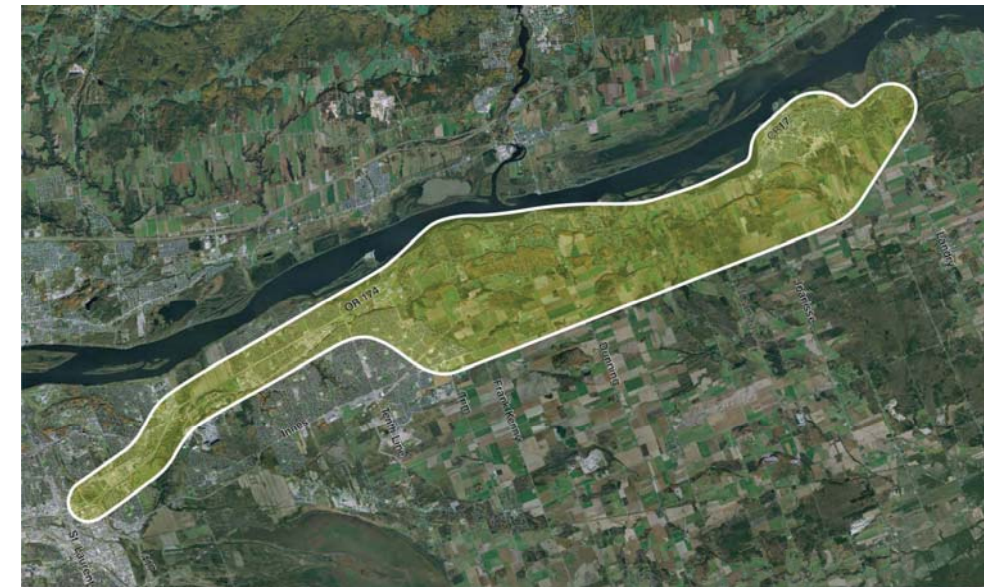
- Round Table Introductions

Project Overview

Study Area



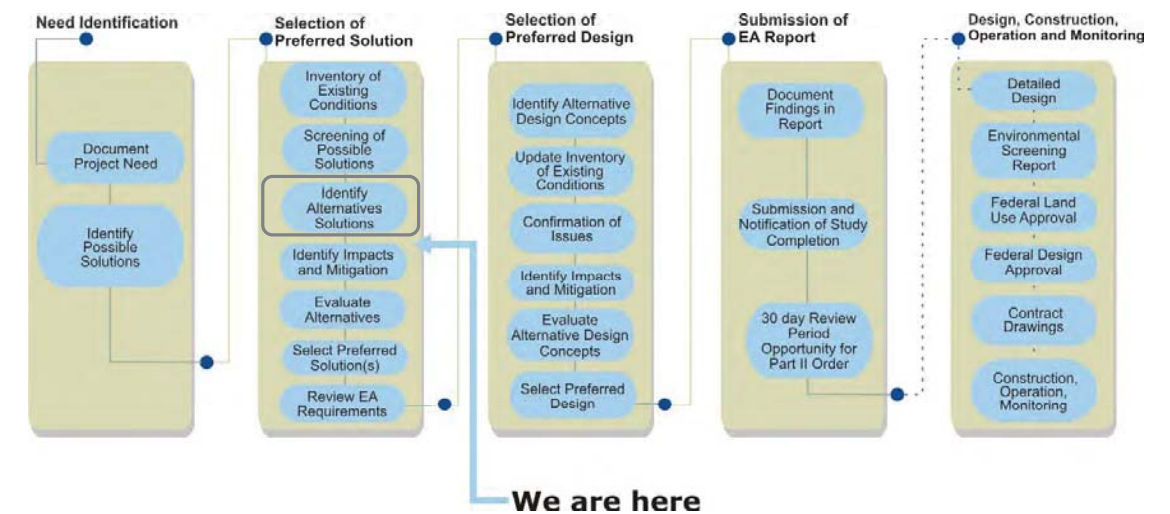
Study Area



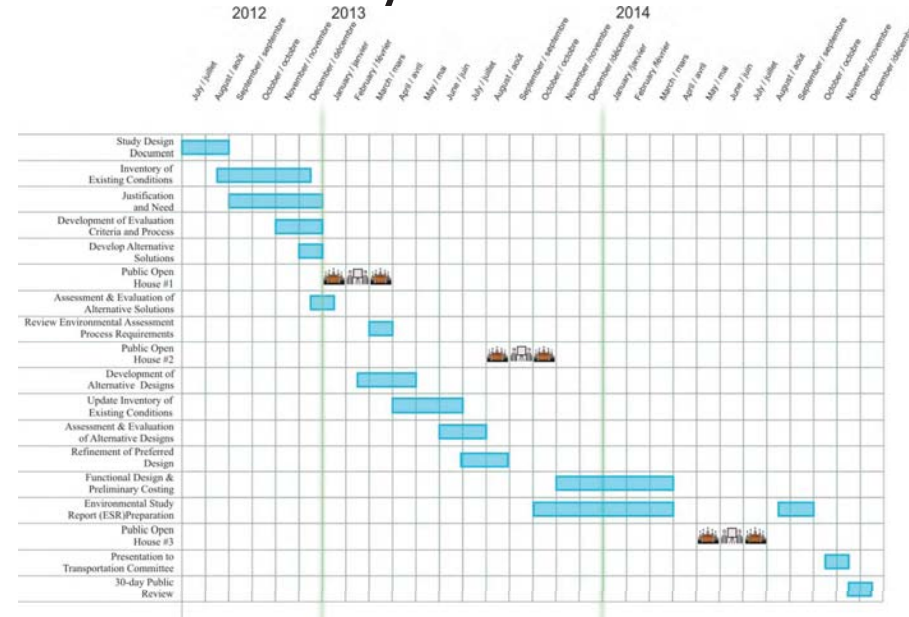
Background

- Provincial Highway 17 until 1997
- Congestion and safety problems documented for years
- Ottawa River and Mer Bleue Bog limit potential east-west routes
- OR 174-CR 17 only high capacity arterial in area
- Widening of OR 174 to 6 lanes to Jeanne d'Arc in TMP
 - Phase 2: 2016 to 2022
- Growth continuing. Development in Orléans and Rockland
- UCPR and City jointly initiated Municipal Class EA Study – Schedule C

Study Process



Study Schedule

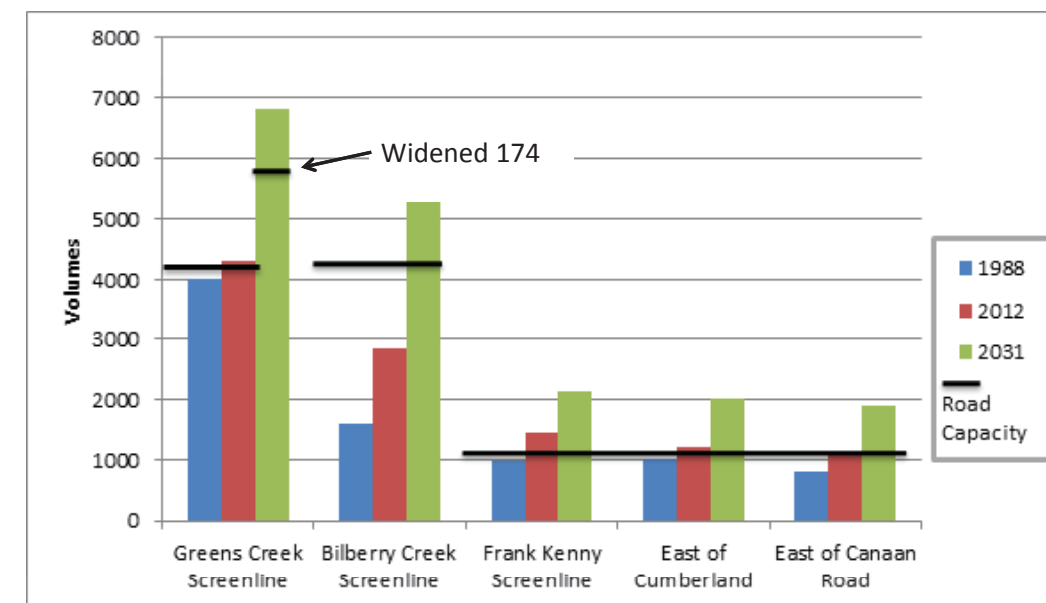


Needs Assessment

Overview

- Transportation problems in the area have been identified for many years. The Needs Assessment for this study has reviewed previous reports and utilized new information to assess:
 - existing and future transportation capacity
 - safety improvement opportunities
 - impacts of new developments and expanded urban areas

Volumes and Capacity Over Time

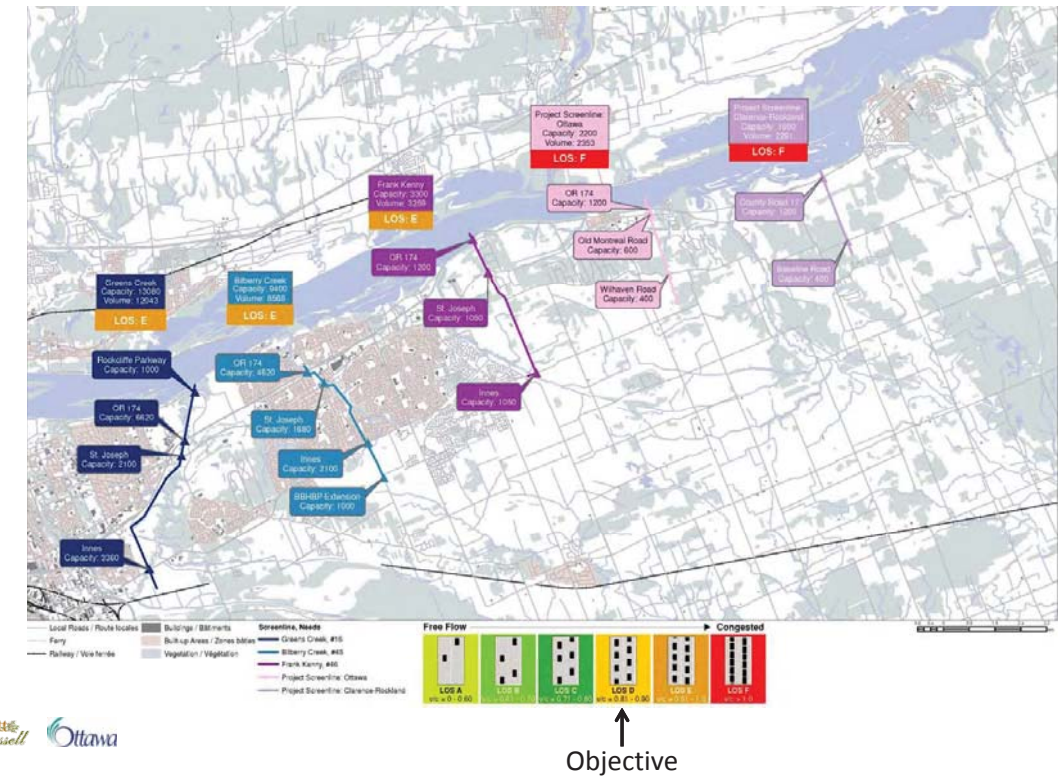


Capacity

- 2031 AM peak volumes examined using screenlines
 - Assuming all road and transit infrastructure planned for 2031 in place
- Considered downstream capacity constraints
 - Highway 417
- Tested for sensitivity for transit use
 - 5 and 10 percentile increase and decrease
- Even with increased transit use and other travel-reducing strategies, will need additional capacity from the split to Rockland

Safety

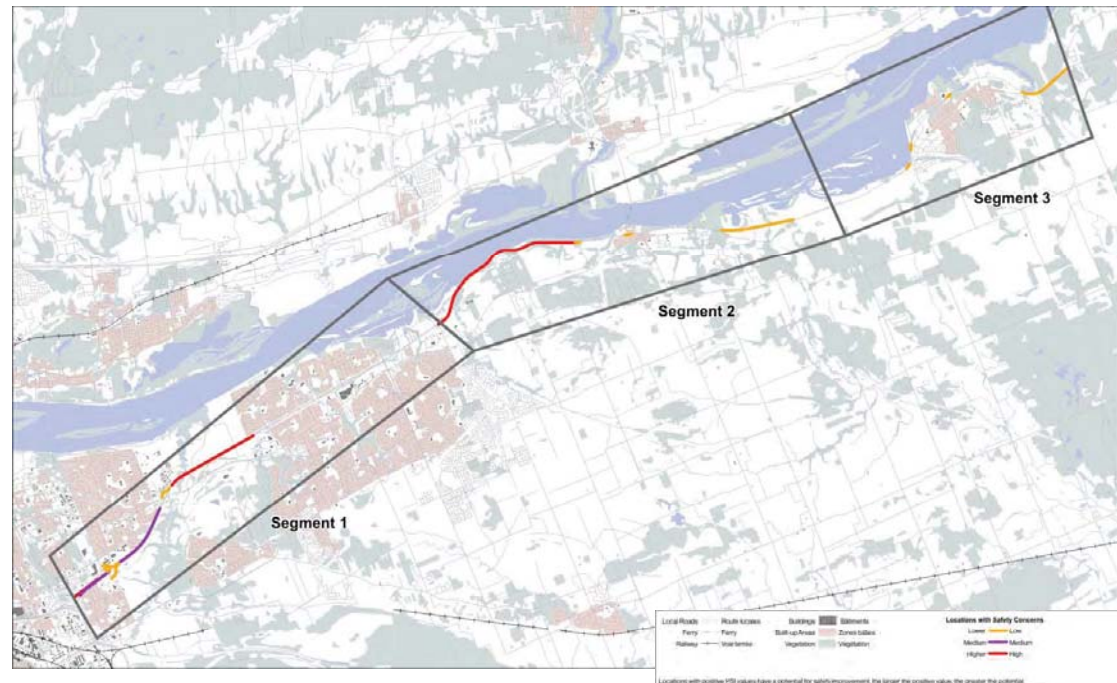
- Considered recent collision data
 - Split to Trim – 1487 collisions
 - Trim to Canaan – 465 collisions
 - Canaan to Landry – 287 collisions
- Examined:
 - Collision severity (property damage only, injury and fatal)
 - Collision impact type (turning, angle, rear end, side swipe, single motor vehicle)
 - Lighting condition (daylight, dark, dawn, dusk)
 - Weather (clear, rain, snow)
 - Time of day (AM/PM peak, mid day, evening/early morning)
- Considered road characteristics (horizontal and vertical curves, lane width, passing opportunities, clear zone)



Safety

continued

- Significant contributors to 174-17 collision experience:
 - Speed, roadside character, access, interchange and intersection geometry, lighting, positive guidance, passing opportunities, accommodating transit
- Best potential to improve safety
 - Address congestion
 - Driver awareness
 - Intersection modifications
 - Improve lighting conditions



Existing Conditions

Conclusions

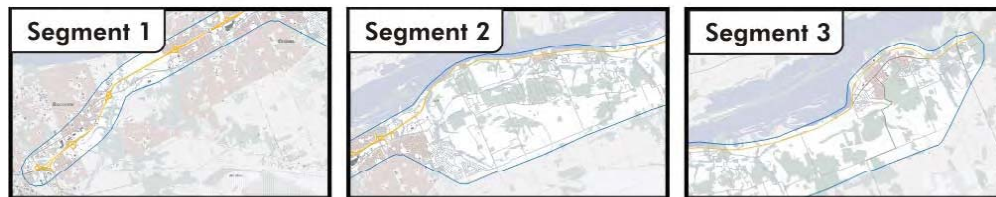
- There is a need to provide additional transportation capacity
- There are opportunities for safety improvements in the OR174/CR17 corridor

Overview

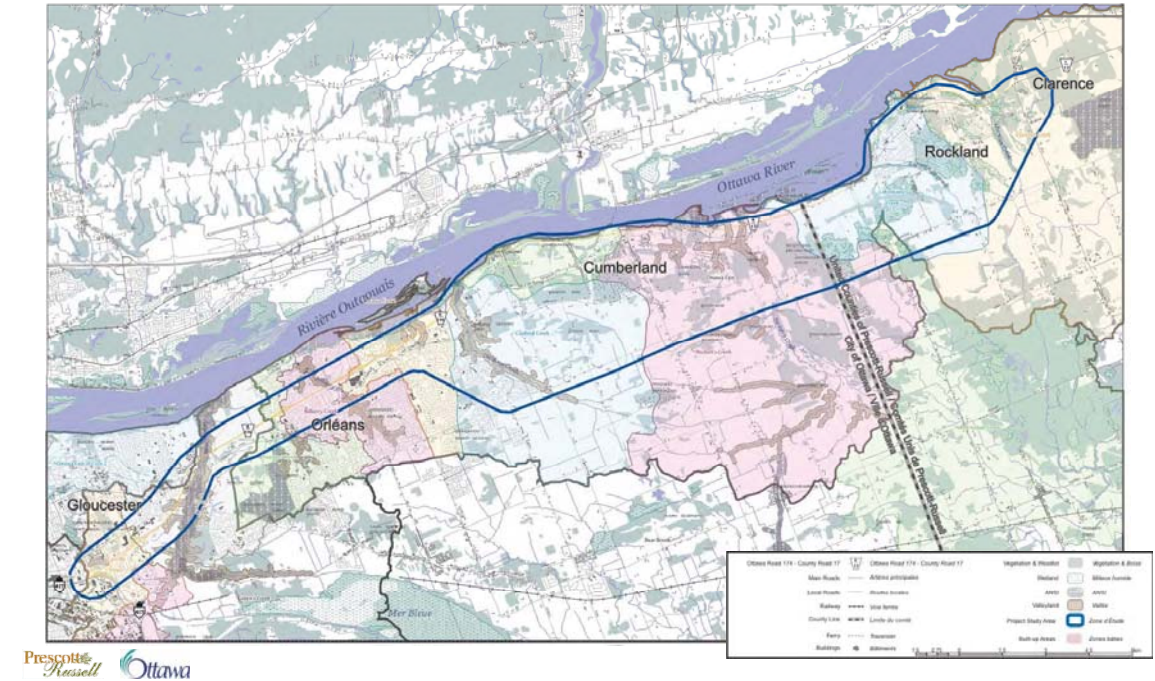
- Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by a transportation facility. The information will be used to:
 - develop reasonable alternatives
 - identify impacts
 - evaluate alternatives
- Information will be updated, as necessary, as the study progresses and more detailed information is required.

Segments

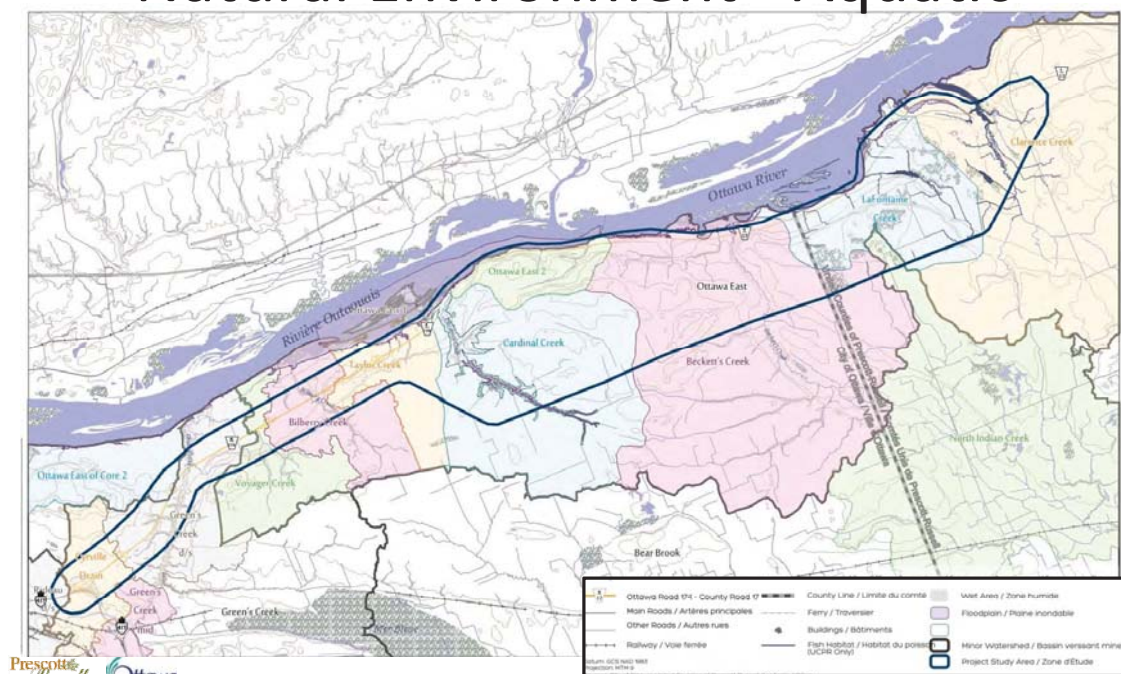
- For illustration and context purposes, the study area has been arranged in three (3) segments
- Study area boundaries may also change to reflect the conditions being considered



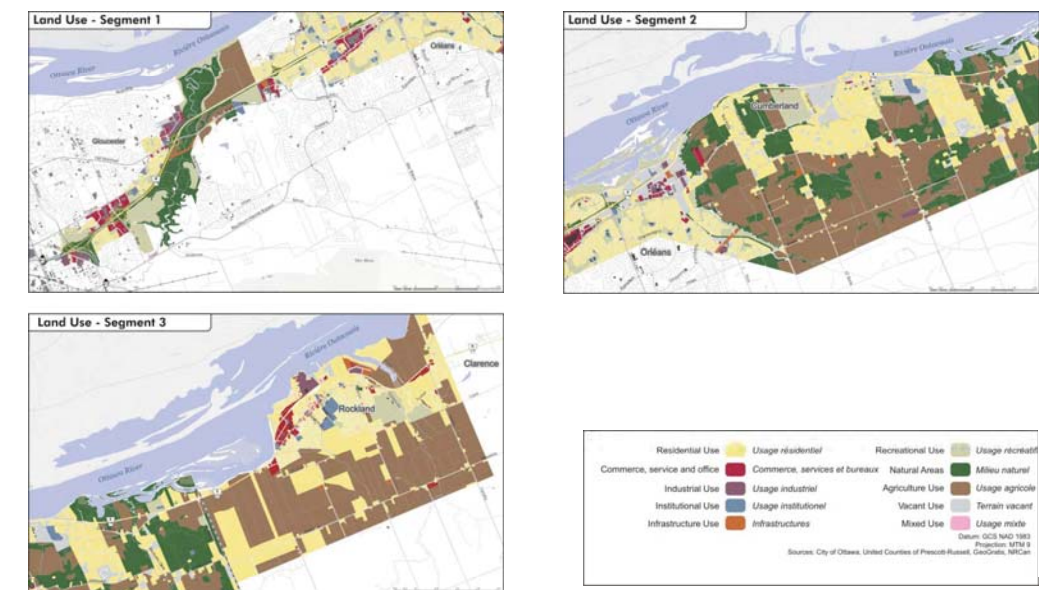
Natural Environment - Terrestrial



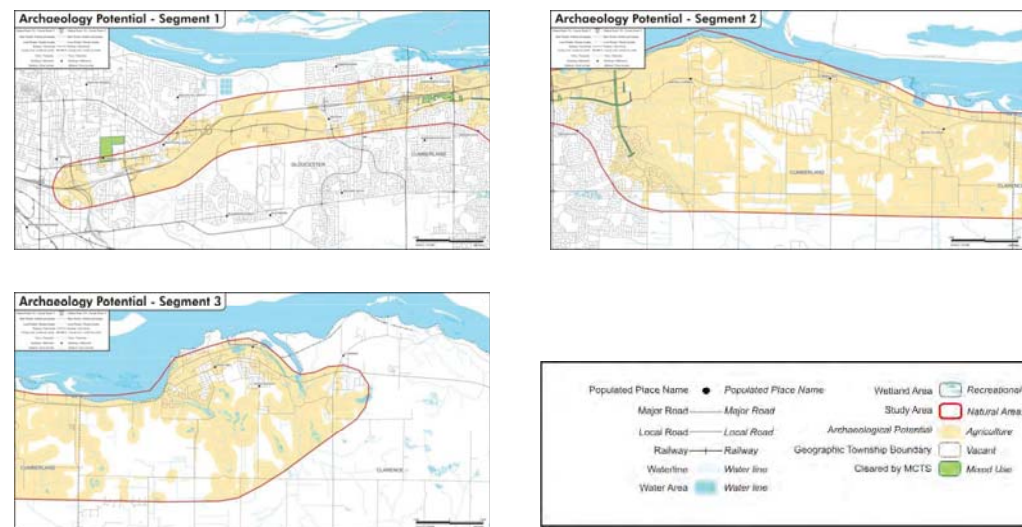
Natural Environment - Aquatic



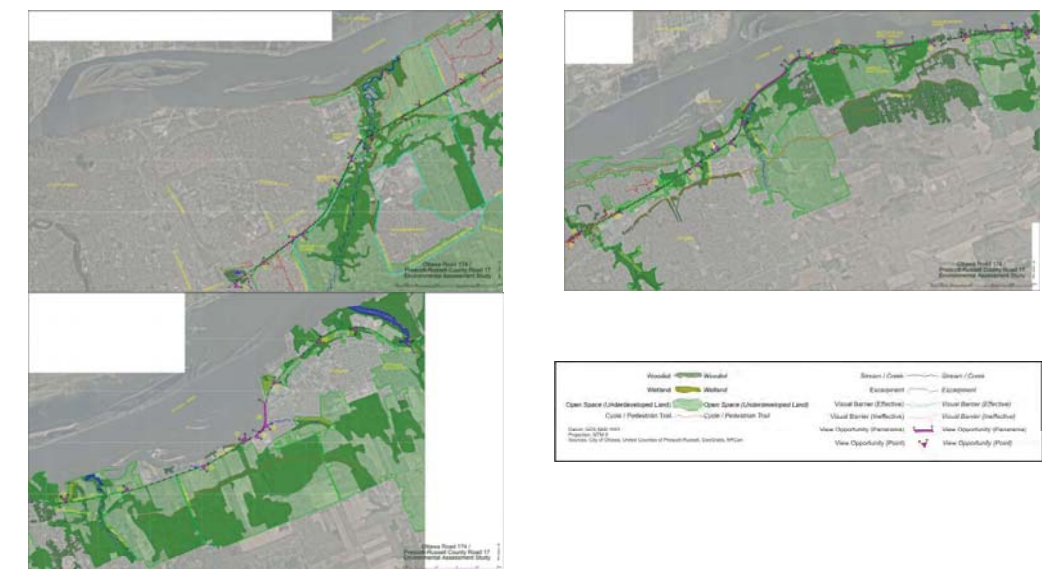
Social Environment – Land Use



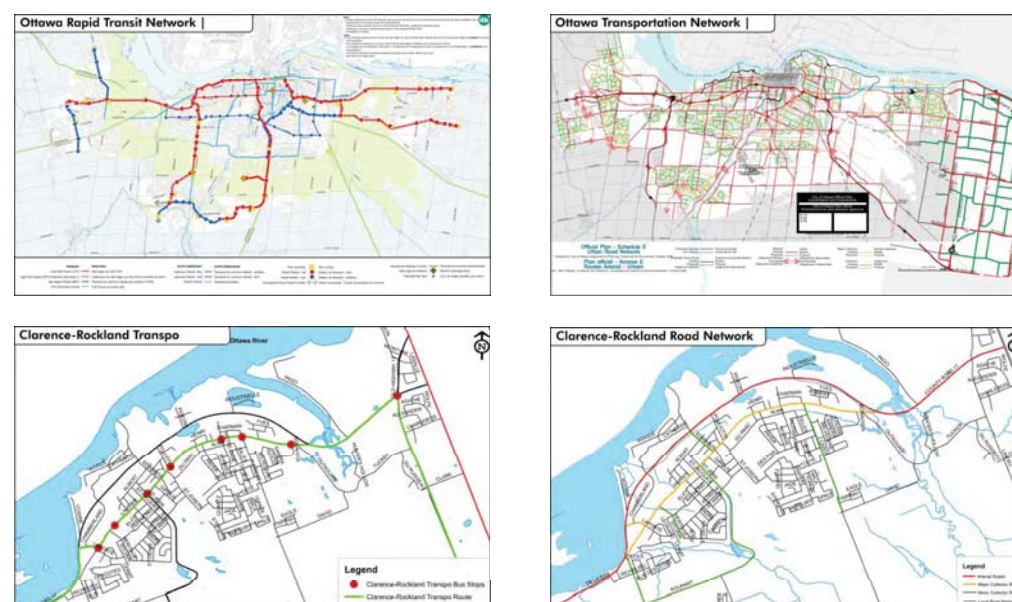
Social Environment – Archaeology



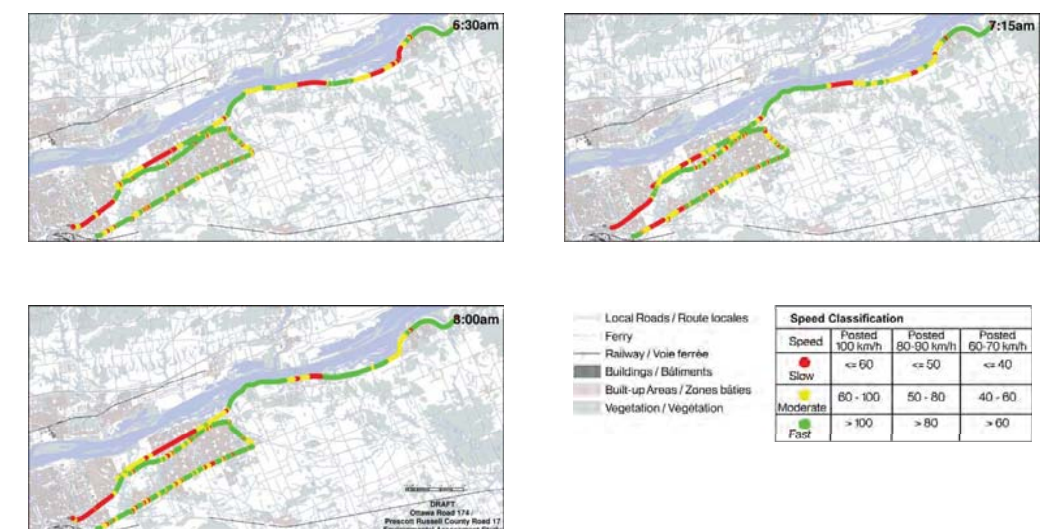
Social Environment – Views and Vistas



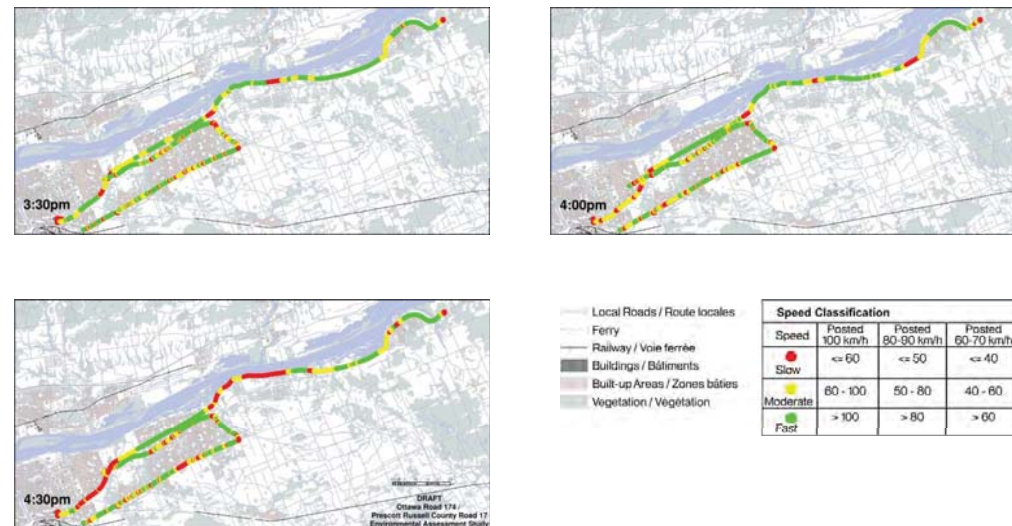
Transportation Environment – Networks



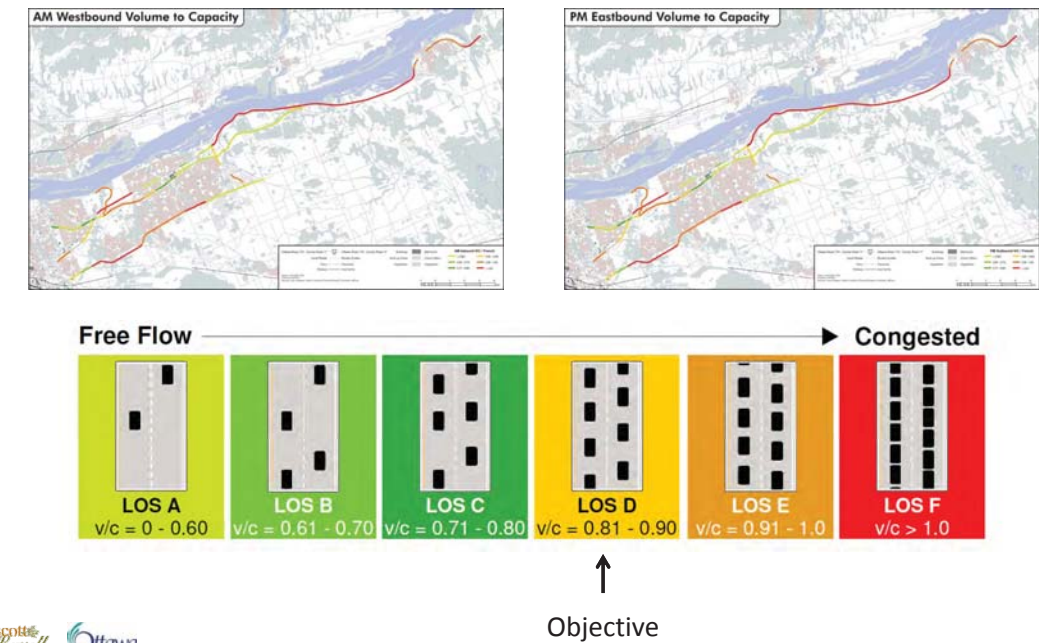
Transportation Environment – AM Speeds



Transportation Environment – PM Speeds



Transportation Environment – Current Capacity



Business Condition Assessment

- The most prominent business type along the OR 174-CR 17 corridor is retail, followed by food, fuel and accommodation outlets
- There is a higher concentration of businesses in Gloucester and the City of Clarence Rockland than in Orléans and Cumberland
- Most prevalent business type:
 - Gloucester - Offices or Professional Services
 - Clarence Rockland – retail
- Only a small proportion of businesses are primarily served by passing traffic

Alternative Solutions

Alternative Solutions

- Do Nothing
- Transportation Demand Management (TDM)
- Transportation Systems Management (TSM)
- Additional road capacity
 - Widen existing roads
 - New roads
- Combination of alternatives
- These alternatives may be applicable to different portions of the corridor or for the entire corridor.

TDM Measures

- Transportation Demand Management (TDM) is the application of strategies and policies to reduce travel demand and / or to redistribute this demand over time. Examples include:
 - priority to public transit
 - make walking and cycling more attractive
 - telework
 - public education

Screening

- Screening criteria were developed to assess the merits of the long list of alternative solutions. The screening criteria are:
 - **The ability of the alternative to address the project need.** Alternative solutions must provide transportation capacity and improve safety.
 - **Adherence to policies, regulations, and local standards of practice.** Alternatives should not contravene provincial/federal policies or municipal regulations or policies.
 - **Consideration of environmental impacts.** Alternatives were reviewed to determine their high level impacts on the various environmental conditions. The ability to avoid/reduce/minimize impacts was considered.

TDM Screening

- Measures on their own do not address need
- TDM measures included as part of overall solution

TSM Measures

- Transportation System Management (TSM) is an approach aimed at optimizing the safety, efficiency and capacity of the existing transportation system through the use of effective, low cost improvements. Examples include:
 - passing lanes, reversible lanes
 - traffic signs, signals, pavement markings and regulations
 - signal optimization
 - video monitoring
 - traveller information systems
 - transit vehicle tracking

Screening

Solution	Result
Do Nothing	Does not address the need
TDM	Does not address the need
TSM	Does not address the need
Additional road capacity	May address the need
Combination of alternatives	May address the need

TDM and TSM cannot address the need on their own. They will be included as a part of the overall solution.

TSM Screening

- Passing lane don't address capacity needs. Not carried forward.
- Reversible lanes applicable to lower speed urban locations. Not appropriate for rural areas. Speeds and driveways create unsafe conditions for motorists and residents. Not carried forward
- Other TSM measures included as part of overall solution

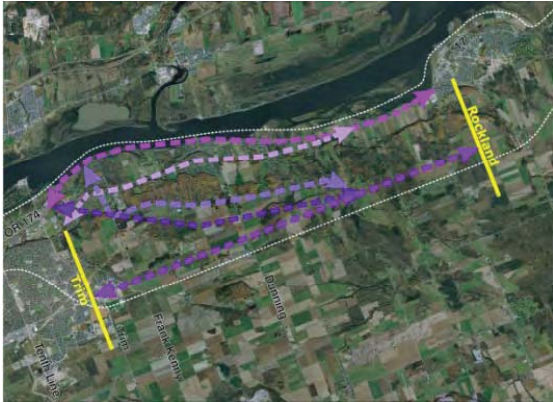
Screening – Split to Trim

Widen existing 174	Widen Innes Road	Widen St. Joseph Blvd	Build New Route
Addresses the need with expected limited environmental impacts when compared with other alternatives. Carry forward.	Widening of Innes Road west of the Greenbelt from 6 to 8 lanes is not operationally feasible. Screen out.	Widening of St. Joseph Blvd provides less capacity than freeway widening and will have significant impacts within the Orleans community. Screen out.	New routes are not in keeping with the policies of the NCC and the Greenbelt Master Plan. Screen out.
✓	✗	✗	✗



Screening - Trim to Rockland

Widen existing 174/17	Widen/new route Innes-Baseline Road	Widen Old Montreal Road from Trim to 174 East	Widen Willhaven Road and Frank Kenny	New route south of Willhaven to Baseline Road
Address the need. Carry forward.	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.	Screen out due to adjacent land use and driveways, planned development and geometry	Screen out due to poor horizontal alignment, lack of continuity and adjacent land use	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.
✓	✓	✗	✗	✓



Screening - Rockland

Improve existing 17	Widen existing urban streets within Rockland	Build new route south of Rockland
Address the needs with some environmental impacts associated with wetlands, adjacent land use and water. Carry forward.	No other roads suitable for widening due to adjacent land use/ development. Screen out. (Baseline Road considered in Segment 2)	Address the need providing an alternative route for ongoing development on the south side of Rockland. Carry forward.
✓	✗	✓



Alternatives Carried Forward

Split to Trim
Widen existing 174 to 6 lanes
Trim to Rockland
Widen existing 174/17 to 4 lanes
Widen/new route Innes-Baseline
Build new route south of Willhaven
Rockland vicinity
Improve existing 17
Build new route south of Rockland

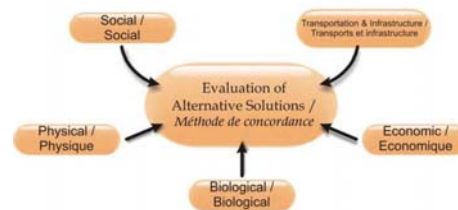
- We will refine the short list of alternatives and undertake a more detailed evaluation of the impacts and benefits of the alternative solutions carried forward to arrive at a preferred alternative solution

Next Steps

Evaluation of Alternative Solutions Carried Forward

- The methodology of evaluating alternative solutions will address the following objectives.

- Use of Criteria
- Weighting or Ranking
- Sensitivity Analysis



Consultation

- Two (2) additional meetings with PCG, BCG and ACG
- Public Open Houses, presentations, comment sheets-questionnaires and other written submissions
- Newspaper notices and contact letters to agencies, stakeholders, etc.
- Individual meetings
- First Nations consultation
- Website and Media coverage
 - www.ottawa.ca/hwy174and17study
 - <http://www.prescott-russell.on.ca/en/public-works/major-projects>
- Newsletters
- Presentations to Committees/Council of UCPR and Ottawa
- ESR for formal public review

Criteria

- Evaluation criteria are specific factors or considerations which are used to compare alternatives. Factors that need to be considered in a development of criteria include:
 - the nature of the project
 - the expectations of stakeholders
 - An understanding of the existing conditions
 - the delineation of boundaries
 - the issues related to the proposed project
- The Study Team will develop the criteria to be used in this assessment with the stakeholders.
- Let us know what you think is important and should be considered in the selection of an alternative

Criteria Group Groupes de critères	Examples of Criteria Exemples de critères
Biological Conditions Milieu biologique	<ul style="list-style-type: none"> Wildlife Fisheries Watercourses Woodlots Vegetation
Social Conditions Milieu social	<ul style="list-style-type: none"> Community Linkages Archaeology Economic Activity Noise Recreation Land requirements/use
Physical Conditions Milieu physique	<ul style="list-style-type: none"> Soils Bedrock Groundwater Drainage
Transportation & Infrastructure Transports et infrastructure	<ul style="list-style-type: none"> Transit Ridership Network/System Integration Level of Service Geometry Capacity Operation
Economic Economique	<ul style="list-style-type: none"> Capital Cost Operational and Maintenance Costs Business Impacts

Public Open Houses

- Tuesday February 5, Cumberland Lions Club
- Wednesday February 6, Orléans Sir Wilfred Laurier Secondary School
- Thursday February 7, Rockland City Hall, Council Boardroom



Questions?

Annex D-2

- Invitation letter to Business Consultation Group Meeting #1
- Presentation for Business Consultation Group Meeting #1

La version française qui suit

**Invitation to the first Business Consultation Group (BCG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

We invite you to the first BCG meeting to be held on:

Wednesday, January 30, 2013
2:00 p.m. to 4:00 p.m.
Cumberland Lions Hall, Maple Hall
2552 Old Montreal Road
Cumberland

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013
6:30 p.m. to 9:00 p.m.
Cumberland Lions Club
Maple Hall
2552 Old Montreal Road
Cumberland

Wednesday, February 6, 2013
6:30 p.m. to 9:00 p.m.
Sir Wilfrid Laurier Secondary School,
Cafeteria
1515 Tenth Line Road
Orléans

Thursday, February 7, 2013
6:30 p.m. to 9:00 p.m.
City Hall
Council boardroom
1560 Laurier Street
Rockland

The purpose of the first BCG meeting (and POH #1) is to:

- Introduce the project and the role of the BCG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at:
www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email:
Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

**Invitation à la première réunion du Groupe de consultation des gens d'affaires (GCGA)
Étude d'évaluation environnementale de portée générale – Route 174 d'Ottawa et route 17 du
comté de Prescott-Russell**

Nous vous invitons à la première réunion du GCGA, qui aura lieu le :

Mercredi 30 janvier 2013
De 14 h à 16 h
Club Lions de Cumberland, salle Maple
2552, chemin Old Montreal
Cumberland

Si vous n'êtes pas en mesure d'assister à cette réunion, vous souhaiterez peut-être vous joindre à nous pour la première série de séances portes ouvertes, qui se dérouleront aux dates et endroits suivants :

Mardi 5 février 2013
De 18 h 30 à 21 h
Club Lions de Cumberland
Salle Maple
2552, chemin Old Montreal
Cumberland

Mercredi 6 février 2013
De 18 h 30 à 21 h
École secondaire Sir Wilfrid Laurier,
Cafétéria
1515, chemin Tenth Line
Orléans

Jeudi 7 février 2013
De 18 h 30 à 21 h
Hôtel de ville
Salle du Conseil
1560, rue Laurier
Rockland

Les objectifs de la première réunion du GCGA et de la première série de séances portes ouvertes sont les suivants :

- Présenter le projet et le rôle du GCGA;
- Constater la nécessité du projet et les conditions actuelles;
- Prendre connaissance des critères d'évaluation proposés;
- Déterminer les solutions de rechange;
- Examiner l'évaluation des solutions de rechange.

Vous trouverez des renseignements au sujet de l'étude, y compris les avis, sur le site Web de la Ville d'Ottawa, au www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure ou au www.ottawa.ca/etuderoute174et17.

Veuillez donner confirmation de votre participation à Danielle Chartrand par téléphone, au 613-820-8282, poste 238, ou par courriel, à l'adresse Danielle.chartrand@aecom.com. Pour obtenir de plus amples renseignements sur le projet, veuillez communiquer avec moi ou avec :

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
Tél. : 613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Marc R. Clermont, ing.
Directeur des Travaux publics
Comtés unis de Prescott et Russell
Tél. : 613-675-4661, poste 3100
Courriel : MClermont@prescott-russell.on.ca

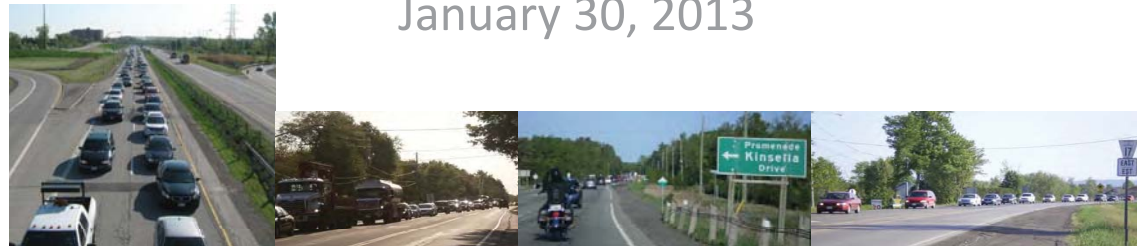
Recevez nos salutations distinguées.
AECOM Canada Ltd.

Valerie McGirr, ing.
Chargée de projet de firme de conseils
Courriel : Valerie.mcgirr@aecom.com
Tél. : 613-820-8282, poste 243

AECOM
1150, promenade Morrison, pièce 302, Ottawa ON K2H 8S9
Tél. : 613-820-8282; téléc. : 613-820-8338
www.aecom.com

Consultation Group Meeting

January 30, 2013



Introductions



Agenda

- Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps



Co-Proponents

- Prescott Russell
 - Marc Clermont
 - Dave McAvoy/Phil Pawliuk
- City of Ottawa
 - Angela Taylor



Study Team

- AECOM
 - Valerie McGirr
 - Vanessa Skelton
- Delcan
 - Kelly Roberts
- Study Team
 - Houle Chevrier, LRL, Golder

Role and Responsibilities

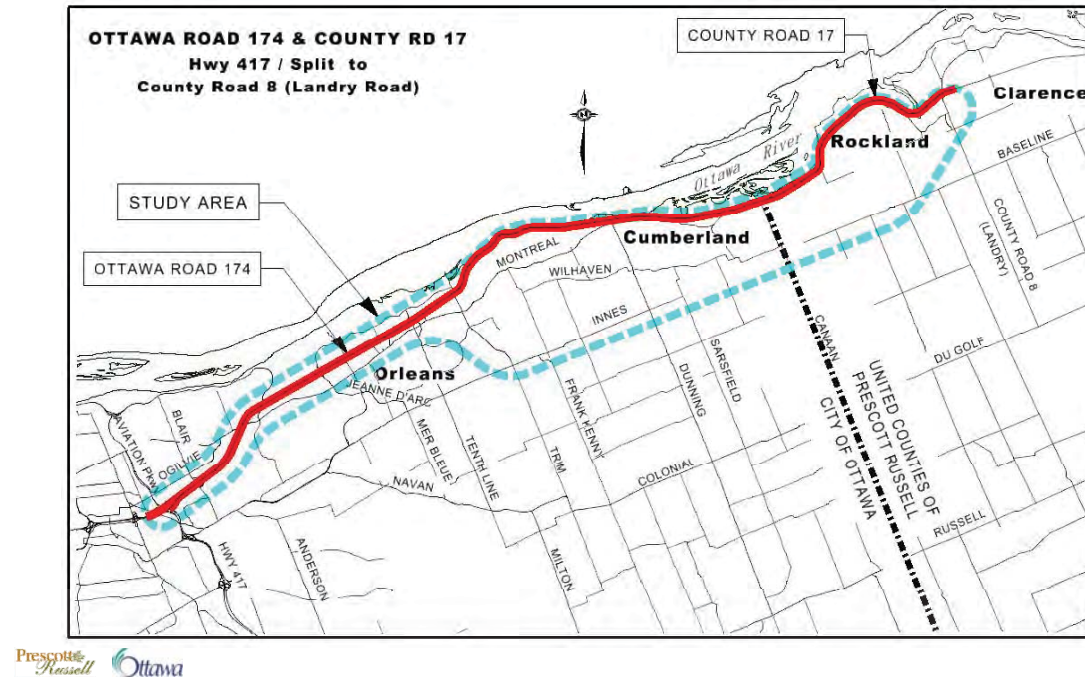
- Role is to:
 - provide input to the study
 - advise and comment on local issues and concerns
- The consultation group is not an approving body
- Three groups are being consulted
 - Agency Consultation Group (ACG)
 - Public Consultation Group (PCG)
 - Business Consultation Group (BCG)
- Businesses, institutions and developers with interest in the area (BCG); community associations, resident groups, City Advisory committees, advocacy groups (PCG)
- Three (3) meetings of each group in total

Consultation Group

- Round Table Introductions

Project Overview

Study Area



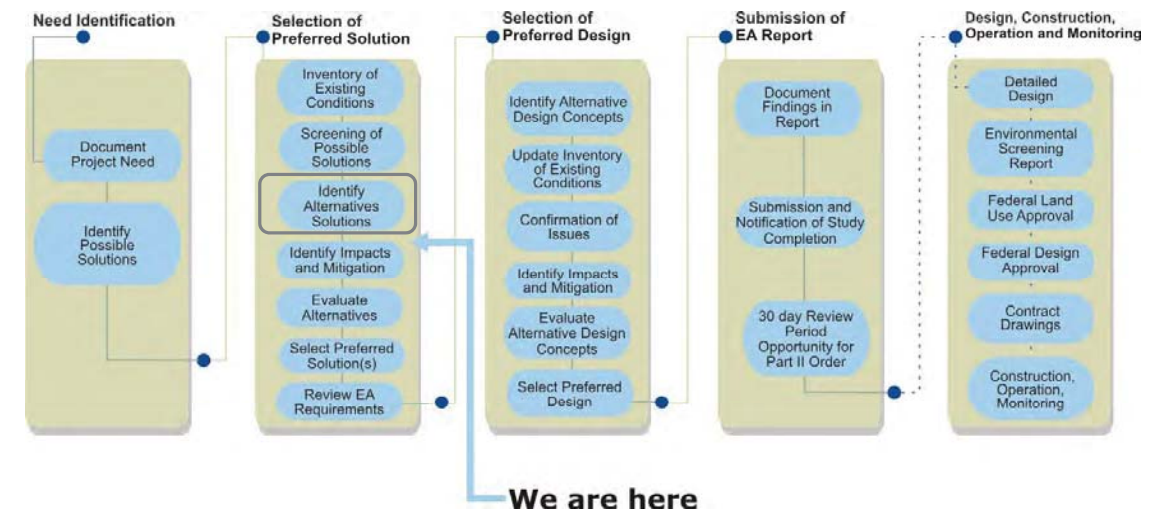
Study Area



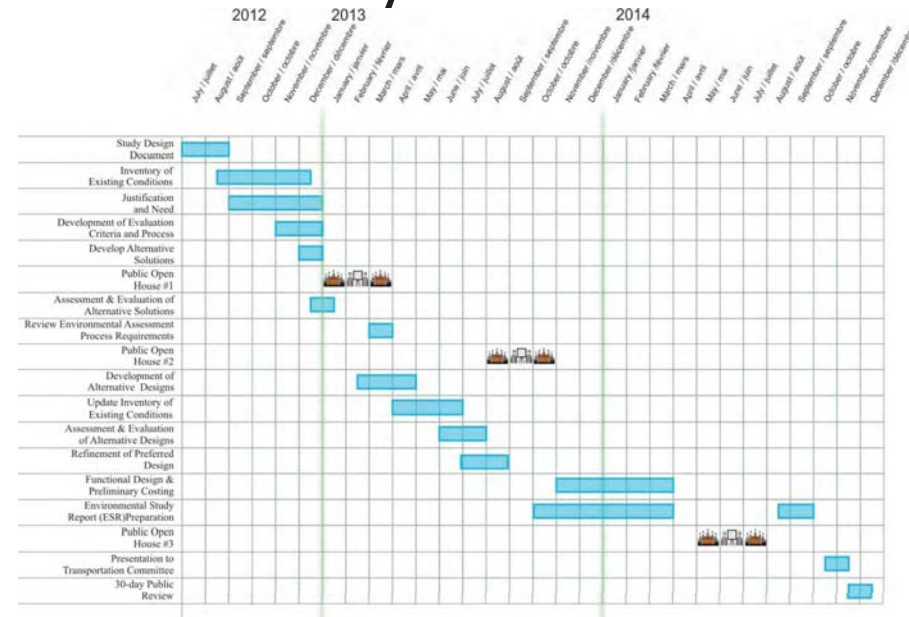
Background

- Provincial Highway 17 until 1997
- Congestion and safety problems documented for years
- Ottawa River and Mer Bleue Bog limit potential east-west routes
- OR 174-CR 17 only high capacity arterial in area
- Widening of OR 174 to 6 lanes to Jeanne d'Arc in TMP
 - Phase 2: 2016 to 2022
- Growth continuing. Development in Orléans and Rockland
- UCPR and City jointly initiated Municipal Class EA Study – Schedule C

Study Process



Study Schedule

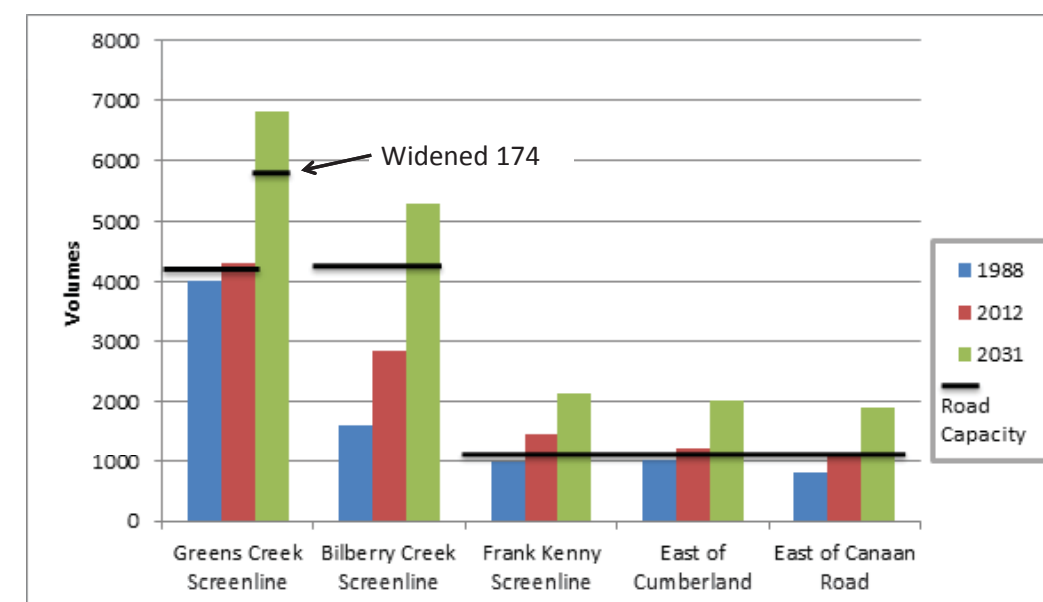


Needs Assessment

Overview

- Transportation problems in the area have been identified for many years. The Needs Assessment for this study has reviewed previous reports and utilized new information to assess:
 - existing and future transportation capacity
 - safety improvement opportunities
 - impacts of new developments and expanded urban areas

Volumes and Capacity Over Time

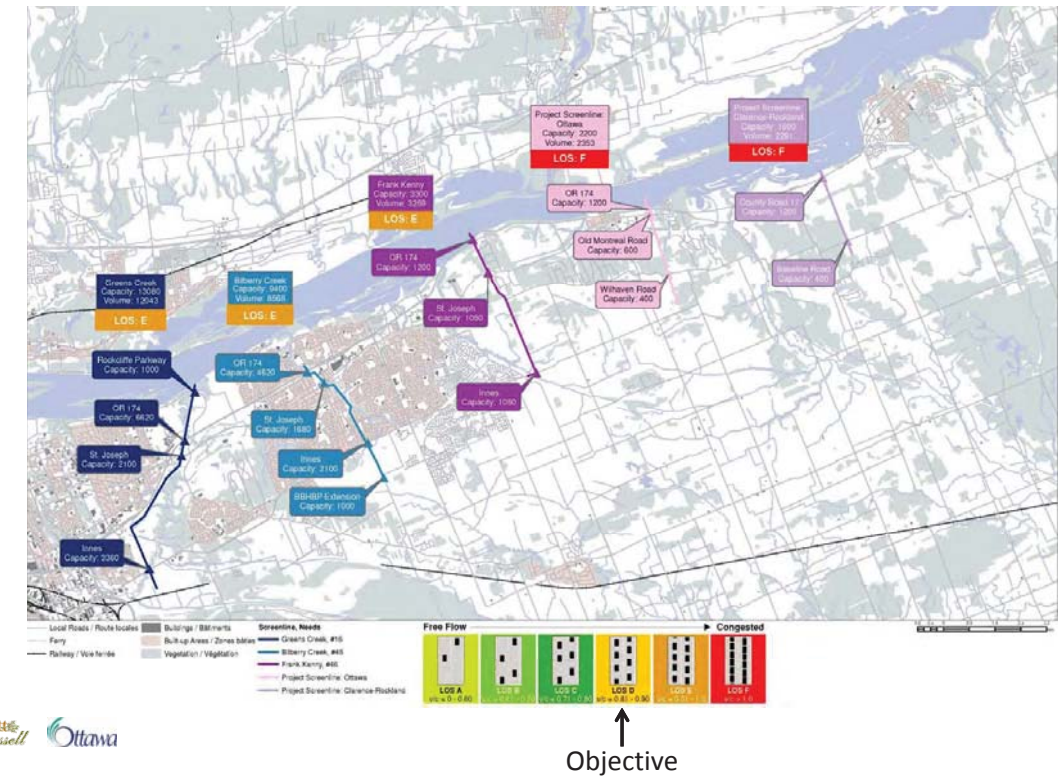


Capacity

- 2031 AM peak volumes examined using screenlines
 - Assuming all road and transit infrastructure planned for 2031 in place
- Considered downstream capacity constraints
 - Highway 417
- Tested for sensitivity for transit use
 - 5 and 10 percentile increase and decrease
- Even with increased transit use and other travel-reducing strategies, will need additional capacity from the split to Rockland

Safety

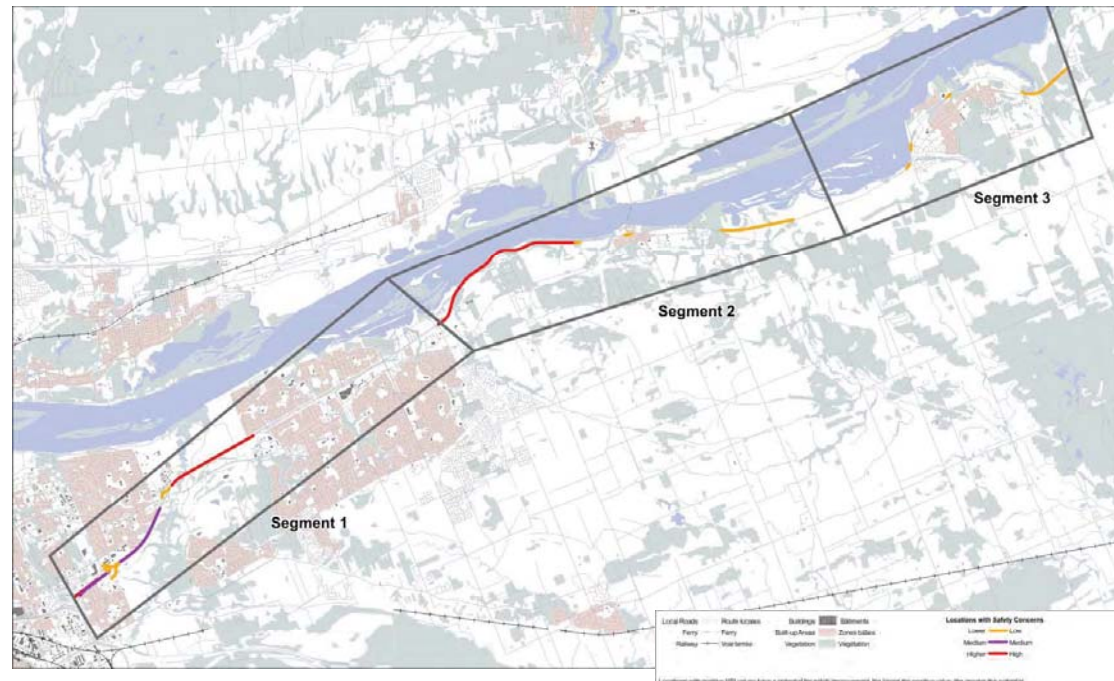
- Considered recent collision data
 - Split to Trim – 1487 collisions
 - Trim to Canaan – 465 collisions
 - Canaan to Landry – 287 collisions
- Examined:
 - Collision severity (property damage only, injury and fatal)
 - Collision impact type (turning, angle, rear end, side swipe, single motor vehicle)
 - Lighting condition (daylight, dark, dawn, dusk)
 - Weather (clear, rain, snow)
 - Time of day (AM/PM peak, mid day, evening/early morning)
- Considered road characteristics (horizontal and vertical curves, lane width, passing opportunities, clear zone)



Safety

continued

- Significant contributors to 174-17 collision experience:
 - Speed, roadside character, access, interchange and intersection geometry, lighting, positive guidance, passing opportunities, accommodating transit
- Best potential to improve safety
 - Address congestion
 - Driver awareness
 - Intersection modifications
 - Improve lighting conditions



Existing Conditions

Conclusions

- There is a need to provide additional transportation capacity
- There are opportunities for safety improvements in the OR174/CR17 corridor

Overview

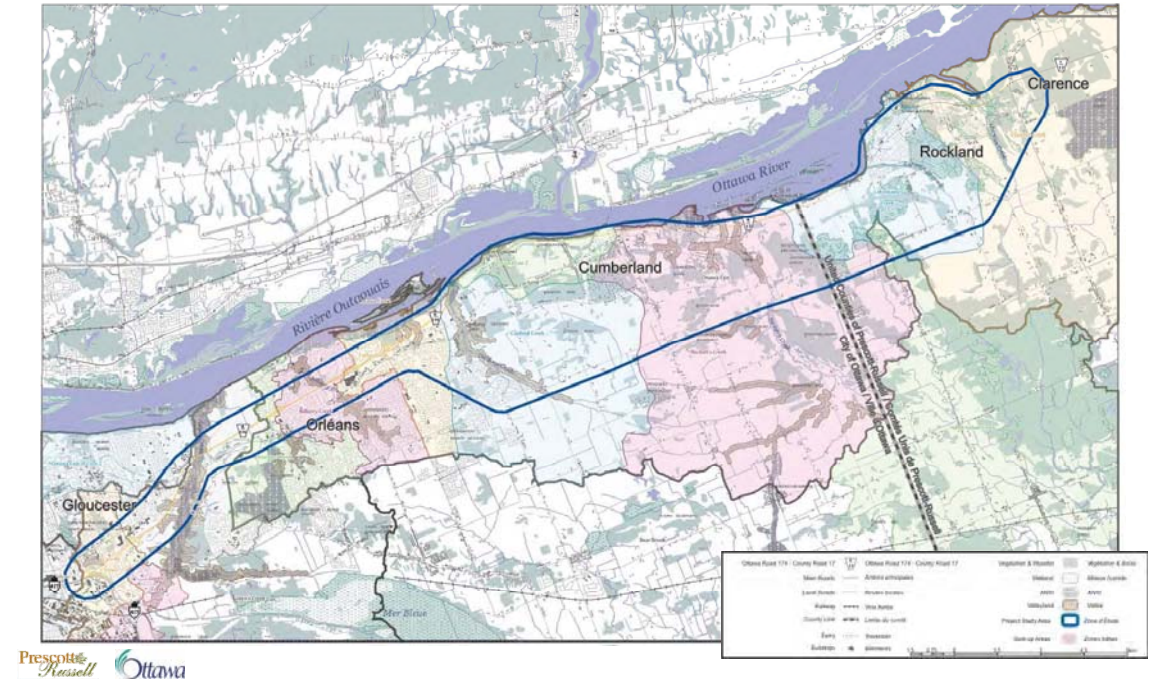
- Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by a transportation facility. The information will be used to:
 - develop reasonable alternatives
 - identify impacts
 - evaluate alternatives
- Information will be updated, as necessary, as the study progresses and more detailed information is required

Segments

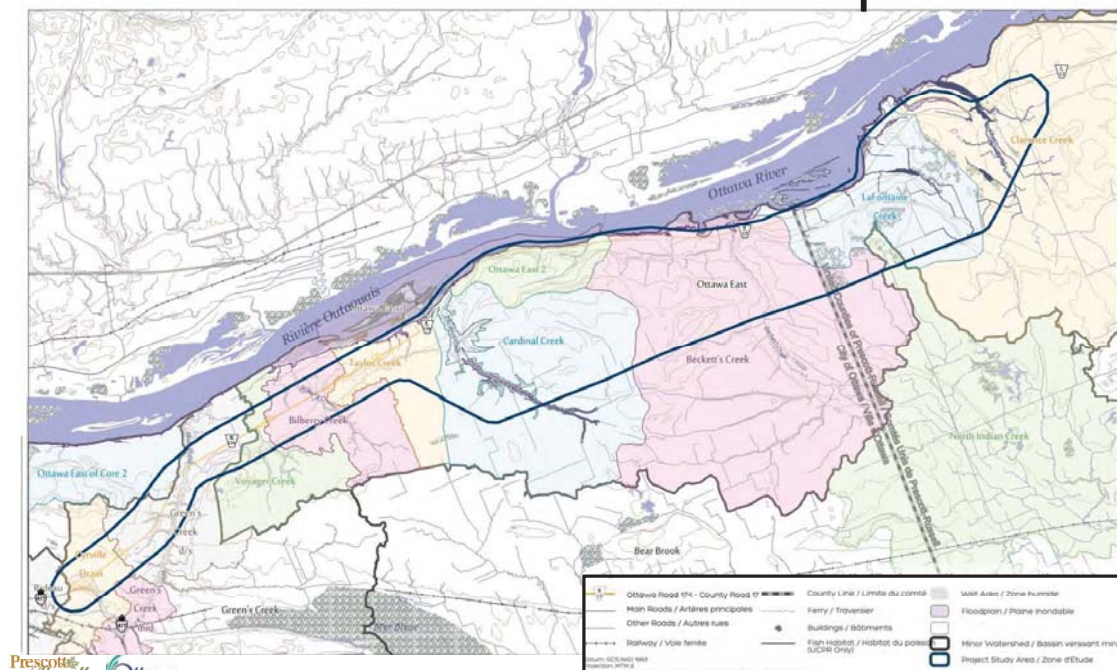
- For illustration and context purposes, the study area has been arranged in three (3) segments
- Study area boundaries may also change to reflect the conditions being considered



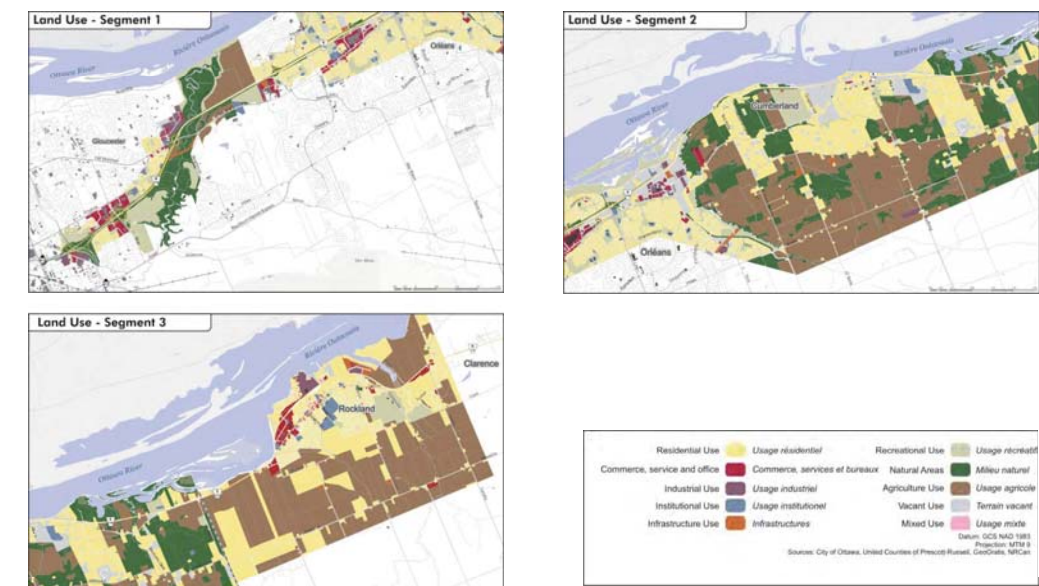
Natural Environment - Terrestrial



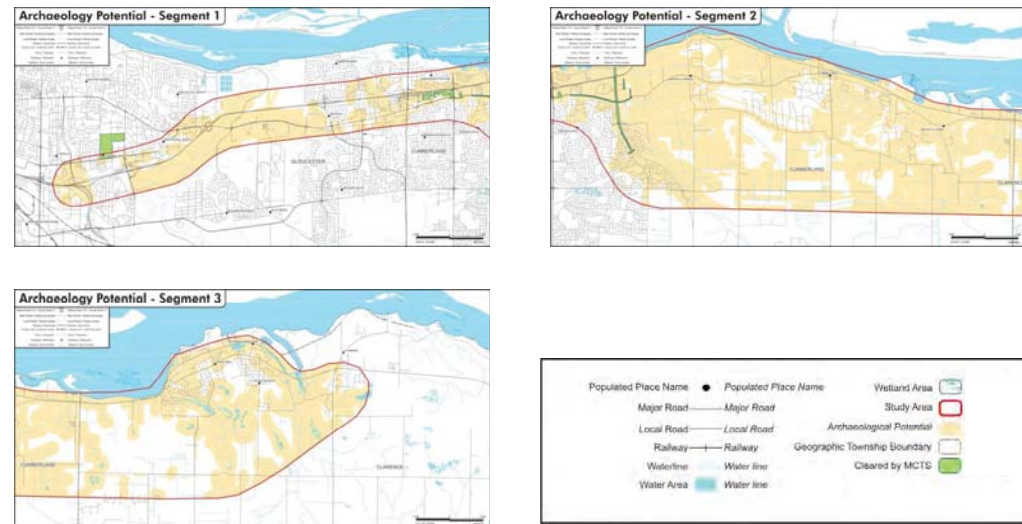
Natural Environment - Aquatic



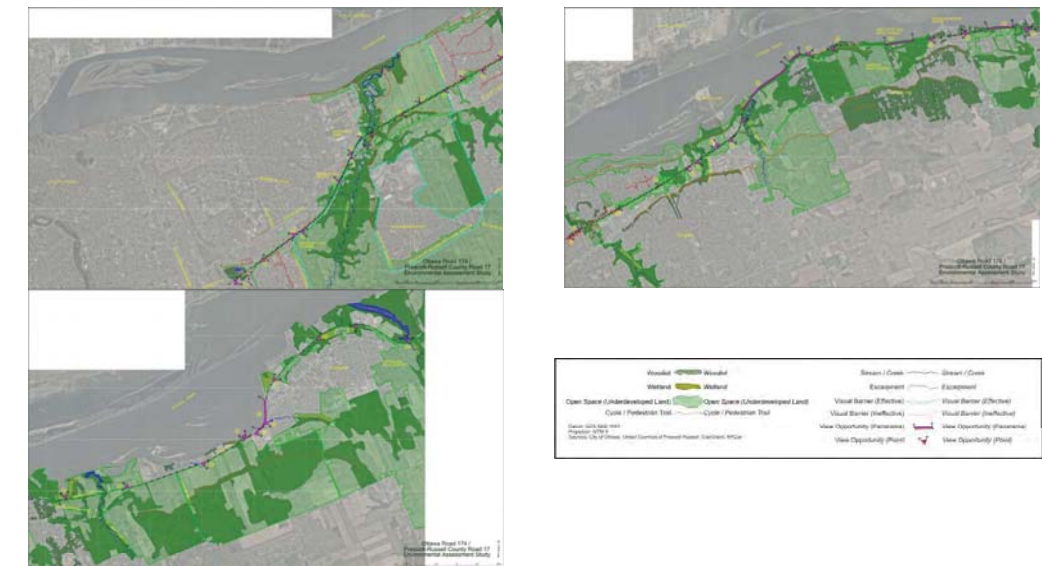
Social Environment – Land Use



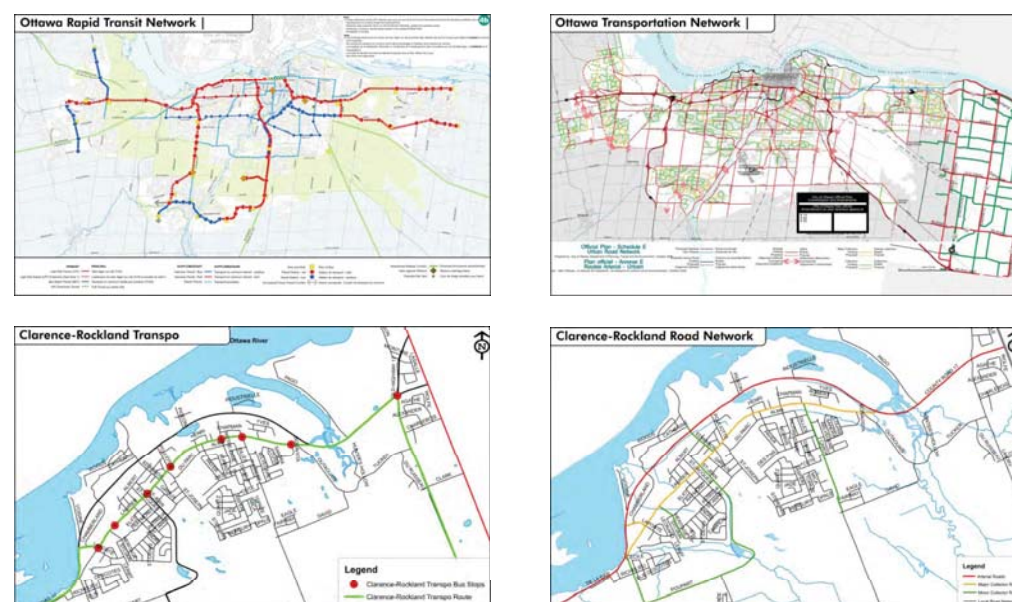
Social Environment – Archaeology



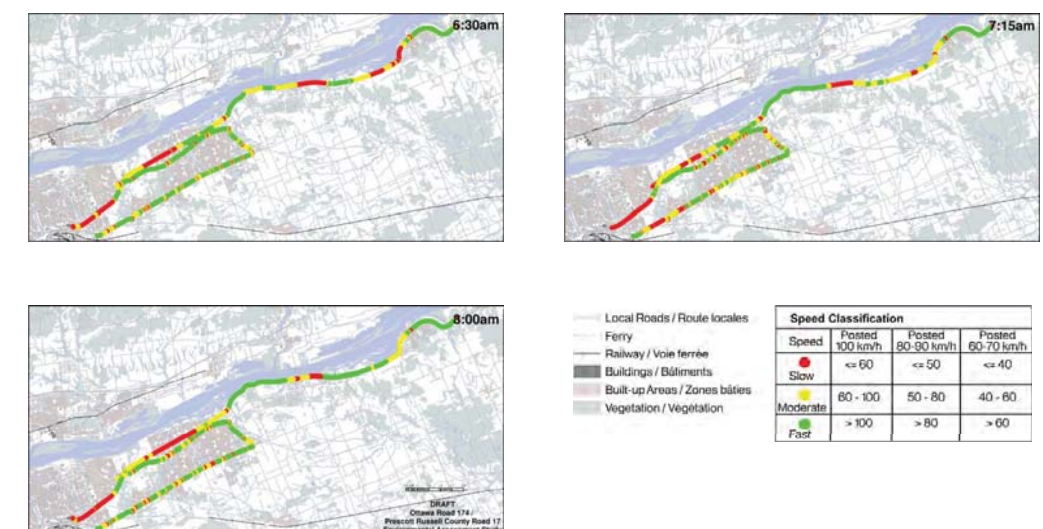
Social Environment – Views and Vistas



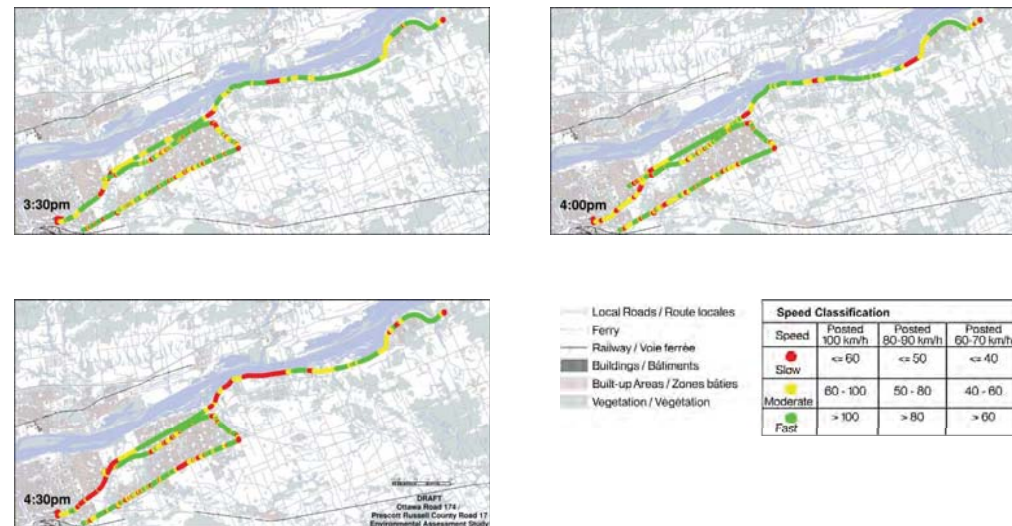
Transportation Environment – Networks



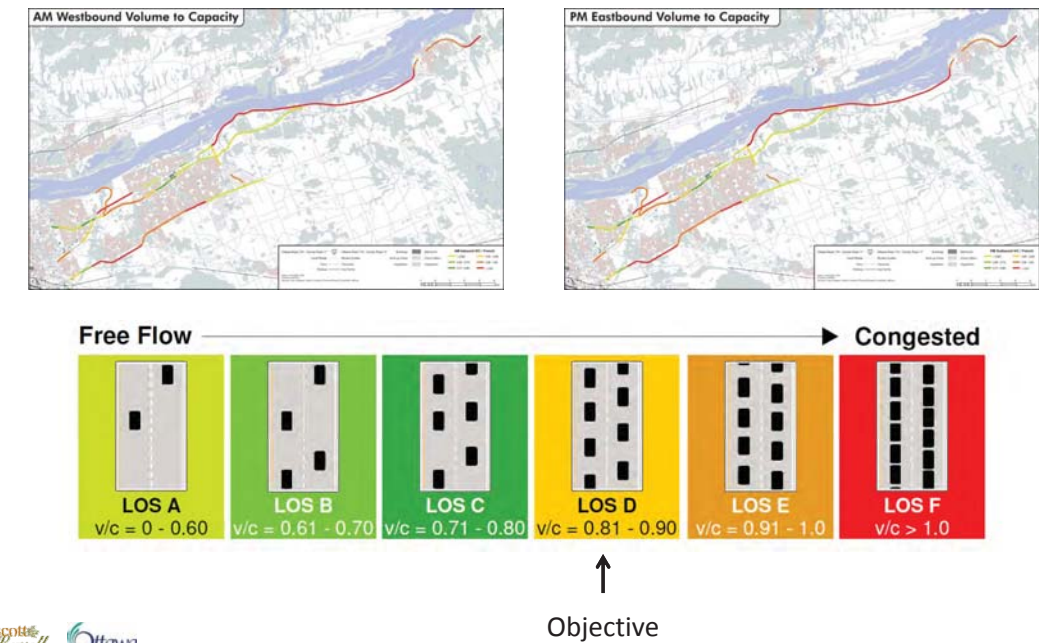
Transportation Environment – AM Speeds



Transportation Environment – PM Speeds



Transportation Environment – Current Capacity



Business Condition Assessment

- The most prominent business type along the OR 174-CR 17 corridor is retail, followed by food, fuel and accommodation outlets
- There is a higher concentration of businesses in Gloucester and the City of Clarence Rockland than in Orléans and Cumberland
- Most prevalent business type:
 - Gloucester - Offices or Professional Services
 - Clarence Rockland – retail
- Only a small proportion of businesses are primarily served by passing traffic

Alternative Solutions

Alternative Solutions

- Do Nothing
- Transportation Demand Management (TDM)
- Transportation Systems Management (TSM)
- Additional road capacity
 - Widen existing roads
 - New roads
- Combination of alternatives
- These alternatives may be applicable to different portions of the corridor or for the entire corridor.

TDM Measures

- Transportation Demand Management (TDM) is the application of strategies and policies to reduce travel demand and / or to redistribute this demand over time. Examples include:
 - priority to public transit (such as OR 174 bus on shoulder operation)
 - make walking and cycling more attractive
 - telework
 - public education

Screening

- Screening criteria were developed to assess the merits of the long list of alternative solutions. The screening criteria are:
 - **The ability of the alternative to address the project need.** Alternative solutions must provide transportation capacity and improve safety.
 - **Adherence to policies, regulations, and local standards of practice.** Alternatives should not contravene provincial/federal policies or municipal regulations or policies.
 - **Consideration of environmental impacts.** Alternatives were reviewed to determine their high level impacts on the various environmental conditions. The ability to avoid/reduce/minimize impacts was considered.

TDM Screening

- Measures on their own do not address need
- TDM measures included as part of overall solution

TSM Measures

- Transportation System Management (TSM) is an approach aimed at optimizing the safety, efficiency and capacity of the existing transportation system through the use of effective, low cost improvements. Examples include:
 - passing lanes, reversible lanes
 - traffic signs, signals, pavement markings and regulations
 - signal optimization
 - video monitoring
 - traveller information systems
 - transit vehicle tracking

Screening

Solution	Result
Do Nothing	Does not address the need
TDM	Does not address the need
TSM	Does not address the need
Additional road capacity	May address the need
Combination of alternatives	May address the need

TDM and TSM cannot address the need on their own. They will be included as a part of the overall solution.

TSM Screening

- Passing lane don't address capacity needs. Not carried forward
- Reversible lanes applicable to lower speed urban locations. Not appropriate for rural areas. Speeds and driveways create unsafe conditions for motorists and residents. Not carried forward
- Other TSM measures included as part of overall solution

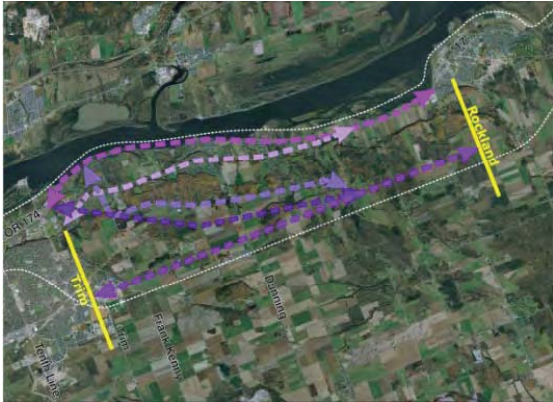
Screening – Split to Trim

Widen existing 174	Widen Innes Road	Widen St. Joseph Blvd	Build New Route
Addresses the need with expected limited environmental impacts when compared with other alternatives. Carry forward.	Widening of Innes Road west of the Greenbelt from 6 to 8 lanes is not operationally feasible; significant environmental impacts. Screen out.	Widening of St. Joseph Blvd provides less capacity than freeway widening and will have significant impacts within the Orleans community and the Greenbelt. Screen out.	New routes are not in keeping with the policies of the NCC and the Greenbelt Master Plan.; significant environmental impacts Screen out.
✓	✗	✗	✗



Screening - Trim to Rockland

Widen existing 174/17	Widen/new route Innes-Baseline Road	Widen Old Montreal Road from Trim to 174 East	Widen Willhaven Road and Frank Kenny	New route south of Willhaven to Baseline Road
Address the need. Carry forward.	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.	Screen out due to adjacent land use and driveways, planned development and geometry	Screen out due to poor horizontal alignment, lack of continuity and adjacent land use	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.
✓	✓	✗	✗	✓



Screening - Rockland

Improve existing 17	Widen existing urban streets within Rockland	Build new route south of Rockland
Address the needs with some environmental impacts associated with wetlands, adjacent land use and water. Carry forward.	No other roads suitable for widening due to adjacent land use/ development. Screen out. (Baseline Road considered in Segment 2)	Address the need providing an alternative route for ongoing development on the south side of Rockland. Carry forward.
✓	✗	✓



Alternatives Carried Forward

Split to Trim
Widen existing 174 to 6 lanes
Trim to Rockland
Widen existing 174/17 to 4 lanes
Widen/new route Innes-Baseline
Build new route south of Willhaven
Rockland vicinity
Improve existing 17
Build new route south of Rockland

- We will refine the short list of alternatives and undertake a more detailed evaluation of the impacts and benefits of the alternative solutions carried forward to arrive at a preferred alternative solution

Next Steps

Evaluation of Alternative Solutions Carried Forward

- The methodology of evaluating alternative solutions will address the following objectives.

- Use of Criteria
- Weighting or Ranking
- Sensitivity Analysis



Consultation

- Two (2) additional meetings with PCG, BCG and ACG
- Public Open Houses, presentations, comment sheets-questionnaires and other written submissions
- Newspaper notices and contact letters to agencies, stakeholders, etc.
- Individual meetings
- First Nations consultation
- Website and Media coverage
 - www.ottawa.ca/hwy174and17study
 - www.prescott-russell.on.ca/en/public-works/major-projects
- Newsletters
- Presentations to Committees/Council of UCPR and Ottawa
- ESR for formal public review

Criteria

- Evaluation criteria are specific factors or considerations which are used to compare alternatives. Factors that need to be considered in a development of criteria include:
 - the nature of the project
 - the expectations of stakeholders
 - An understanding of the existing conditions
 - the delineation of boundaries
 - the issues related to the proposed project
- The Study Team will develop the criteria to be used in this assessment with the stakeholders.
- Let us know what you think is important and should be considered in the selection of an alternative

Criteria Group Groupes de critères	Examples of Criteria Exemples de critères
Biological Conditions Milieu biologique	<ul style="list-style-type: none"> Wildlife Fisheries Watercourses Woodlots Vegetation
Social Conditions Milieu social	<ul style="list-style-type: none"> Community Linkages Archaeology Economic Activity Noise Recreation Land requirements/use
Physical Conditions Milieu physique	<ul style="list-style-type: none"> Soils Bedrock Groundwater Drainage
Transportation & Infrastructure Transports et infrastructure	<ul style="list-style-type: none"> Transit Ridership Network/System Integration Level of Service Geometry Capacity Operation
Economic Economique	<ul style="list-style-type: none"> Capital Cost Operational and Maintenance Costs Business Impacts

Public Open Houses

- Tuesday February 5, Cumberland Lions Club
- Wednesday February 6, Orléans Sir Wilfred Laurier Secondary School
- Thursday February 7, Rockland City Hall, Council Boardroom



Questions?



Annex D-3

- Invitation letter to Public Consultation Group Meeting #1
- Presentation for Public Consultation Group Meeting #1

La version française qui suit

**Invitation to the first Public Consultation Group (PCG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

We invite you to the first PCG meeting to be held on:

Wednesday, January 30, 2013

6:30 p.m. to 8:30 p.m.

Cumberland Lions Hall, Maple Hall

2552 Old Montreal Road

Cumberland

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013

6:30 p.m. to 9:00 p.m.

Cumberland Lions Club

Maple Hall

2552 Old Montreal Road

Cumberland

Wednesday, February 6, 2013

6:30 p.m. to 9:00 p.m.

Sir Wilfrid Laurier Secondary School,

Cafeteria

1515 Tenth Line Road

Orléans

Thursday, February 7, 2013

6:30 p.m. to 9:00 p.m.

City Hall

Council boardroom

1560 Laurier Street

Rockland

The purpose of the first PCG meeting (and POH #1) is to:

- Introduce the project and the role of the PCG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at:
www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email:
Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

Invitation à la première réunion du Groupe de consultation publique (GCP)
Étude d'évaluation environnementale de portée générale – Route 174 d'Ottawa et route 17 du
comté de Prescott-Russell

Nous vous invitons à la première réunion du GCP, qui aura lieu le :

Mercredi 30 janvier 2013
De 18 h 30 à 20 h 30
Club Lions de Cumberland, salle Maple
2552, chemin Old Montréal
Cumberland

Si vous n'êtes pas en mesure d'assister à cette réunion, vous souhaiterez peut-être vous joindre à nous pour la première série de séances portes ouvertes, qui se dérouleront aux dates et endroits suivants :

Mardi 5 février 2013
De 18 h 30 à 21 h
Club Lions de Cumberland
Salle Maple
2552, chemin Old Montreal
Cumberland

Mercredi 6 février 2013
De 18 h 30 à 21 h
École secondaire Sir Wilfrid Laurier,
Cafétéria
1515, chemin Tenth Line
Orléans

Jeudi 7 février 2013
De 18 h 30 à 21 h
Hôtel de ville
Salle du Conseil
1560, rue Laurier
Rockland

Les objectifs de la première réunion du GCP et de la première série de séances portes ouvertes sont les suivants :

- Présenter le projet et le rôle du GCP;
- Constater la nécessité du projet et les conditions actuelles;
- Prendre connaissance des critères d'évaluation proposés;
- Déterminer les solutions de rechange;
- Examiner l'évaluation des solutions de rechange.

Vous trouverez des renseignements au sujet de l'étude, y compris les avis, sur le site Web de la Ville d'Ottawa, au www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure ou au www.ottawa.ca/etuderoute174et17.

Veuillez donner confirmation de votre participation à Danielle Chartrand par téléphone, au 613-820-8282, poste 238, ou par courriel, à l'adresse Danielle.chartrand@aecom.com. Pour obtenir de plus amples renseignements sur le projet, veuillez communiquer avec moi ou avec :

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
Tél. : 613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Marc R. Clermont, ing.
Directeur des Travaux publics
Comtés unis de Prescott et Russell
Tél. : 613-675-4661, poste 3100
Courriel : MClermont@prescott-russell.on.ca

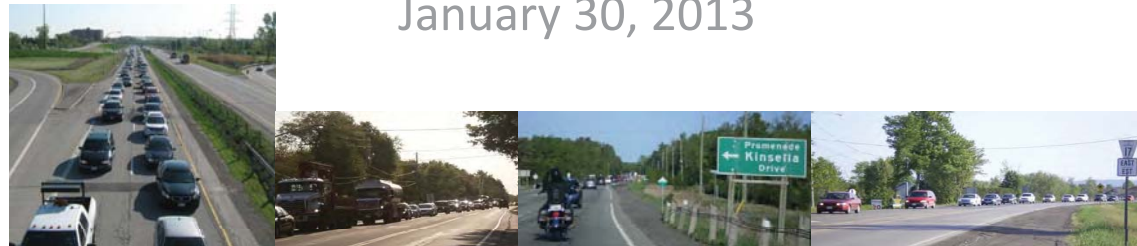
Recevez nos salutations distinguées.
AECOM Canada Ltd.

Valerie McGirr, ing.
Chargée de projet de firme de conseils
Courriel : Valerie.mcgirr@aecom.com
Tél. : 613-820-8282, poste 243

AECOM
1150, promenade Morrison, pièce 302, Ottawa (Ontario) K2H 8S9
Tél. : 613-820-8282; téléc. : 613-820-8338
www.aecom.com

Consultation Group Meeting

January 30, 2013



Introductions



Agenda

- Introductions
- Roles and Responsibilities
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps



Co-Proponents

- Prescott Russell
 - Marc Clermont
 - Dave McAvoy/Phil Pawliuk
- City of Ottawa
 - Angela Taylor



Study Team

- AECOM
 - Valerie McGirr
 - Vanessa Skelton
- Delcan
 - Kelly Roberts
- Study Team
 - Houle Chevrier, LRL, Golder

Role and Responsibilities

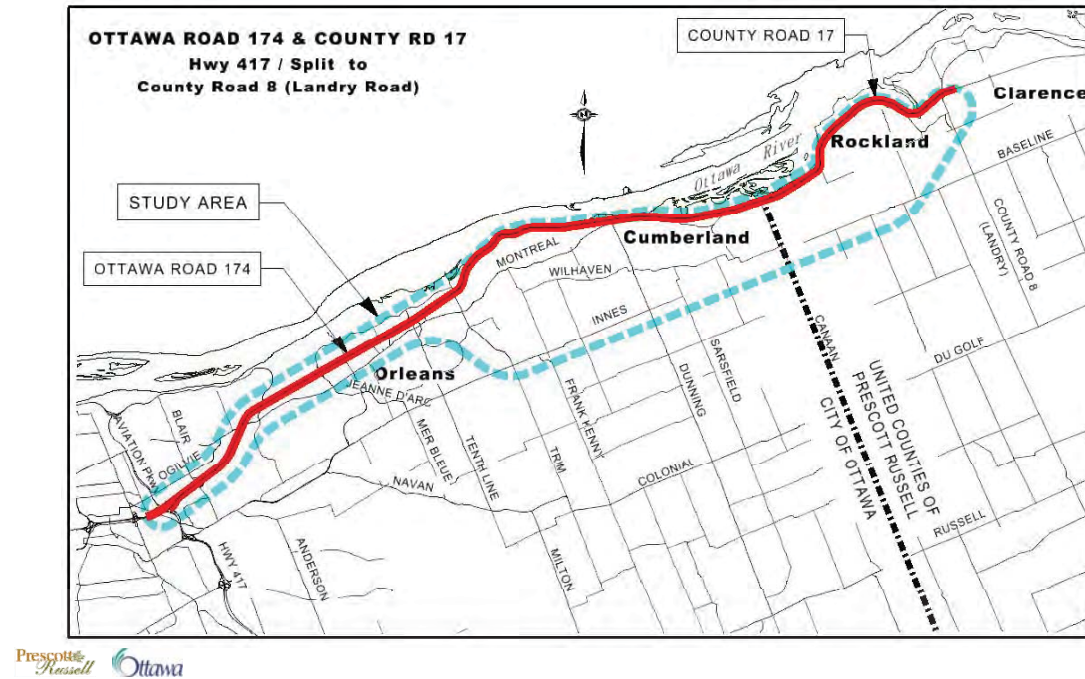
- Role is to:
 - provide input to the study
 - advise and comment on local issues and concerns
- The consultation group is not an approving body
- Three groups are being consulted
 - Agency Consultation Group (ACG)
 - Public Consultation Group (PCG)
 - Business Consultation Group (BCG)
- Businesses, institutions and developers with interest in the area (BCG); community associations, resident groups, City Advisory committees, advocacy groups (PCG)
- Three (3) meetings of each group in total

Consultation Group

- Round Table Introductions

Project Overview

Study Area



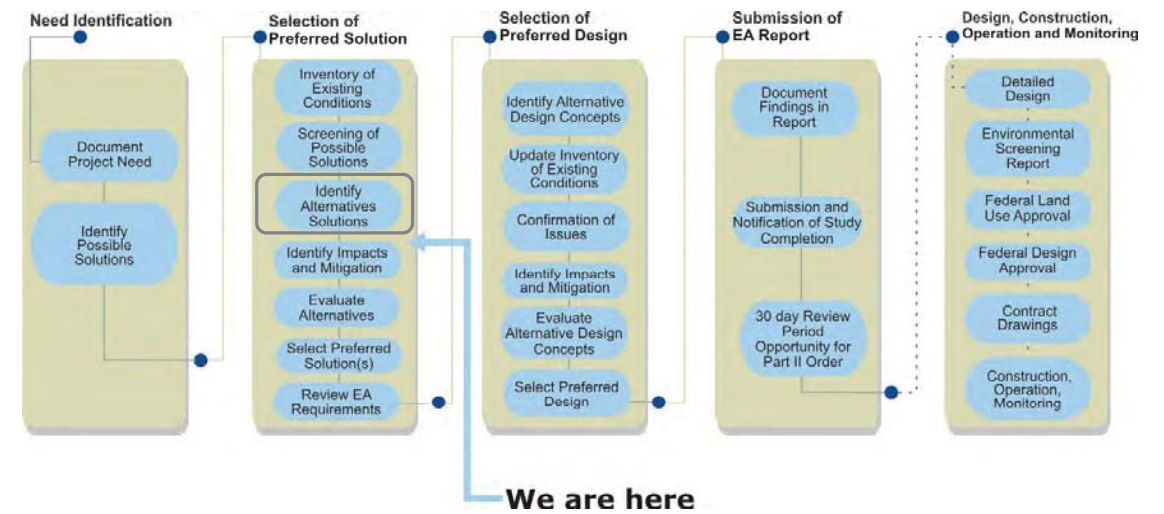
Study Area



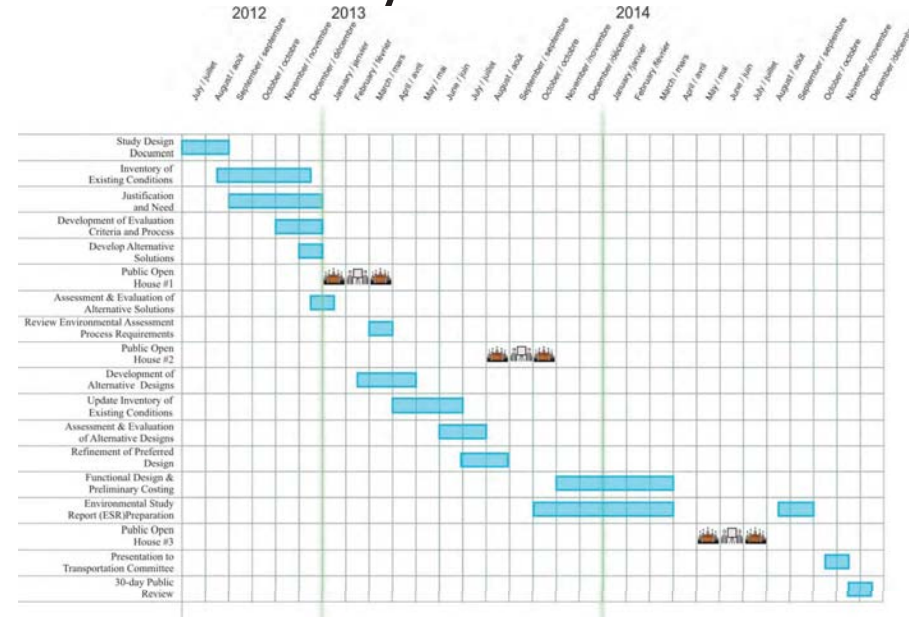
Background

- Provincial Highway 17 until 1997
- Congestion and safety problems documented for years
- Ottawa River and Mer Bleue Bog limit potential east-west routes
- OR 174-CR 17 only high capacity arterial in area
- Widening of OR 174 to 6 lanes to Jeanne d'Arc in TMP
 - Phase 2: 2016 to 2022
- Growth continuing. Development in Orléans and Rockland
- UCPR and City jointly initiated Municipal Class EA Study – Schedule C

Study Process



Study Schedule

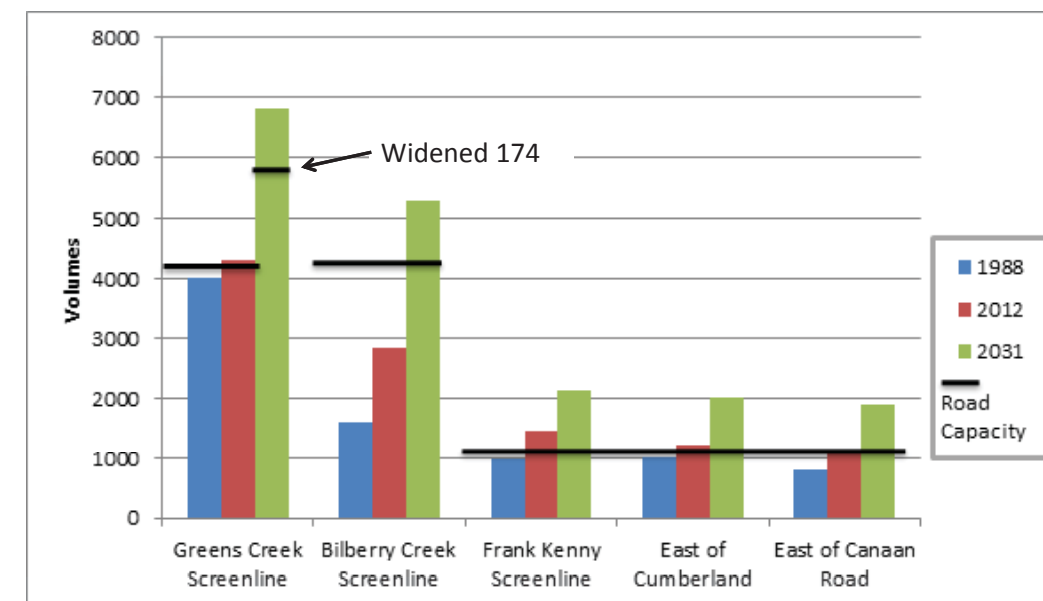


Overview

- Transportation problems in the area have been identified for many years. The Needs Assessment for this study has reviewed previous reports and utilized new information to assess:
 - existing and future transportation capacity
 - safety improvement opportunities
 - impacts of new developments and expanded urban areas

Needs Assessment

Volumes and Capacity Over Time

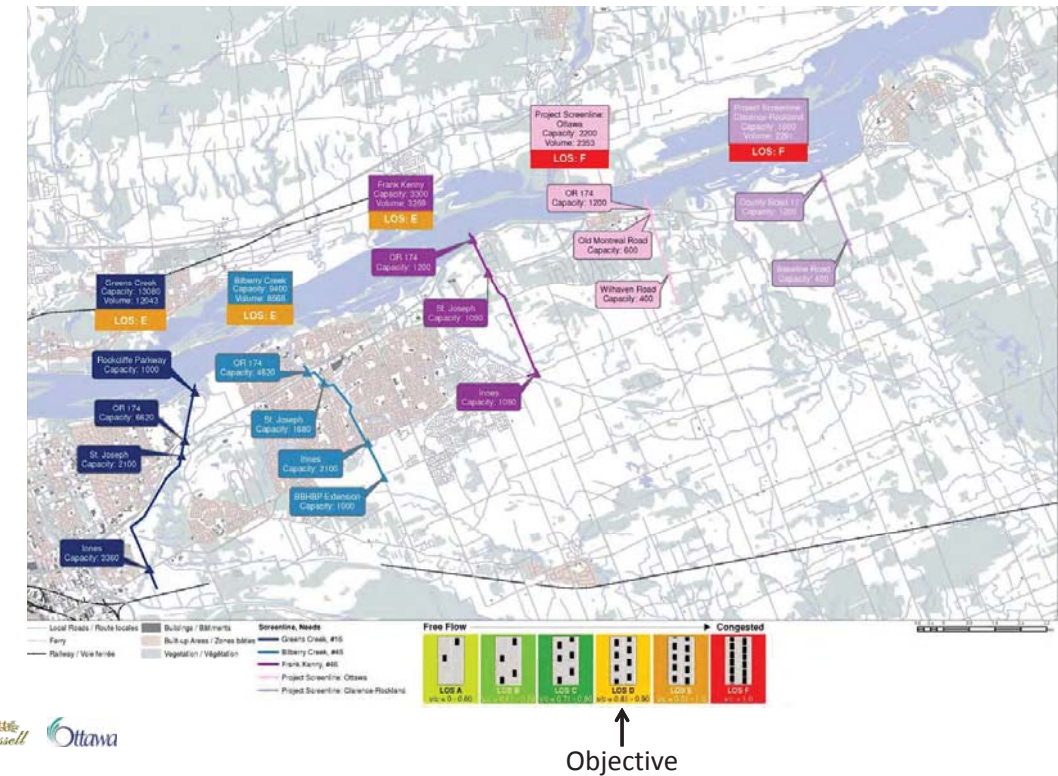


Capacity

- 2031 AM peak volumes examined using screenlines
 - Assuming all road and transit infrastructure planned for 2031 in place
- Considered downstream capacity constraints
 - Highway 417
- Tested for sensitivity for transit use
 - 5 and 10 percentile increase and decrease
- Even with increased transit use and other travel-reducing strategies, will need additional capacity from the split to Rockland

Safety

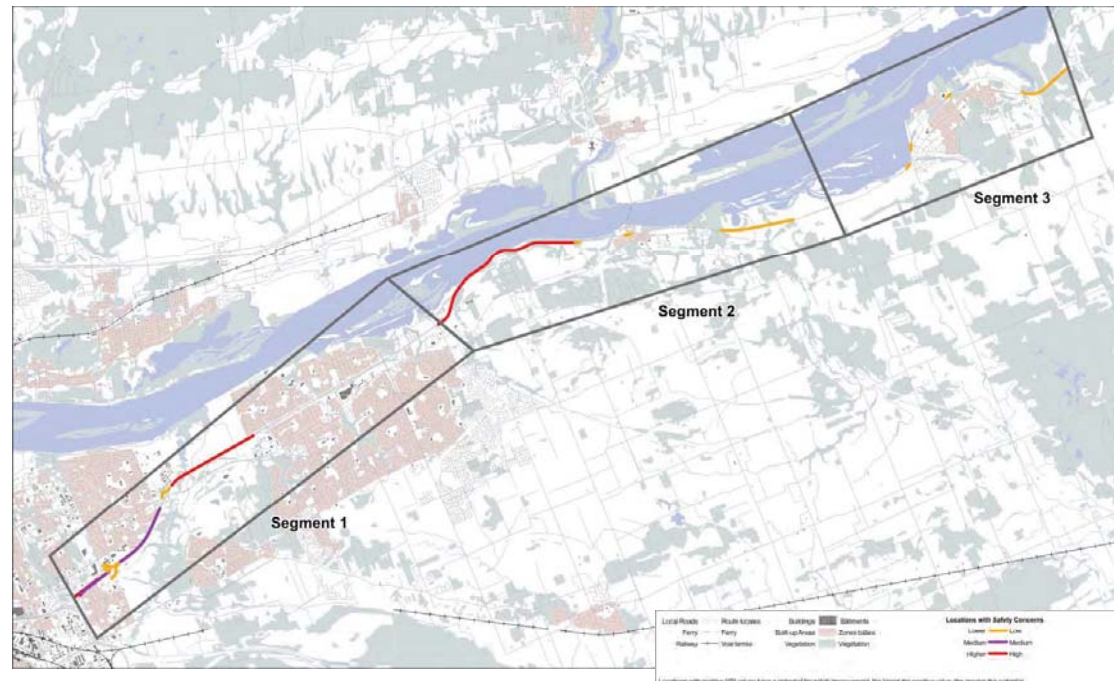
- Considered recent collision data
 - Split to Trim – 1487 collisions
 - Trim to Canaan – 465 collisions
 - Canaan to Landry – 287 collisions
- Examined:
 - Collision severity (property damage only, injury and fatal)
 - Collision impact type (turning, angle, rear end, side swipe, single motor vehicle)
 - Lighting condition (daylight, dark, dawn, dusk)
 - Weather (clear, rain, snow)
 - Time of day (AM/PM peak, mid day, evening/early morning)
- Considered road characteristics (horizontal and vertical curves, lane width, passing opportunities, clear zone)



Safety

continued

- Significant contributors to 174-17 collision experience:
 - Speed, roadside character, access, interchange and intersection geometry, lighting, positive guidance, passing opportunities, accommodating transit
- Best potential to improve safety
 - Address congestion
 - Driver awareness
 - Intersection modifications
 - Improve lighting conditions



Existing Conditions

Conclusions

- There is a need to provide additional transportation capacity
- There are opportunities for safety improvements in the OR174/CR17 corridor

Overview

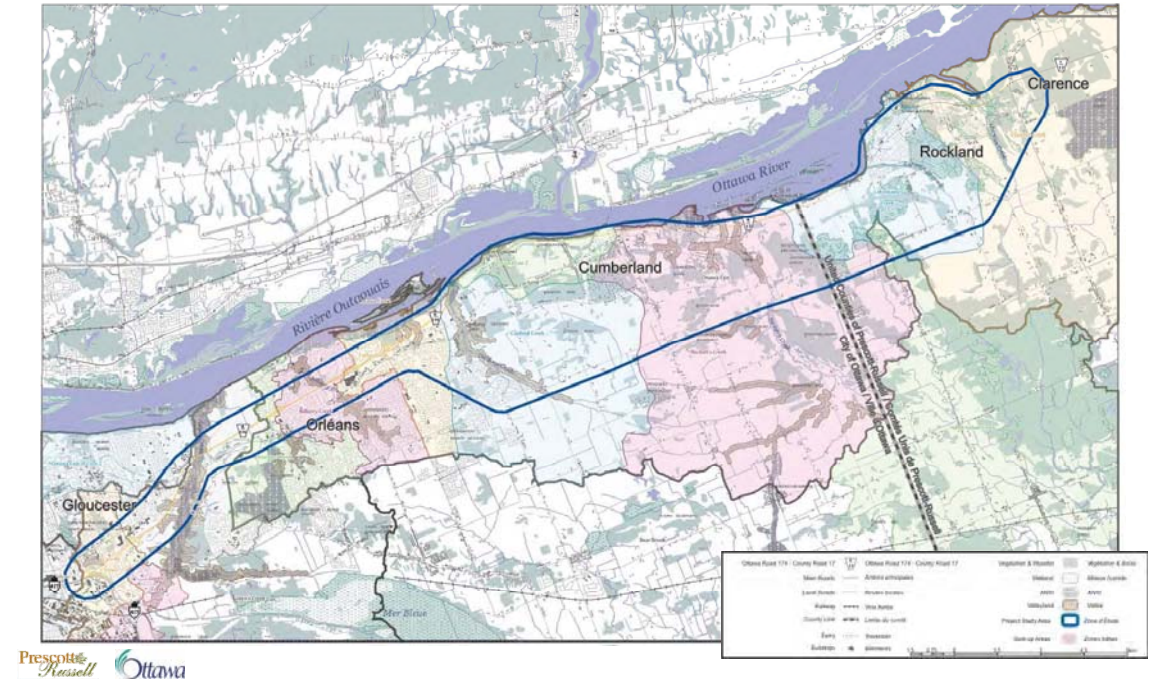
- Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by a transportation facility. The information will be used to:
 - develop reasonable alternatives
 - identify impacts
 - evaluate alternatives
- Information will be updated, as necessary, as the study progresses and more detailed information is required

Segments

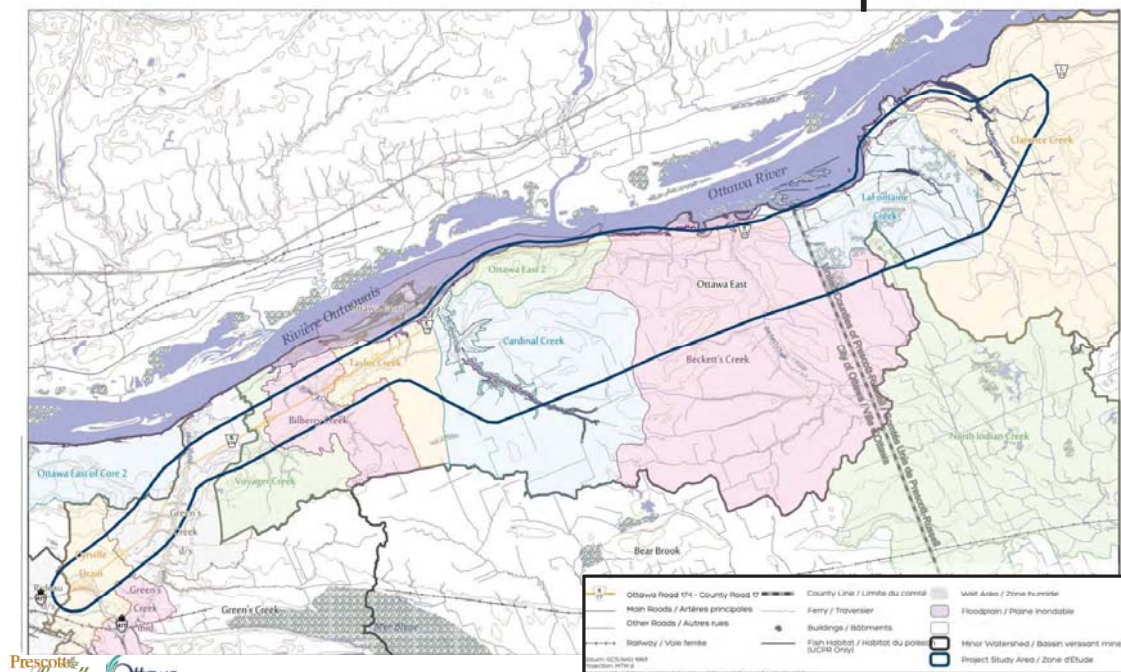
- For illustration and context purposes, the study area has been arranged in three (3) segments
- Study area boundaries may also change to reflect the conditions being considered



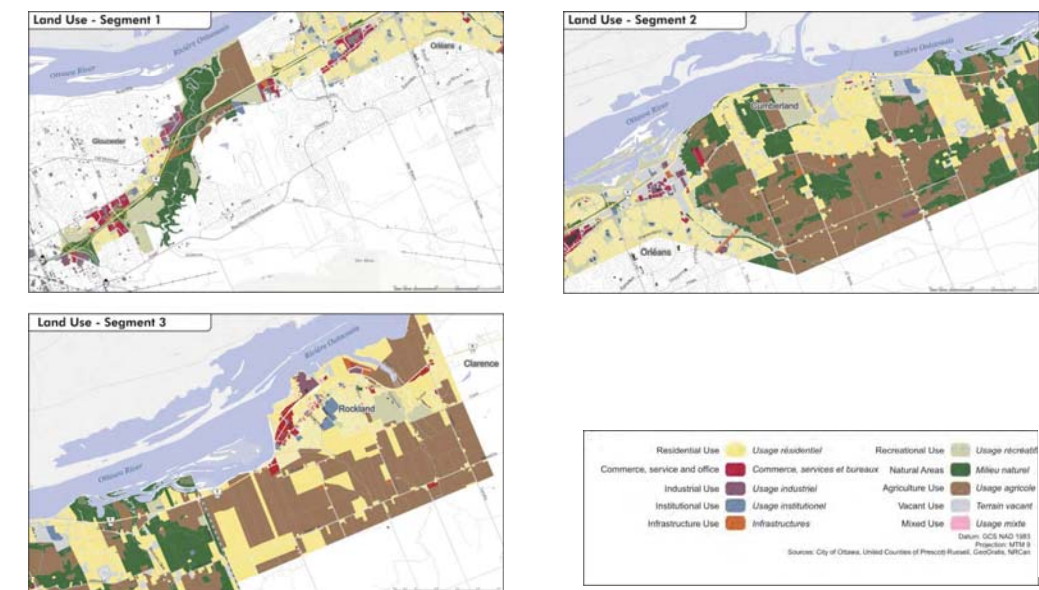
Natural Environment - Terrestrial



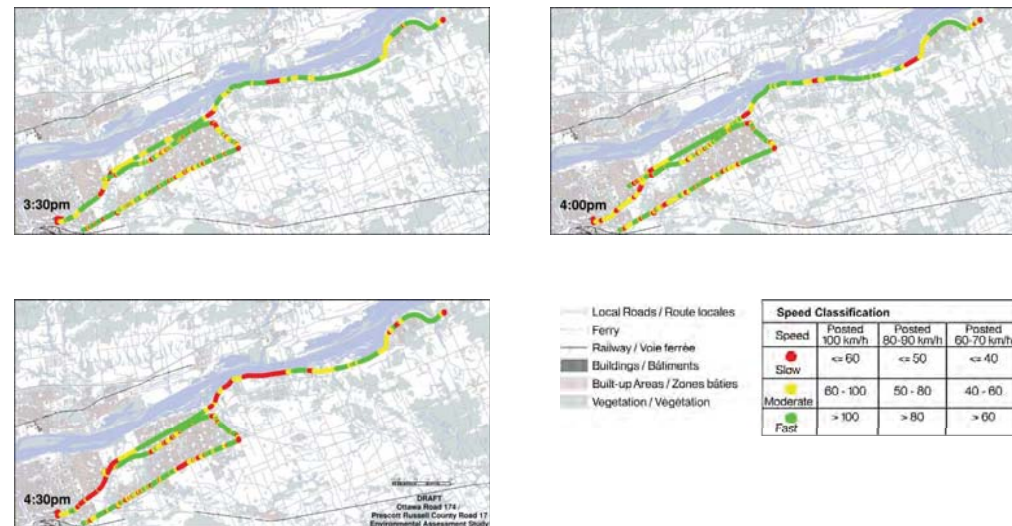
Natural Environment - Aquatic



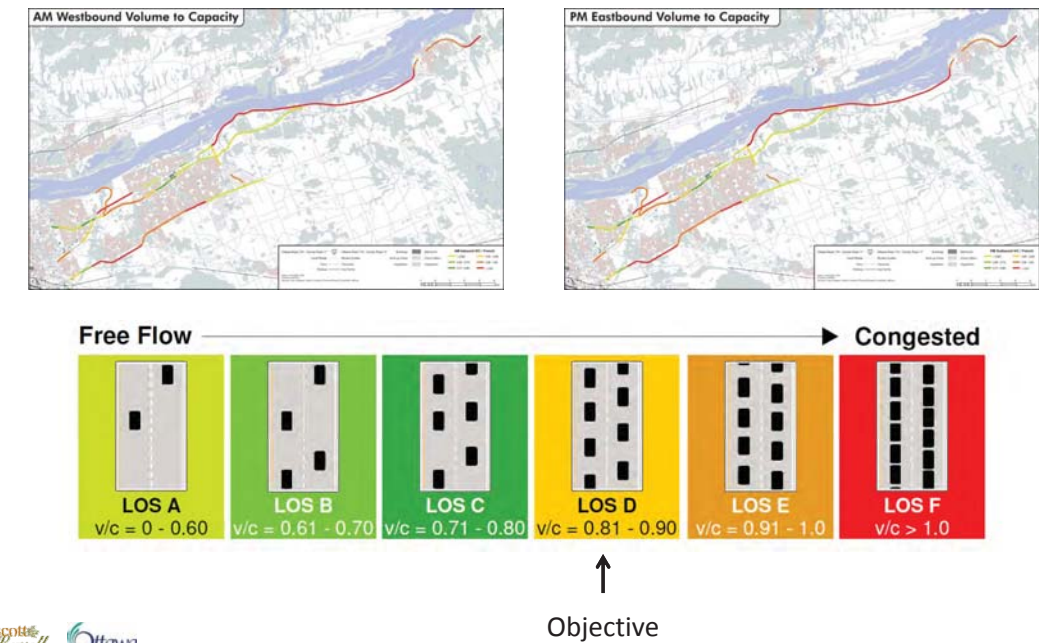
Social Environment – Land Use



Transportation Environment – PM Speeds



Transportation Environment – Current Capacity



Business Condition Assessment

- The most prominent business type along the OR 174-CR 17 corridor is retail, followed by food, fuel and accommodation outlets
- There is a higher concentration of businesses in Gloucester and the City of Clarence Rockland than in Orléans and Cumberland
- Most prevalent business type:
 - Gloucester - Offices or Professional Services
 - Clarence Rockland – retail
- Only a small proportion of businesses are primarily served by passing traffic

Alternative Solutions

Alternative Solutions

- Do Nothing
- Transportation Demand Management (TDM)
- Transportation Systems Management (TSM)
- Additional road capacity
 - Widen existing roads
 - New roads
- Combination of alternatives
- These alternatives may be applicable to different portions of the corridor or for the entire corridor.

TDM Measures

- Transportation Demand Management (TDM) is the application of strategies and policies to reduce travel demand and / or to redistribute this demand over time. Examples include:
 - priority to public transit (such as OR 174 bus on shoulder operation)
 - make walking and cycling more attractive
 - telework
 - public education

Screening

- Screening criteria were developed to assess the merits of the long list of alternative solutions. The screening criteria are:
 - **The ability of the alternative to address the project need.** Alternative solutions must provide transportation capacity and improve safety.
 - **Adherence to policies, regulations, and local standards of practice.** Alternatives should not contravene provincial/federal policies or municipal regulations or policies.
 - **Consideration of environmental impacts.** Alternatives were reviewed to determine their high level impacts on the various environmental conditions. The ability to avoid/reduce/minimize impacts was considered.

TDM Screening

- Measures on their own do not address need
- TDM measures included as part of overall solution

TSM Measures

- Transportation System Management (TSM) is an approach aimed at optimizing the safety, efficiency and capacity of the existing transportation system through the use of effective, low cost improvements. Examples include:
 - passing lanes, reversible lanes
 - traffic signs, signals, pavement markings and regulations
 - signal optimization
 - video monitoring
 - traveller information systems
 - transit vehicle tracking

Screening

Solution	Result
Do Nothing	Does not address the need
TDM	Does not address the need
TSM	Does not address the need
Additional road capacity	May address the need
Combination of alternatives	May address the need

TDM and TSM cannot address the need on their own. They will be included as a part of the overall solution.

TSM Screening

- Passing lane don't address capacity needs. Not carried forward
- Reversible lanes applicable to lower speed urban locations. Not appropriate for rural areas. Speeds and driveways create unsafe conditions for motorists and residents. Not carried forward
- Other TSM measures included as part of overall solution

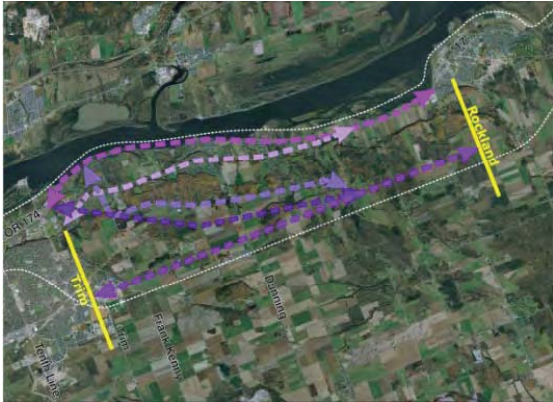
Screening – Split to Trim

Widen existing 174	Widen Innes Road	Widen St. Joseph Blvd	Build New Route
Addresses the need with expected limited environmental impacts when compared with other alternatives. Carry forward.	Widening of Innes Road west of the Greenbelt from 6 to 8 lanes is not operationally feasible; significant environmental impacts. Screen out.	Widening of St. Joseph Blvd provides less capacity than freeway widening and will have significant impacts within the Orleans community and the Greenbelt. Screen out.	New routes are not in keeping with the policies of the NCC and the Greenbelt Master Plan.; significant environmental impacts Screen out.
✓	✗	✗	✗



Screening - Trim to Rockland

Widen existing 174/17	Widen/new route Innes-Baseline Road	Widen Old Montreal Road from Trim to 174 East	Widen Willhaven Road and Frank Kenny	New route south of Willhaven to Baseline Road
Address the need. Carry forward.	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.	Screen out due to adjacent land use and driveways, planned development and geometry	Screen out due to poor horizontal alignment, lack of continuity and adjacent land use	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.
✓	✓	✗	✗	✓



Screening - Rockland

Improve existing 17	Widen existing urban streets within Rockland	Build new route south of Rockland
Address the needs with some environmental impacts associated with wetlands, adjacent land use and water. Carry forward.	No other roads suitable for widening due to adjacent land use/ development. Screen out. (Baseline Road considered in Segment 2)	Address the need providing an alternative route for ongoing development on the south side of Rockland. Carry forward.
✓	✗	✓



Alternatives Carried Forward

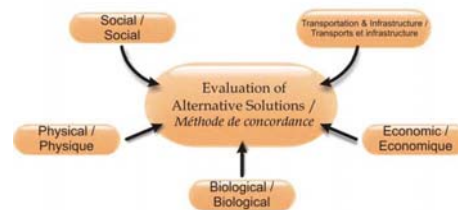
Split to Trim
Widen existing 174 to 6 lanes
Trim to Rockland
Widen existing 174/17 to 4 lanes
Widen/new route Innes-Baseline
Build new route south of Willhaven
Rockland vicinity
Improve existing 17
Build new route south of Rockland

- We will refine the short list of alternatives and undertake a more detailed evaluation of the impacts and benefits of the alternative solutions carried forward to arrive at a preferred alternative solution

Next Steps

Evaluation of Alternative Solutions Carried Forward

- The methodology of evaluating alternative solutions will address the following objectives.
 - Use of Criteria
 - Weighting or Ranking
 - Sensitivity Analysis



Consultation

- Two (2) additional meetings with PCG, BCG and ACG
- Public Open Houses, presentations, comment sheets-questionnaires and other written submissions
- Newspaper notices and contact letters to agencies, stakeholders, etc.
- Individual meetings
- First Nations consultation
- Website and Media coverage
 - www.ottawa.ca/hwy174and17study
 - www.prescott-russell.on.ca/en/public-works/major-projects
- Newsletters
- Presentations to Committees/Council of UCPR and Ottawa
- ESR for formal public review

Criteria

- Evaluation criteria are specific factors or considerations which are used to compare alternatives. Factors that need to be considered in a development of criteria include:
 - the nature of the project
 - the expectations of stakeholders
 - An understanding of the existing conditions
 - the delineation of boundaries
 - the issues related to the proposed project
- The Study Team will develop the criteria to be used in this assessment with the stakeholders.
- Let us know what you think is important and should be considered in the selection of an alternative

Criteria Group Groupes de critères	Examples of Criteria Exemples de critères
Biological Conditions Milieu biologique	<ul style="list-style-type: none"> Wildlife Fisheries Watercourses Woodlots Vegetation
Social Conditions Milieu social	<ul style="list-style-type: none"> Community Linkages Archaeology Economic Activity Noise Recreation Land requirements/use
Physical Conditions Milieu physique	<ul style="list-style-type: none"> Soils Bedrock Groundwater Drainage
Transportation & Infrastructure Transports et infrastructure	<ul style="list-style-type: none"> Transit Ridership Network/System Integration Level of Service Geometry Capacity Operation
Economic Economique	<ul style="list-style-type: none"> Capital Cost Operational and Maintenance Costs Business Impacts

Public Open Houses

- Tuesday February 5, Cumberland Lions Club
- Wednesday February 6, Orléans Sir Wilfred Laurier Secondary School
- Thursday February 7, Rockland City Hall, Council Boardroom



Questions?



Annex D-4

- Invitation letter to First Nations for Agency Consultation Group Meeting #1

January 16, 2013

Ms. Janet Stavinga
Executive Director
Algonquins of Ontario Consultation Office
31 Riverside Drive, Suite 301
Pembroke, ON K8A 8R6
E-mail: jstavinga@nrtco.net

Dear Ms. Stavinga:

Project No: 60270243
Regarding: Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study

Please find attached a copy of the Invitation to the first Agency Consultation Group (ACG) meeting and Notice of Round # 1 of Public Open Houses for the Class Environmental Assessment Study for improvements to Ottawa Road 174 and Prescott-Russell County Road 17.

Please be advised that Chief Gilbert Whiteduck (Kitigan Zibi Anishinabeg First Nation), Janet Leader (Metis Nation of Ontario) and the Québec Métis Nation have also been contacted separately informing them of this project.

If you are interested in attending the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Sincerely,

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Delcan

**Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

We invite you to the first ACG meeting to be held on:

Monday, January 28, 2013
9:00 a.m. to 12:00 p.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West
Ottawa

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013 6:30 p.m. to 9:00 p.m. Cumberland Lions Club Maple Hall 2552 Old Montreal Road Cumberland	Wednesday, February 6, 2013 6:30 p.m. to 9:00 p.m. Sir Wilfrid Laurier Secondary School, Cafeteria 1515 Tenth Line Road Orléans	Thursday, February 7, 2013 6:30 p.m. to 9:00 p.m. City Hall Council boardroom 1560 Laurier Street Rockland
--	--	---

The purpose of the first ACG meeting (and POH #1) is to:

- Introduce the project and the role of the ACG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at:
www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email:
Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com



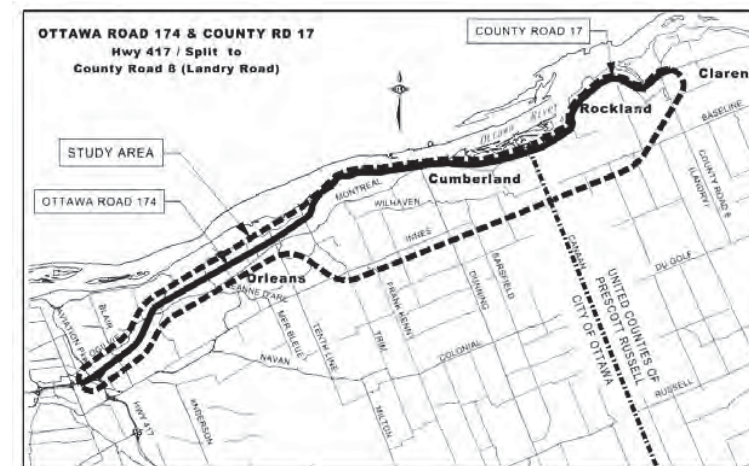
Notice of Round #1 of Public Open Houses
Ottawa Road 174 / Prescott-Russell County Road 17
Class Environmental Assessment Study

Tuesday, February 5, 2013 (Cumberland)
Cumberland Lions Club, Maple Hall
2552 Old Montreal Road, Cumberland
6:30 to 9pm, presentation at 7:00

Wednesday, February 6, 2013 (Orléans)
Sir Wilfrid Laurier Secondary School, Cafeteria
1515 Tenth Line Road, Orléans
6:30 to 9pm, presentation at 7:00

Thursday, February 7, 2013 (Rockland)
City Hall, Council boardroom
1560 Laurier Street, Rockland
6:30 to 9pm, presentation at 7:00

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class Environmental Assessment (EA) study for the Ottawa Road 174 and Prescott-Russell County Road 17 corridor from Highway 417 to County Road 8 (Landry Road). This study will consider improvements to the **existing OR 174-CR 17** corridor, improvements to **other existing roads** and the construction of **new roads** in the Study Area illustrated below. This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class Environmental Assessment (2007, 2011).



The purpose of the first round of public open houses is to:

- Introduce the project;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

This is the first of three rounds of public consultation that will be held during the course of the study to review and discuss the project with the study team and solicit feedback.

For further information on this project, or to be added to our mailing list, visit the web site at www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study or contact:

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Funding for this study is being provided by the Government of Ontario.



January 16, 2013

Ms. Janet Leader
Director of Communications
Metis Nation of Ontario
500 Old St. Patrick Street, Unit D
Ottawa, ON K1N 9G4

Dear Ms. Leader:

Project No: 60270243

**Regarding: Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

Please find attached a copy of the Invitation to the first Agency Consultation Group (ACG) meeting and Notice of Round # 1 of Public Open Houses for the Class Environmental Assessment Study for improvements to Ottawa Road 174 and Prescott-Russell County Road 17.

Please be advised that Janet Stavinga (Algonquins of Ontario Consultation Office), Chief Gilbert Whiteduck (Kitigan Zibi Anishinabeg First Nation) and the Québec Métis Nation have also been contacted informing them of this project.

If you are interested in attending the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Sincerely,

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Delcan

**Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

We invite you to the first ACG meeting to be held on:

Monday, January 28, 2013
9:00 a.m. to 12:00 p.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West
Ottawa

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013
6:30 p.m. to 9:00 p.m.
Cumberland Lions Club
Maple Hall
2552 Old Montreal Road
Cumberland

Wednesday, February 6, 2013
6:30 p.m. to 9:00 p.m.
Sir Wilfrid Laurier Secondary School,
Cafeteria
1515 Tenth Line Road
Orléans

Thursday, February 7, 2013
6:30 p.m. to 9:00 p.m.
City Hall
Council boardroom
1560 Laurier Street
Rockland

The purpose of the first ACG meeting (and POH #1) is to:

- Introduce the project and the role of the ACG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at:
www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email:
Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com



Notice of Round #1 of Public Open Houses
Ottawa Road 174 / Prescott-Russell County Road 17
Class Environmental Assessment Study

Tuesday, February 5, 2013 (Cumberland)

Cumberland Lions Club, Maple Hall
2552 Old Montreal Road, Cumberland
6:30 to 9pm, presentation at 7:00

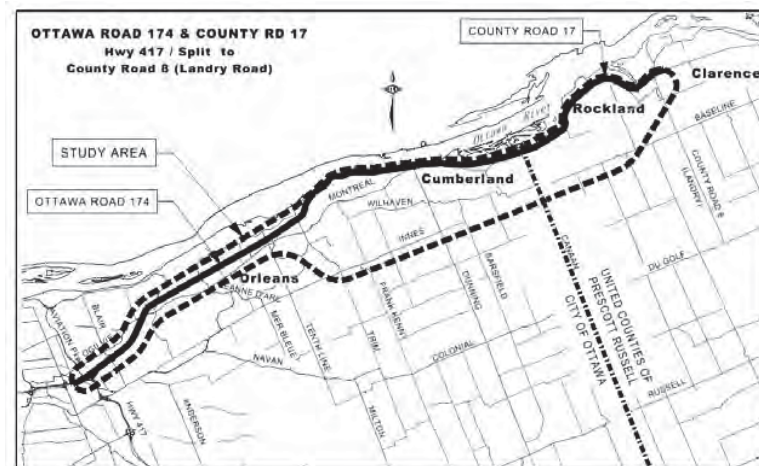
Wednesday, February 6, 2013 (Orléans)

Sir Wilfrid Laurier Secondary School, Cafeteria
1515 Tenth Line Road, Orléans
6:30 to 9pm, presentation at 7:00

Thursday, February 7, 2013 (Rockland)

City Hall, Council boardroom
1560 Laurier Street, Rockland
6:30 to 9pm, presentation at 7:00

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class Environmental Assessment (EA) study for the Ottawa Road 174 and Prescott-Russell County Road 17 corridor from Highway 417 to County Road 8 (Landry Road). This study will consider improvements to the **existing OR 174-CR 17** corridor, improvements to **other existing roads** and the construction of **new roads** in the Study Area illustrated below. This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class Environmental Assessment (2007, 2011).



The purpose of the first round of public open houses is to:

- Introduce the project;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

This is the first of three rounds of public consultation that will be held during the course of the study to review and discuss the project with the study team and solicit feedback.

For further information on this project, or to be added to our mailing list, visit the web site at www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study or contact:

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Funding for this study is being provided by the Government of Ontario.



January 16, 2013

Chief Gilbert Whiteduck
Kitigan Zibi Anishinabeg First Nation
P.O. Box 309
1 Paganakomin Mikan
Maniwaki, QC J9E 3C9

Dear Chief Whiteduck:

Project No: 60270243

**Regarding: Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

Please find attached a copy of the Invitation to the first Agency Consultation Group (ACG) meeting and Notice of Round # 1 of Public Open Houses for the Class Environmental Assessment Study for improvements to Ottawa Road 174 and Prescott-Russell County Road 17.

Please be advised that Janet Stavinga (Algonquins of Ontario Consultation Office), Janet Leader (Metis Nation of Ontario) and the Québec Métis Nation have also been contacted informing them of this project.

If you are interested in attending the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Sincerely,

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Delcan

Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study

We invite you to the first ACG meeting to be held on:

Monday, January 28, 2013
9:00 a.m. to 12:00 p.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West
Ottawa

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013 6:30 p.m. to 9:00 p.m. Cumberland Lions Club Maple Hall 2552 Old Montreal Road Cumberland	Wednesday, February 6, 2013 6:30 p.m. to 9:00 p.m. Sir Wilfrid Laurier Secondary School, Cafeteria 1515 Tenth Line Road Orléans	Thursday, February 7, 2013 6:30 p.m. to 9:00 p.m. City Hall Council boardroom 1560 Laurier Street Rockland
--	--	---

The purpose of the first ACG meeting (and POH #1) is to:

- Introduce the project and the role of the ACG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at:
www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email:
Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com



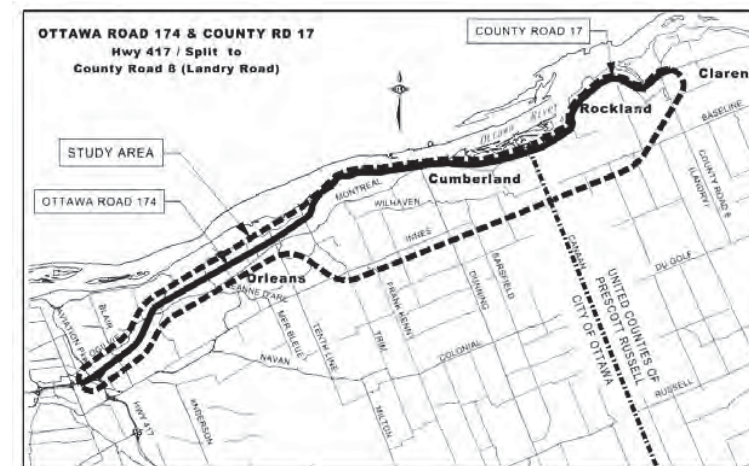
Notice of Round #1 of Public Open Houses
Ottawa Road 174 / Prescott-Russell County Road 17
Class Environmental Assessment Study

Tuesday, February 5, 2013 (Cumberland)
Cumberland Lions Club, Maple Hall
2552 Old Montreal Road, Cumberland
6:30 to 9pm, presentation at 7:00

Wednesday, February 6, 2013 (Orléans)
Sir Wilfrid Laurier Secondary School, Cafeteria
1515 Tenth Line Road, Orléans
6:30 to 9pm, presentation at 7:00

Thursday, February 7, 2013 (Rockland)
City Hall, Council boardroom
1560 Laurier Street, Rockland
6:30 to 9pm, presentation at 7:00

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class Environmental Assessment (EA) study for the Ottawa Road 174 and Prescott-Russell County Road 17 corridor from Highway 417 to County Road 8 (Landry Road). This study will consider improvements to the **existing OR 174-CR 17** corridor, improvements to **other existing roads** and the construction of **new roads** in the Study Area illustrated below. This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class Environmental Assessment (2007, 2011).



The purpose of the first round of public open houses is to:

- Introduce the project;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

This is the first of three rounds of public consultation that will be held during the course of the study to review and discuss the project with the study team and solicit feedback.

For further information on this project, or to be added to our mailing list, visit the web site at www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study or contact:

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Funding for this study is being provided by the Government of Ontario.



January 16, 2013

Québec Métis Nation
115 rue Lessard
St-Raymond, QC
G3L 2N1

Dear Sir/Madam:

Project No: 60270243

**Regarding: Invitation to the first Agency Consultation Group (ACG) meeting
Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study**

Please find attached a copy of the Invitation to the first Agency Consultation Group (ACG) meeting and Notice of Round # 1 of Public Open Houses for the Class Environmental Assessment Study for improvements to Ottawa Road 174 and Prescott-Russell County Road 17.

Please be advised that Chief Gilbert Whiteduck (Kitigan Zibi Anishinabeg First Nation), Janet Stavinga (Algonquins of Ontario Consultation Office) and Janet Leader (Metis Nation of Ontario) have also been contacted separately informing them of this project.

If you are interested in attending the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Sincerely,

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Delcan

**Invitation à la première réunion du Groupe de consultation d'organismes (GCO)
Étude d'évaluation environnementale de portée générale – Route 174 d'Ottawa et route 17 du
comté de Prescott-Russell**

Nous vous invitons à la première réunion du GCO, qui aura lieu le :

Lundi 28 janvier 2013
De 9 h 00 à 12 h 00
l'hôtel de ville, Salle Honeywell
110, avenue Laurier Ouest
Ottawa

Si vous n'êtes pas en mesure d'assister à cette réunion, vous souhaitez peut-être vous joindre à nous pour la première série de séances portes ouvertes, qui se dérouleront aux dates et endroits suivants :

Mardi 5 février 2013
De 18 h 30 à 21 h
Club Lions de Cumberland
Salle Maple
2552, chemin Old Montreal
Cumberland

Mercredi 6 février 2013
De 18 h 30 à 21 h
École secondaire Sir Wilfrid Laurier,
Cafétéria
1515, chemin Tenth Line
Orléans

Jeudi 7 février 2013
De 18 h 30 à 21 h
Hôtel de ville
Salle du Conseil
1560, rue Laurier
Rockland

Les objectifs de la première réunion du GCO et de la première série de séances portes ouvertes sont les suivants :

- Présenter le projet et le rôle du GCO;
- Constater la nécessité du projet et les conditions actuelles;
- Prendre connaissance des critères d'évaluation proposés;
- Déterminer les solutions de rechange;
- Examiner l'évaluation des solutions de rechange.

Vous trouverez des renseignements au sujet de l'étude, y compris les avis, sur le site Web de la Ville d'Ottawa, au www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure ou au www.ottawa.ca/etuderoute174et17.

Veuillez donner confirmation de votre participation à Danielle Chartrand par téléphone, au 613-820-8282, poste 238, ou par courriel, à l'adresse Danielle.chartrand@aecom.com. Pour obtenir de plus amples renseignements sur le projet, veuillez communiquer avec moi ou avec :

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
Tél. : 613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Marc R. Clermont, ing.
Directeur des Travaux publics
Comtés unis de Prescott et Russell
Tél. : 613-675-4661, poste 3100
Courriel : MClermont@prescott-russell.on.ca

Recevez nos salutations distinguées.
AECOM Canada Ltd.

Valerie McGirr, ing.
Chargée de projet de firme de conseils
Courriel : Valerie.mcgirr@aecom.com
Tél. : 613-820-8282, poste 243

AECOM
1150, promenade Morrison, pièce 302, Ottawa (Ontario) K2H 8S9
Tél. : 613-820-8282; téléc. : 613-820-8338
www.aecom.com



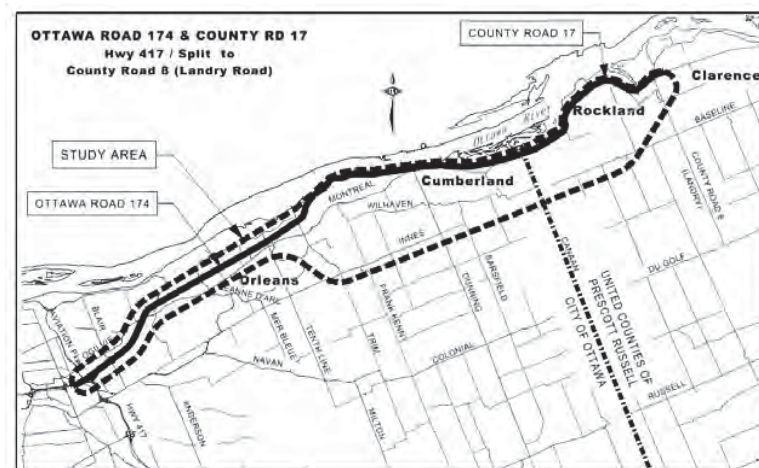
Avis – Première série de séances portes ouvertes
Route 174 d'Ottawa / Route 17 du comté de Prescott-Russell
Étude d'évaluation environnementale de portée générale

Mardi 5 février 2013 (Cumberland)
Club Lions de Cumberland, salle Maple
2552, chemin Old Montreal, Cumberland
De 18 h 30 à 21 h, exposé à 19 h

Mercredi 6 février 2013 (Orléans)
École secondaire Sir Wilfrid Laurier, cafétéria
1515, chemin Tenth Line, Orléans
De 18 h 30 à 21 h, exposé à 19 h

Jeudi 7 février 2013 (Rockland)
Hôtel de ville, Salle du Conseil municipal
1560, rue Laurier, Rockland
De 18 h 30 à 21 h, exposé à 19 h

Les Comtés unis de Prescott et Russell entreprennent, en partenariat avec la Ville d'Ottawa, une étude d'évaluation environnementale de portée générale pour le couloir de la route 174 d'Ottawa et de la route 17 du comté de Prescott-Russell, de l'autoroute 417 à la route de comté 8 (chemin Landry). Cette étude portera sur les améliorations à apporter **au couloir de la route 174 d'Ottawa et de la route de comté 17**, sur les améliorations à apporter **aux autres routes existantes** et sur la construction de **nouvelles routes** dans le secteur visé par l'étude, qui est illustré ci-dessous. Cette étude est menée conformément aux exigences de l'annexe C du document sur les évaluations environnementales municipales de portée générale (2007, 2011).



Les objectifs de cette première série de séances portes ouvertes sont les suivants :

- Présenter le projet;
- Constater la nécessité du projet et les conditions actuelles;
- Prendre connaissance des critères d'évaluation proposés;
- Déterminer les solutions de rechange;
- Examiner l'évaluation des solutions de rechange.

Il y aura trois séries de séances de consultation publique au cours de l'étude. Ces séances auront pour but de permettre à la population d'examiner le projet, d'en discuter avec l'équipe chargée de l'étude et de formuler des commentaires.

Pour obtenir de plus amples renseignements sur le projet ou pour faire ajouter votre nom à notre liste d'envoi, allez au www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure ou au www.ottawa.ca/hwy174and17study ou encore communiquez avec :

Marc R. Clermont, ing.
Directeur des Travaux publics
Comtés unis de Prescott et Russell
Tél. : 613-675-4661, poste 3100
Courriel : MClermont@prescott-russell.on.ca

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
Tél. : 613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Valerie McGirr, ing.
Chargée de projet de firme de conseils
AECOM
Tél. : 613-820-8282, poste 243
Courriel : Valerie.mcgirr@aecom.com

Le financement de cette étude est assuré par le gouvernement de l'Ontario.

Invitation to the first Agency Consultation Group (ACG) meeting Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study

We invite you to the first ACG meeting to be held on:

Monday, January 28, 2013
9:00 a.m. to 12:00 p.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West
Ottawa

If you are unable to attend this first meeting you may wish to join us at the first round of public open houses being held at the following locations:

Tuesday, February 5, 2013
6:30 p.m. to 9:00 p.m.
Cumberland Lions Club
Maple Hall
2552 Old Montreal Road
Cumberland

Wednesday, February 6, 2013
6:30 p.m. to 9:00 p.m.
Sir Wilfrid Laurier Secondary School,
Cafeteria
1515 Tenth Line Road
Orléans

Thursday, February 7, 2013
6:30 p.m. to 9:00 p.m.
City Hall
Council boardroom
1560 Laurier Street
Rockland

The purpose of the first ACG meeting (and POH #1) is to:

- Introduce the project and the role of the ACG;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

Information about the study including notices can be found on the City of Ottawa web site at: www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study

Please confirm attendance with Danielle Chartrand at 613-820-8282 ext. 238 or by email: Danielle.chartrand@aecom.com. For further information on this project please contact me or:

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Consultant Project Manager
E-mail: Valerie.mcgirr@aecom.com
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com



Notice of Round #1 of Public Open Houses
Ottawa Road 174 / Prescott-Russell County Road 17
Class Environmental Assessment Study

Tuesday, February 5, 2013 (Cumberland)

Cumberland Lions Club, Maple Hall
2552 Old Montreal Road, Cumberland
6:30 to 9pm, presentation at 7:00

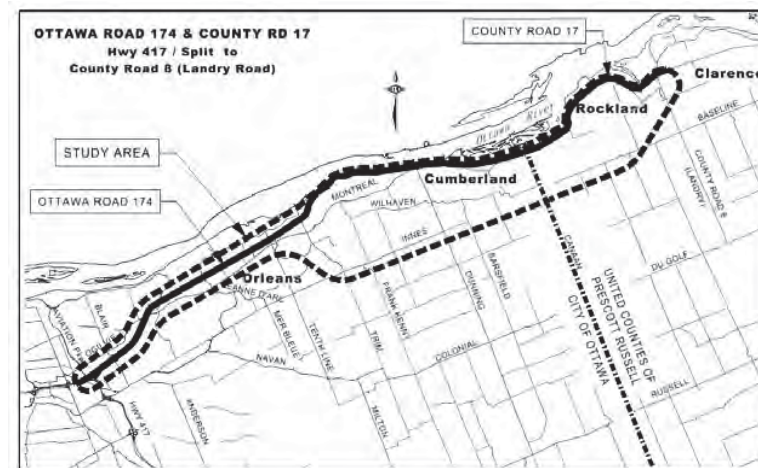
Wednesday, February 6, 2013 (Orléans)

Sir Wilfrid Laurier Secondary School, Cafeteria
1515 Tenth Line Road, Orléans
6:30 to 9pm, presentation at 7:00

Thursday, February 7, 2013 (Rockland)

City Hall, Council boardroom
1560 Laurier Street, Rockland
6:30 to 9pm, presentation at 7:00

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class Environmental Assessment (EA) study for the Ottawa Road 174 and Prescott-Russell County Road 17 corridor from Highway 417 to County Road 8 (Landry Road). This study will consider improvements to the **existing OR 174-CR 17** corridor, improvements to **other existing roads** and the construction of **new roads** in the Study Area illustrated below. This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class Environmental Assessment (2007, 2011).



The purpose of the first round of public open houses is to:

- Introduce the project;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

This is the first of three rounds of public consultation that will be held during the course of the study to review and discuss the project with the study team and solicit feedback.

For further information on this project, or to be added to our mailing list, visit the web site at www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study or contact:

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Funding for this study is being provided by the Government of Ontario.

Annex D-5

- Notice of Round #1 of Public Open Houses (English and French)
- Presentation for Public Open House #1 (English and French)
- Display Material for Public Open House #1 (English and French)



Notice of Round #1 of Public Open Houses
Ottawa Road 174 / Prescott-Russell County Road 17
Class Environmental Assessment Study

Tuesday, February 5, 2013 (Cumberland)

Cumberland Lions Club, Maple Hall
2552 Old Montreal Road, Cumberland
6:30 to 9pm, presentation at 7:00

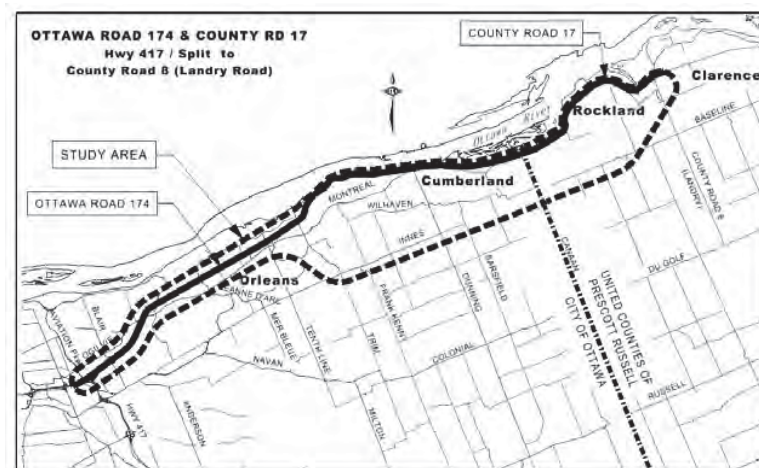
Wednesday, February 6, 2013 (Orléans)

Sir Wilfrid Laurier Secondary School, Cafeteria
1515 Tenth Line Road, Orléans
6:30 to 9pm, presentation at 7:00

Thursday, February 7, 2013 (Rockland)

City Hall, Council boardroom
1560 Laurier Street, Rockland
6:30 to 9pm, presentation at 7:00

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class Environmental Assessment (EA) study for the Ottawa Road 174 and Prescott-Russell County Road 17 corridor from Highway 417 to County Road 8 (Landry Road). This study will consider improvements to the **existing OR 174-CR 17** corridor, improvements to **other existing roads** and the construction of **new roads** in the Study Area illustrated below. This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class Environmental Assessment (2007, 2011).



The purpose of the first round of public open houses is to:

- Introduce the project;
- Review the Project Need and Existing Conditions;
- Consult on the proposed evaluation criteria;
- Identify the alternative solutions; and
- Review the evaluation of alternative solutions.

This is the first of three rounds of public consultation that will be held during the course of the study to review and discuss the project with the study team and solicit feedback.

For further information on this project, or to be added to our mailing list, visit the web site at www.prescott-russell.on.ca/en/public-works/major-projects or www.ottawa.ca/hwy174and17study or contact:

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Funding for this study is being provided by the Government of Ontario.



Avis – Première série de séances portes ouvertes
Route 174 d'Ottawa / Route 17 du comté de Prescott-Russell
Étude d'évaluation environnementale de portée générale

Mardi 5 février 2013 (Cumberland)

Club Lions de Cumberland, salle Maple
2552, chemin Old Montreal, Cumberland
De 18 h 30 à 21 h, exposé à 19 h

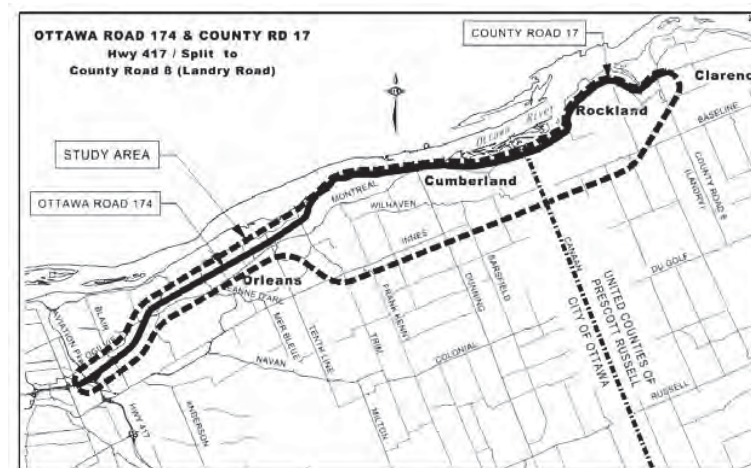
Mercredi 6 février 2013 (Orléans)

École secondaire Sir Wilfrid Laurier, cafétéria
1515, chemin Tenth Line, Orléans
De 18 h 30 à 21 h, exposé à 19 h

Jeudi 7 février 2013 (Rockland)

Hôtel de ville, Salle du Conseil municipal
1560, rue Laurier, Rockland
De 18 h 30 à 21 h, exposé à 19 h

Les Comtés unis de Prescott et Russell entreprennent, en partenariat avec la Ville d'Ottawa, une étude d'évaluation environnementale de portée générale pour le couloir de la route 174 d'Ottawa et de la route 17 du comté de Prescott-Russell, de l'autoroute 417 à la route de comté 8 (chemin Landry). Cette étude portera sur les améliorations à apporter **au couloir de la route 174 d'Ottawa et de la route de comté 17**, sur les améliorations à apporter **aux autres routes existantes** et sur la construction de **nouvelles routes** dans le secteur visé par l'étude, qui est illustré ci-dessous. Cette étude est menée conformément aux exigences de l'annexe C du document sur les évaluations environnementales municipales de portée générale (2007, 2011).



Les objectifs de cette première série de séances portes ouvertes sont les suivants :

- Présenter le projet;
- Constater la nécessité du projet et les conditions actuelles;
- Prendre connaissance des critères d'évaluation proposés;
- Déterminer les solutions de rechange;
- Examiner l'évaluation des solutions de rechange.

Il y aura trois séries de séances de consultation publique au cours de l'étude. Ces séances auront pour but de permettre à la population d'examiner le projet, d'en discuter avec l'équipe chargée de l'étude et de formuler des commentaires.

Pour obtenir de plus amples renseignements sur le projet ou pour faire ajouter votre nom à notre liste d'envoi, allez au www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure ou au www.ottawa.ca/etuderoute174et17 ou encore communiquez avec :

Marc R. Clermont, ing.
Directeur des Travaux publics
Comtés unis de Prescott et Russell
Tél. : 613-675-4661, poste 3100
Courriel : MClermont@prescott-russell.on.ca

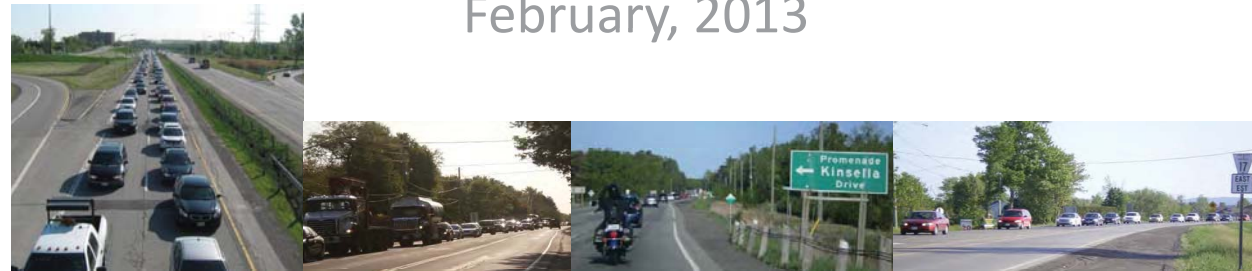
Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
Tél. : 613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Valerie McGirr, ing.
Chargée de projet de firme de conseils
AECOM
Tél. : 613-820-8282, poste 243
Courriel : Valerie.mcgirr@aecom.com

Le financement de cette étude est assuré par le gouvernement de l'Ontario.

Public Meeting

February, 2013



Introductions



Agenda

- Introductions
- Project Overview
 - Background
 - Study Process
- Needs Assessment
- Existing Conditions
- Alternative Solutions
- Next Steps



Co-Proponents

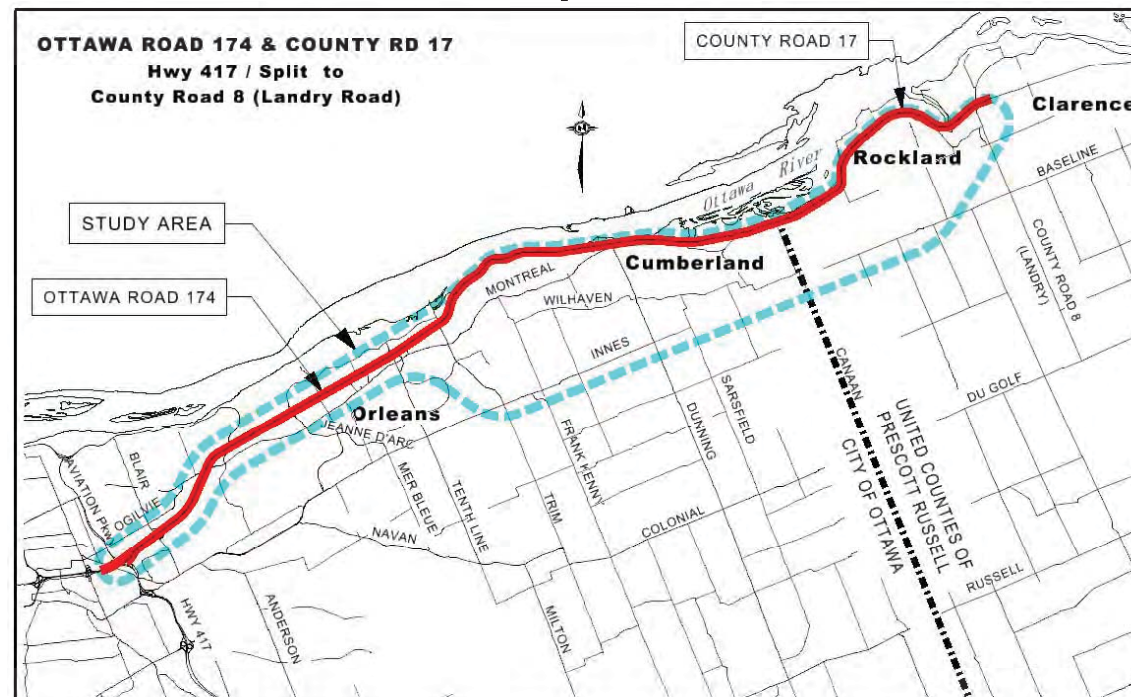
- Prescott Russell
 - Marc Clermont
 - Dave McAvoy/Phil Pawliuk
- City of Ottawa
 - Angela Taylor



Study Team

- AECOM
- Delcan
- Study Team
 - Houle Chevrier, LRL, Golder

Study Area

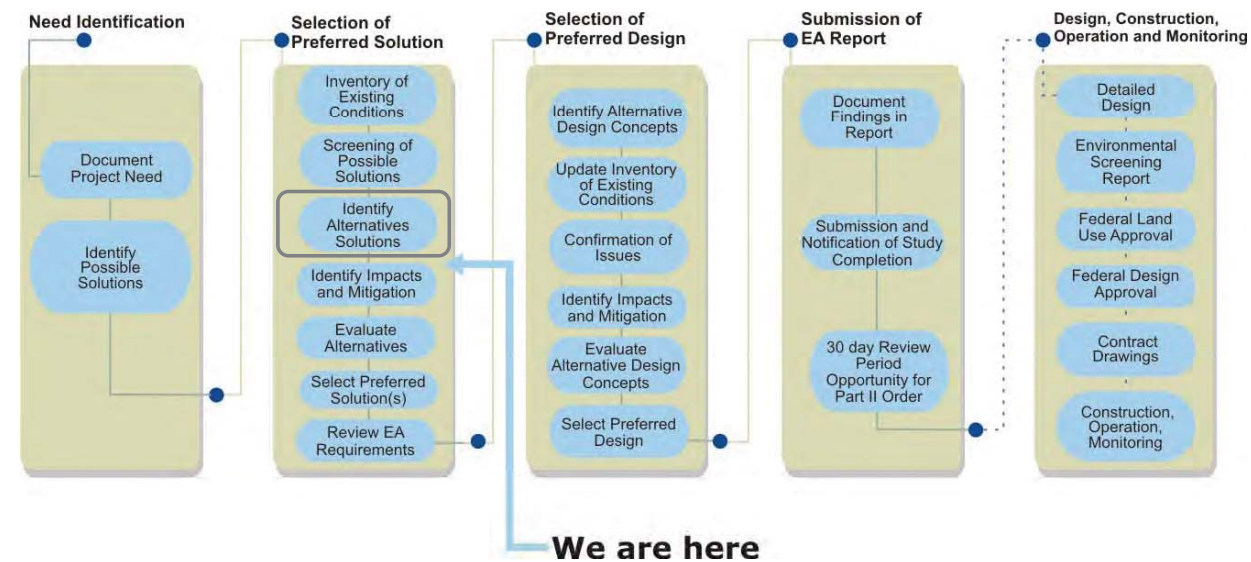


Project Overview

Background

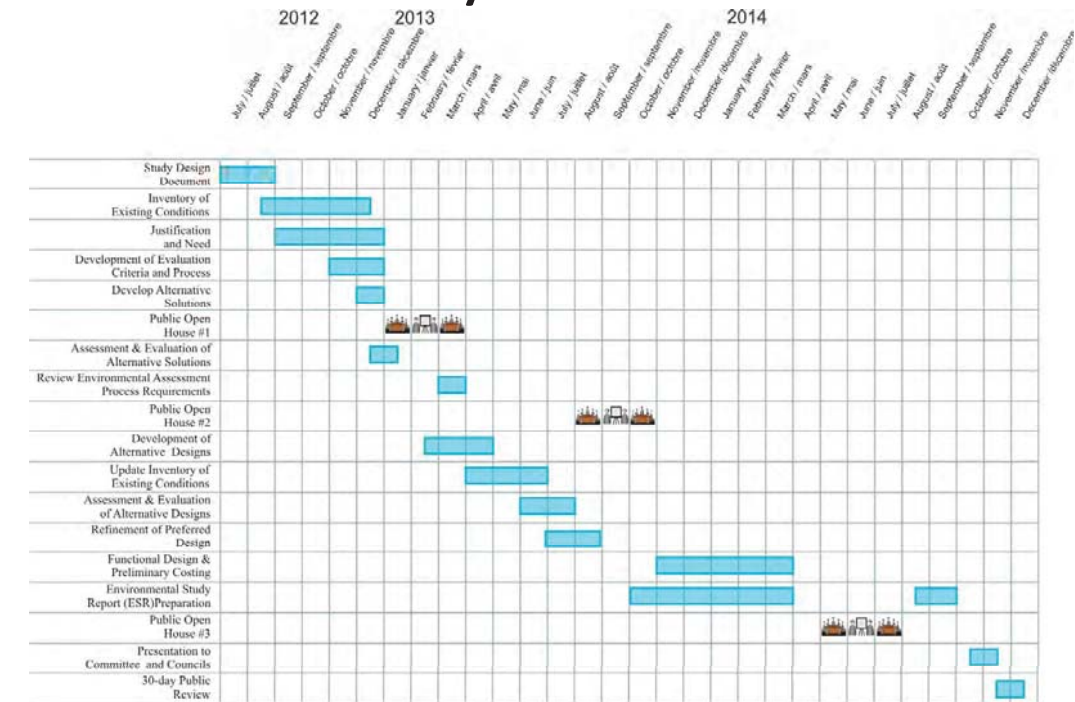
- Provincial Highway 17 until 1997
- Congestion and safety problems documented for years
- Ottawa River and Mer Bleue Bog limit potential east-west routes
- OR 174-CR 17 only high capacity arterial in area
- Widening of OR 174 to 6 lanes to Jeanne d'Arc in TMP
 - Phase 2: 2016 to 2022
- Growth continuing. Development in Orléans and Rockland
- UCPR and City jointly initiated Municipal Class EA Study – Schedule C

Study Process



Needs Assessment

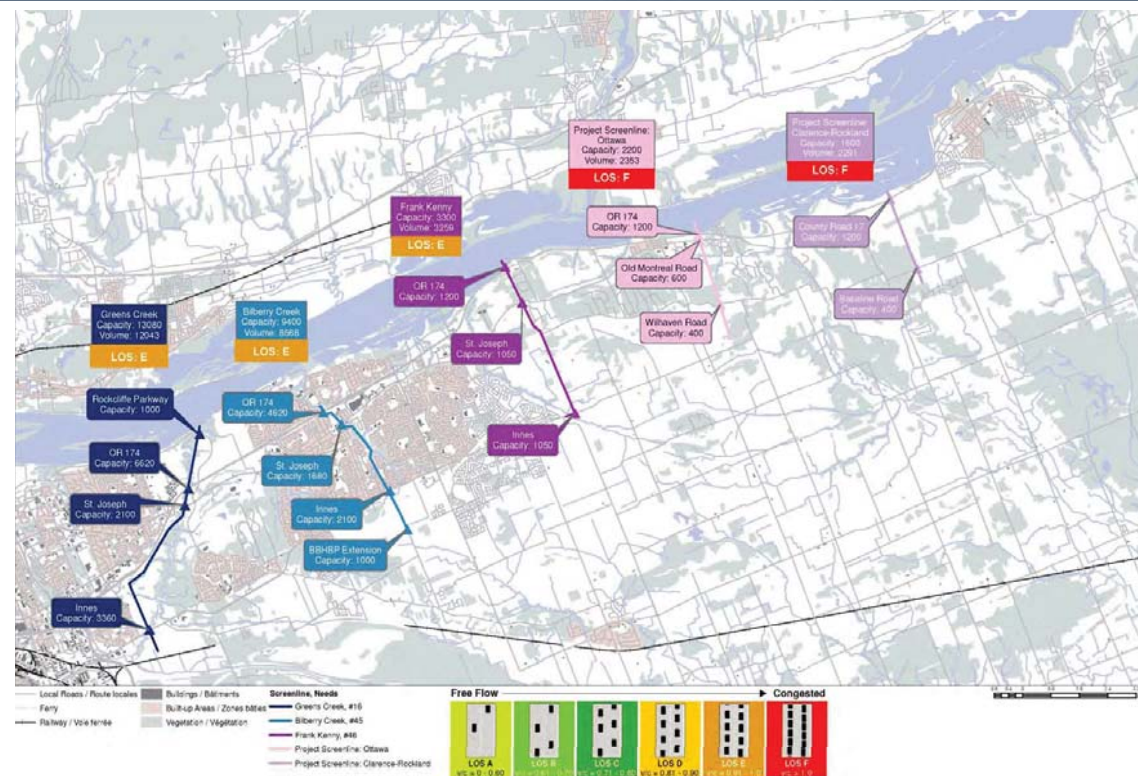
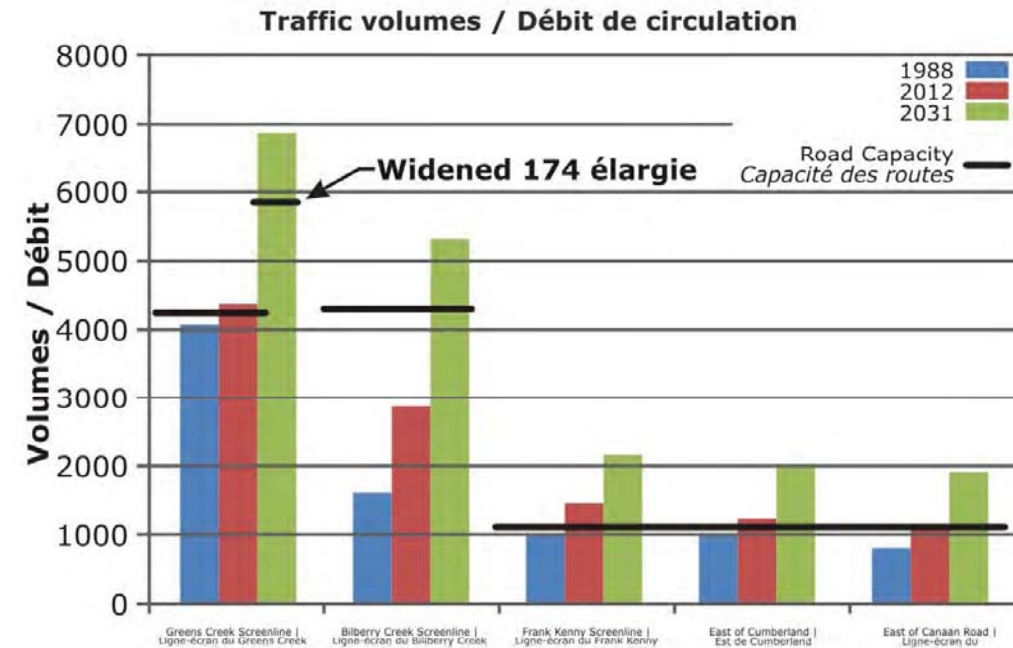
Study Schedule



Overview

- Transportation problems in the area have been identified for many years. The Needs Assessment for this study has reviewed previous reports and utilized new information to assess:
 - existing and future transportation capacity
 - safety improvement opportunities
 - impacts of new developments and expanded urban areas

Volumes and Capacity Over Time



Objective

Capacity

- 2031 AM peak volumes examined using screenlines
 - Assuming all road and transit infrastructure planned for 2031 in place
- Considered downstream capacity constraints
 - Highway 417
- Tested for sensitivity for transit use
 - 5 and 10 percentile increase and decrease
- Even with increased transit use and other travel-reducing strategies, will need additional capacity from the split to Rockland

Safety

- Considered recent collision data
 - Split to Trim – 1487 collisions
 - Trim to Canaan – 465 collisions
 - Canaan to Landry – 287 collisions
- Examined:
 - Collision severity (property damage only, injury and fatal)
 - Collision impact type (turning, angle, rear end, side swipe, single motor vehicle)
 - Lighting condition (daylight, dark, dawn, dusk)
 - Weather (clear, rain, snow)
 - Time of day (AM/PM peak, mid day, evening/early morning)
- Considered road characteristics (horizontal and vertical curves, lane width, passing opportunities, clear zone)

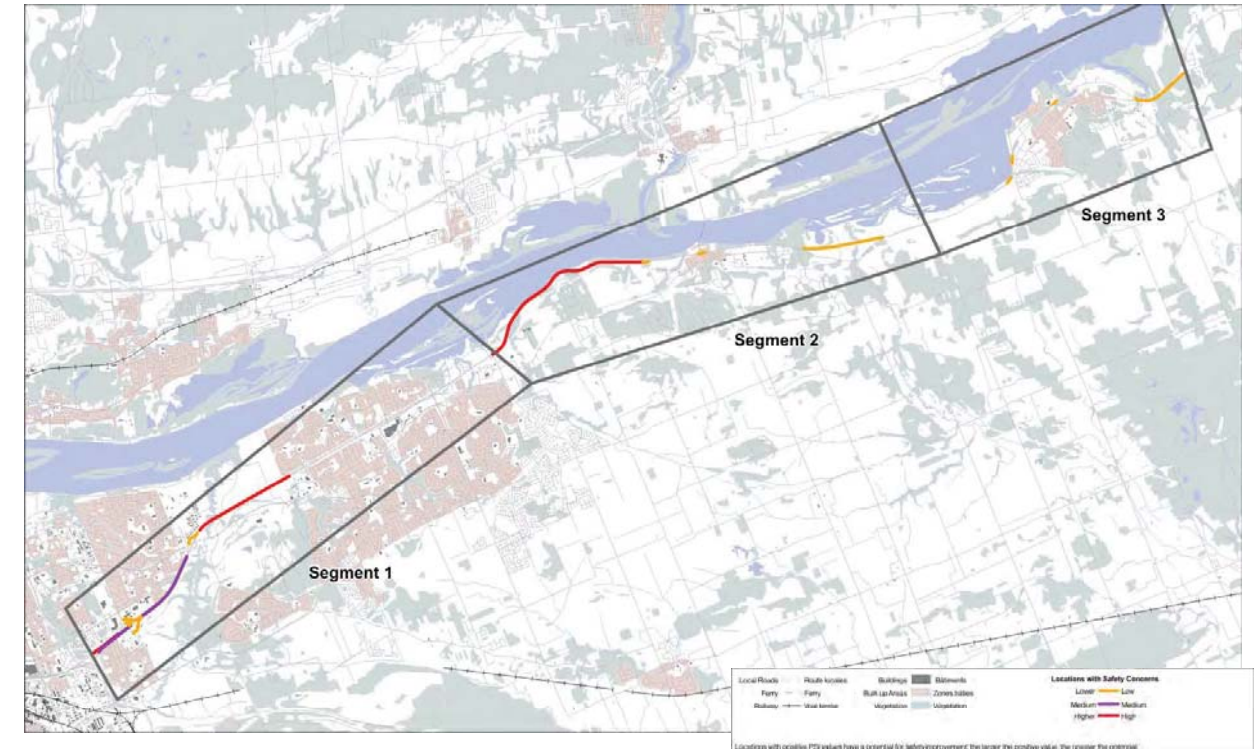
Safety

continued

- Significant contributors to 174-17 collision experience:
 - Speed, roadside character, access, interchange and intersection geometry, lighting, positive guidance, passing opportunities, accommodating transit
- Best potential to improve safety
 - Address congestion
 - Driver awareness
 - Intersection modifications
 - Improve lighting conditions

Conclusions

- There is a need to provide additional transportation capacity
- There are opportunities for safety improvements in the OR174/CR17 corridor



Existing Conditions

Overview

- Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by a transportation facility. The information will be used to:
 - develop reasonable alternatives
 - identify impacts
 - evaluate alternatives
- Information will be updated, as necessary, as the study progresses and more detailed information is required.

Existing Conditions

- Natural Environment
 - Significant features, watercourses, species at risk
- Social Environment
 - Land use, policies, archaeology/heritage, views and vistas
- Transportation
 - Transit, roads, pathways, cycling, travel volumes and capacity, level of service
- Economics
 - Business conditions assessment

Study Area



Alternative Solutions

Alternative Solutions

- Do Nothing
- Transportation Demand Management (TDM)
- Transportation Systems Management (TSM)
- Additional road capacity
 - Widen existing roads
 - New roads
- Combination of alternatives
- These alternatives may be applicable to different portions of the corridor or for the entire corridor.

TDM Measures

- Transportation Demand Management (TDM) is the application of strategies and policies to reduce travel demand and / or to redistribute this demand over time. Examples include:
 - priority to public transit
 - make walking and cycling more attractive
 - telework
 - public education

Screening

- Screening criteria were developed to assess the merits of the long list of alternative solutions. The screening criteria are:
 - **The ability of the alternative to address the project need.** Alternative solutions must provide transportation capacity and improve safety.
 - **Adherence to policies, regulations, and local standards of practice.** Alternatives should not contravene provincial/federal policies or municipal regulations or policies.
 - **Consideration of environmental impacts.** Alternatives were reviewed to determine their high level impacts on the various environmental conditions. The ability to avoid/reduce/minimize impacts was considered.

TDM Screening

- Measures on their own do not address need
- TDM measures included as part of overall solution

TSM Measures

- Transportation System Management (TSM) is an approach aimed at optimizing the safety, efficiency and capacity of the existing transportation system through the use of effective, low cost improvements. Examples include:
 - passing lanes, reversible lanes
 - traffic signs, signals, pavement markings and regulations
 - signal optimization
 - video monitoring
 - traveller information systems
 - transit vehicle tracking

Screening – Split to Trim

Widen existing 174	Widen Innes Road	Widen St. Joseph Blvd	Build New Route
Addresses the need with expected limited environmental impacts when compared with other alternatives. Carry forward.	Widening of Innes Road west of the Greenbelt from 6 to 8 lanes is not operationally feasible. Screen out.	Widening of St. Joseph Blvd provides less capacity than freeway widening and will have significant impacts within the Orleans community. Screen out.	New routes are not in keeping with the policies of the NCC and the Greenbelt Master Plan. Screen out.
✓	✗	✗	✗



TSM Screening

- Passing lane don't address capacity needs. Not carried forward.
- Reversible lanes applicable to lower speed urban locations. Not appropriate for rural areas. Speeds and driveways create unsafe conditions for motorists and residents. Not carried forward
- Other TSM measures included as part of overall solution

Screening - Trim to Rockland

Widen existing 174/17	Widen/new route Innes-Baseline Road	Widen Old Montreal Road from Trim to 174 East	Widen Wilhaven Road and Frank Kenny	New route south of Wilhaven to Baseline Road
Address the need. Carry forward.	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.	Screen out due to adjacent land use and driveways, planned development and geometry	Screen out due to poor horizontal alignment, lack of continuity and adjacent land use	Addresses the need if appropriate connection to OR 174 can be developed. Carry forward.
✓	✓	✗	✗	✓



Screening - Rockland

Improve existing 17	Widen existing urban streets within Rockland	Build new route south of Rockland
Address the needs with some environmental impacts associated with wetlands, adjacent land use and water. Carry forward.	No other roads suitable for widening due to adjacent land use/ development. Screen out. (Baseline Road considered in Segment 2)	Address the need providing an alternative route for ongoing development on the south side of Rockland. Carry forward.
✓	✗	✓



Next Steps

Alternatives Carried Forward

Split to Trim

Widen existing 174 to 6 lanes

Trim to Rockland

Widen existing 174/17 to 4 lanes

Widen/new route Innes-Baseline

Build new route south of Wilhaven

Rockland vicinity

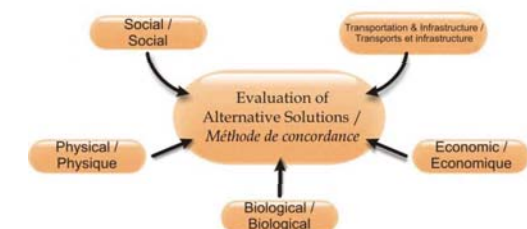
Improve existing 17

Build new route south of Rockland

- We will refine the short list of alternatives and undertake a more detailed evaluation of the impacts and benefits of the alternative solutions carried forward to arrive at a preferred alternative solution

Evaluation of Alternative Solutions Carried Forward

- The methodology of evaluating alternative solutions will address the following objectives.
 - Use of Criteria
 - Weighting or Ranking
 - Sensitivity Analysis



Criteria

- Evaluation criteria are specific factors or considerations which are used to compare alternatives. Factors that need to be considered in a development of criteria include:
 - the nature of the project
 - the expectations of stakeholders
 - An understanding of the existing conditions
 - the delineation of boundaries
 - the issues related to the proposed project
- The Study Team will develop the criteria to be used in this assessment with the stakeholders.
- Let us know what you think is important and should be considered in the selection of an alternative

Criteria Group Groupes de critères	Examples of Criteria Exemples de critères	
Biological Conditions Milieu biologique	<ul style="list-style-type: none"> • Wildlife • Fisheries • Watercourses • Woodlots • Vegetation 	<ul style="list-style-type: none"> • Faune • Pêches • Cours d'eau • Boisés • Végétation
Social Conditions Milieu social	<ul style="list-style-type: none"> • Community Linkages • Archaeology • Economic Activity • Noise • Recreation • Land requirements/use 	<ul style="list-style-type: none"> • Liens communautaires • Archéologie • Activité économique • Bruit • Loisirs • Exigences et utilisation des terres
Physical Conditions Milieu physique	<ul style="list-style-type: none"> • Soils • Bedrock • Groundwater • Drainage 	<ul style="list-style-type: none"> • Sols • Fondation rocheuse • Eaux souterraines • Drainage
Transportation & Infrastructure Transports et infrastructure	<ul style="list-style-type: none"> • Transit Ridership • Network/System Integration • Level of Service • Geometry • Capacity • Operation 	<ul style="list-style-type: none"> • Acheminement du transport en commun • Intégration du réseau ou du système • Niveau de service • Géométrie • Capacité • Opération
Economic Économique	<ul style="list-style-type: none"> • Capital Cost • Operational and Maintenance Costs • Business Impacts 	<ul style="list-style-type: none"> • Coût en capital • Coûts de fonctionnement et d'entretien • Impact sur les entreprises

Questions?

Please state your name and where
your are from

Consultation

- Meetings with PCG, BCG and ACG
- Public Open Houses, presentations, comment sheets-questionnaires and other written submissions
- Newspaper notices and contact letters to agencies, stakeholders, etc.
- Individual meetings
- First Nations consultation
- Website and Media coverage
 - www.ottawa.ca/hwy174and17study
 - <http://www.prescott-russell.on.ca/en/public-works/major-projects>
- Newsletters
- Presentations to Committees/Council of UCPR and Ottawa
- ESR for formal public review

Séance portes ouvertes

Février 2013





Ordre du jour

- Présentations
- Aperçu du projet
 - Contexte
 - Processus d'étude
- Évaluation des besoins
- Conditions actuelles
- Solutions possibles
- Prochaines étapes



Promoteurs

- Comtés unis de Prescott et Russell
 - Marc Clermont
 - Dave McAvoy et Phil Pawluik
- Ville d'Ottawa
 - Angela Taylor



Présentations



Équipe d'étude

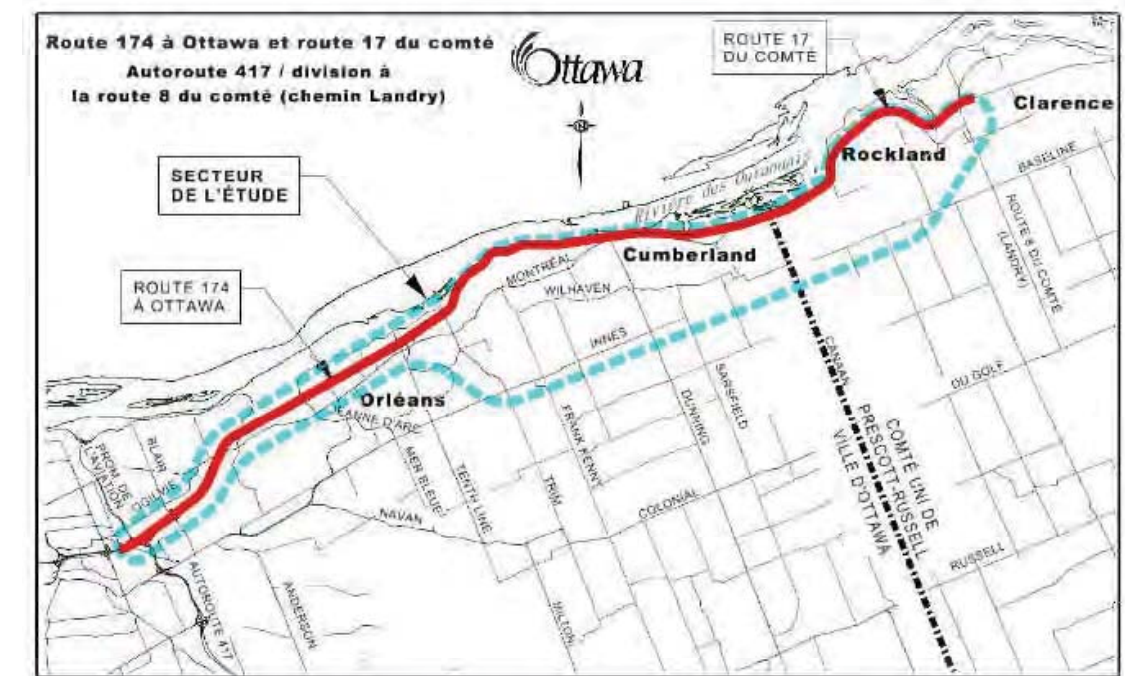
- AECOM
- Delcan
- Équipe d'étude
 - Houle Chevrier, LRL, Golder

Aperçu du projet

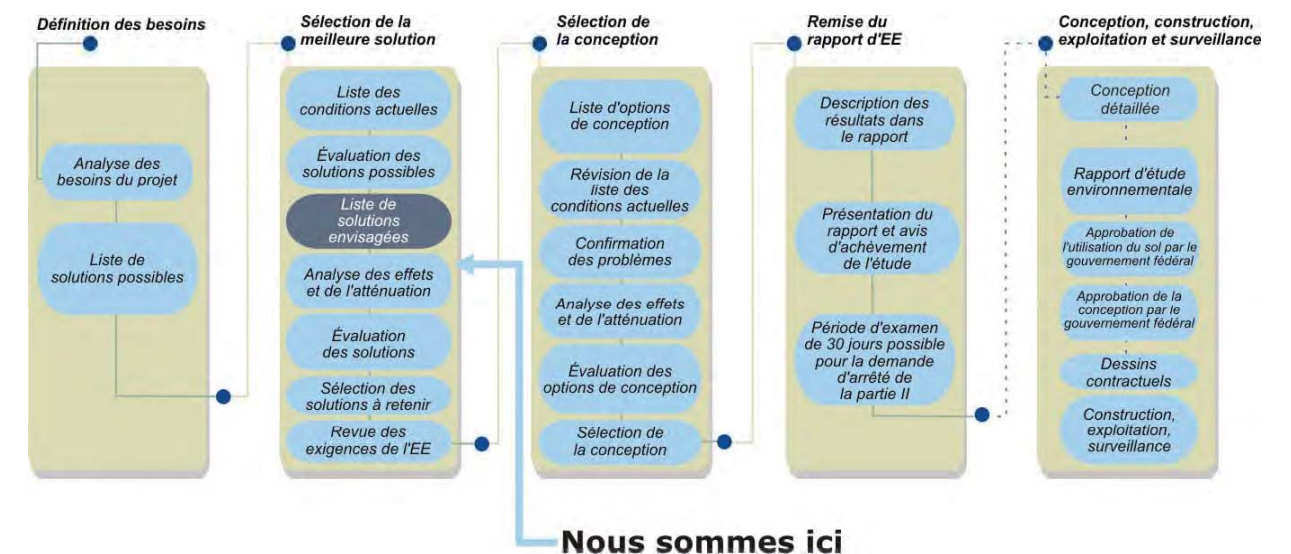
Contexte

- Route provinciale 17 jusqu'en 1997
- Problèmes de congestion routière et de sécurité observés depuis plusieurs années
- Création de routes est-ouest limitée par la rivière Outaouais et la tourbière de la Mer Bleue
- Seule artère de grande capacité du secteur
- Élargissement de la route 174 à six voies jusqu'au boulevard Jeanne d'Arc prévu par le PDT
 - Phase 2 : 2016 à 2022
- Poursuite de la croissance : urbanisation à Orléans et à Rockland
- Mise en œuvre d'une étude conformément à l'annexe C de l'évaluation environnementale municipale de portée générale effectuée conjointement par les CUPR et la Ville

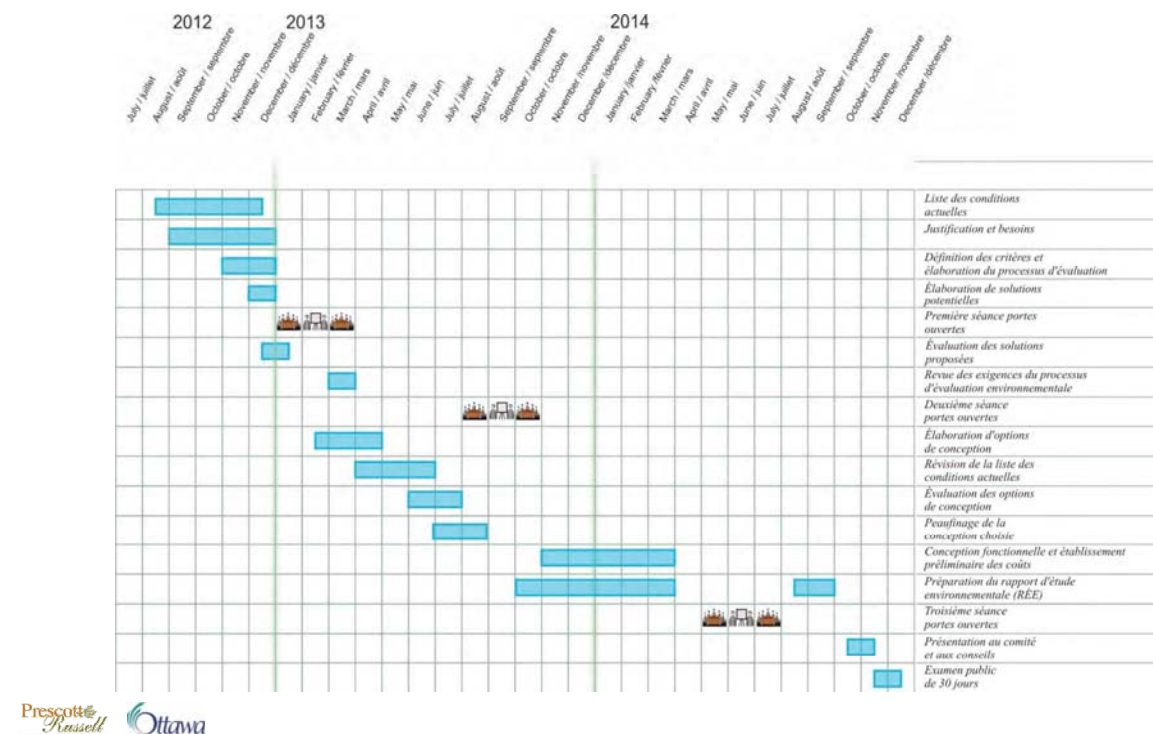
Zone d'étude



Processus d'étude



Échéancier de l'étude

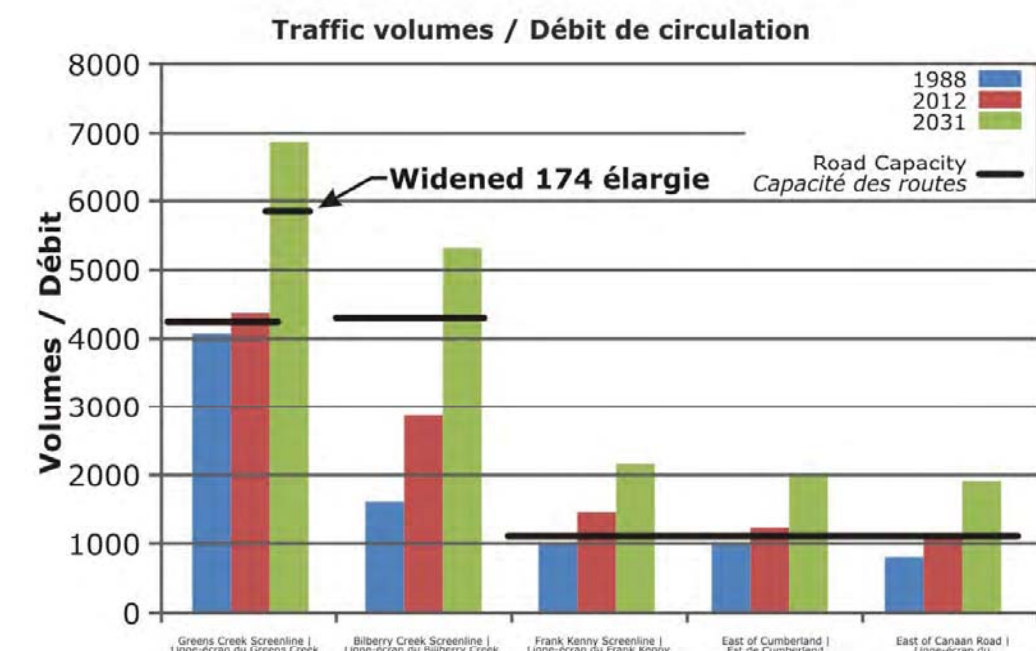


Évaluation des besoins

Survol

- Nous observons des problèmes de transport dans ce secteur depuis de nombreuses années. L'évaluation des besoins de cette étude a été effectuée en fonction de rapports antérieurs et de nouveaux renseignements afin d'évaluer :
 - la capacité routière actuelle et future;
 - les possibilités d'amélioration de la sécurité; et
 - l'incidence des nouveaux aménagements et de l'étalement du milieu urbain.

Évolution du débit et de la capacité

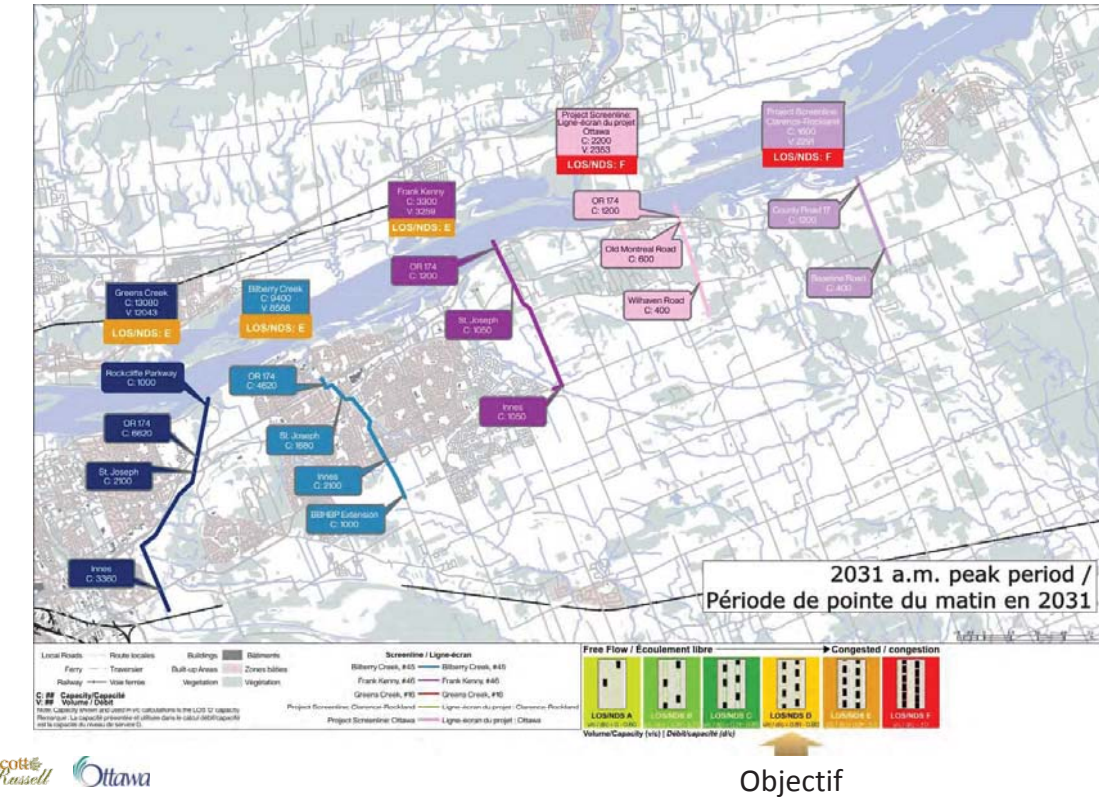


Capacité

- Étude, au moyen de lignes-écrans, du débit de circulation à l'heure de pointe matinale prévu pour 2031
 - Si on suppose que toutes les nouvelles infrastructures routières et du transport en commun prévues pour 2031 sont en place
- Prise en compte des limites de capacité en aval
 - Autoroute 417
- Évaluation de la sensibilité à l'utilisation du transport en commun
 - Augmentation ou diminution de 5 % et de 10 %, respectivement
- Même avec une utilisation accrue du transport en commun et la mise en œuvre d'autres stratégies de réduction des déplacements, une plus grande capacité sera requise à partir de la jonction jusqu'à Rockland.

Sécurité

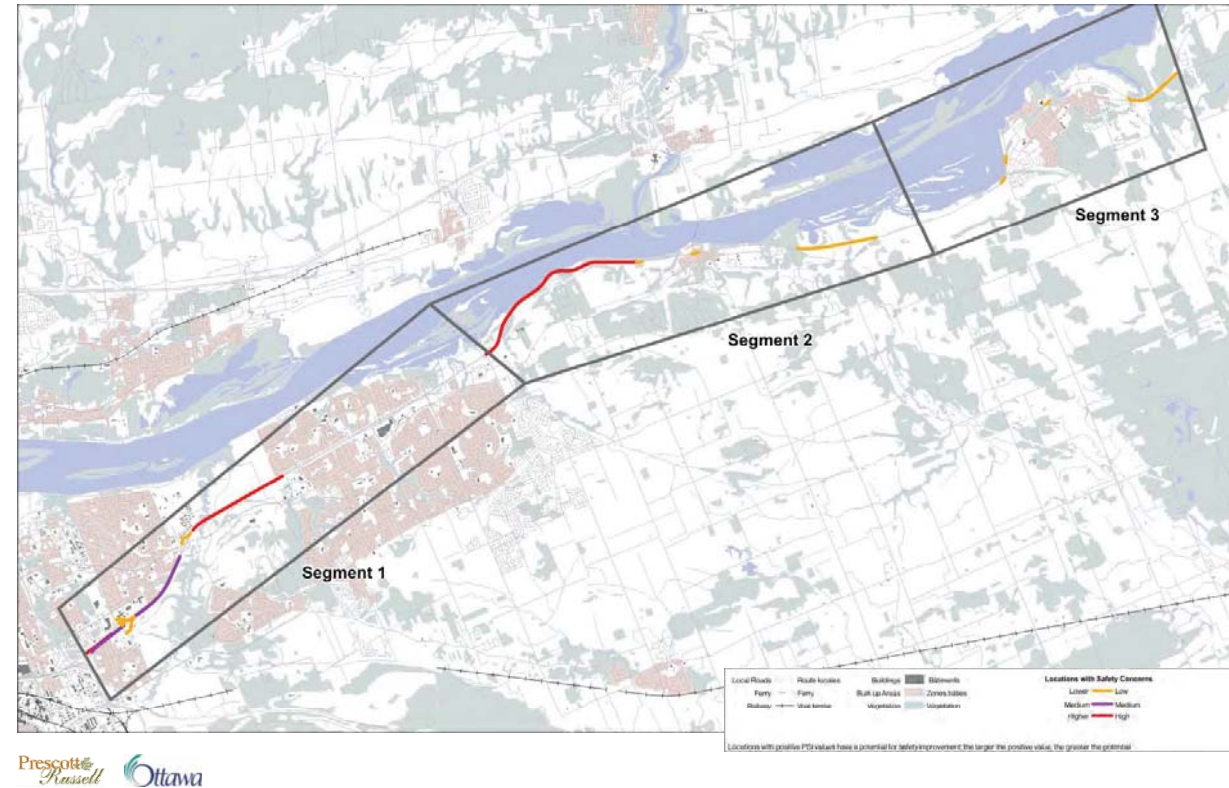
- Étude des données récentes sur les collisions
 - Jonction au chemin Trim : 1 487 collisions
 - Entre le chemin Trim et le chemin Canaan : 465 collisions
 - Entre le chemin Canaan et la rue Landry : 287 collisions
- Caractéristiques étudiées
 - Gravité de la collision (dommages matériels seulement, blessures ou mort)
 - Type d'impact (virage, angle, arrière, frottements latéraux, un seul véhicule automobile)
 - Luminosité (lumière du jour, obscurité, pénombre à l'aube ou au crépuscule)
 - Conditions météorologiques (temps dégagé, pluie, neige)
 - Période de la journée (heures de pointe du matin ou de l'après-midi, mi-journée, soir, petit matin)
- Analyse des caractéristiques de la route (courbes horizontales ou verticales, largeur des voies, possibilités de dépassement, accotement)



Sécurité

(suite)

- Éléments influençant grandement le résultat d'une collision sur la route 174/17
 - Vitesse, environnement paysager, accès, configuration des intersections et des échangeurs, éclairage, orientation positive, possibilités de dépassement, adaptation au transport en commun
- Principales mesures pour améliorer la sécurité
 - Réduction de la congestion
 - Responsabilisation des conducteurs
 - Modification des intersections
 - Amélioration de l'éclairage



Conditions actuelles

Conclusions

- Il faut offrir une plus grande capacité routière.
- Il est possible d'améliorer la sécurité du couloir de la route 174 et de la route de comté 17.

Survol

- Des données environnementales de référence ont été recueillies; elles serviront à évaluer les effets d'installations de transport sur l'environnement et permettront ainsi :
 - de proposer des solutions raisonnables;
 - de déterminer leurs effets;
 - de les évaluer.
- Les données seront mises à jour au besoin, à mesure que l'étude avancera et que des précisions seront jugées nécessaires.

Zone d'étude



Solutions possibles

État actuel

- Environnement naturel
 - Éléments naturels importants, cours d'eau, espèces en péril
- Environnement social
 - Utilisation du sol, politiques, archéologie et patrimoine, points de vue et panoramas
- Transports
 - Transport en commun, routes, sentiers, pistes cyclables, débit et capacité de circulation, niveau de service
- Économie
 - Analyse de la conjoncture économique

Solutions possibles

- Ne rien faire
- Gestion de la demande en transport (GDT)
- Gestion du réseau de transport (GRT)
- Augmentation de la capacité routière
 - Élargissement des routes existantes
 - Nouvelles routes
- Combinaison de solutions
- Solutions applicables à différentes portions du couloir ou à son ensemble



Évaluation

- Les critères ci-dessous ont été définis afin d'évaluer la valeur de chacune des multiples solutions envisagées.
 - **Satisfaction des besoins du projet** : la solution permet d'augmenter la capacité routière et la sécurité.
 - **Respect des politiques, règlements et normes de pratique locales** : la solution n'enfreint ni les politiques provinciales et fédérales, ni les règlements et politiques municipaux.
 - **Prise en compte des répercussions sur l'environnement** : les effets majeurs de chaque solution sur les diverses conditions environnementales ont été évalués. L'absence de répercussions ou la présence d'effets limités a été prise en compte.



Évaluation de la GDT

- Les mesures seules ne répondent pas à nos besoins.
- Les mesures de GDT sont intégrées à une solution globale.



Mesures de GDT

- La gestion de la demande en transport (GDT) consiste à appliquer des stratégies et des politiques de réduction de la demande en transport et de répartition de cette demande dans le temps. Voici quelques exemples :
 - Accorder la priorité au transport en commun
 - Rendre la marche et le vélo plus attrayants
 - Promouvoir le télétravail
 - Sensibiliser le public



Mesures de GRT

- La gestion du réseau de transport (GRT) est une approche visant à optimiser la sécurité, l'efficacité et la capacité du réseau de transport en place, au moyen d'améliorations efficaces et peu dispendieuses. Voici quelques exemples :
 - Voies de dépassement et voies à sens réversible
 - Panneaux et feux de signalisation, marques sur la chaussée et réglementation
 - Optimisation de la signalisation
 - Surveillance par caméra
 - Système de renseignements pour les usagers de la route
 - Localisation des véhicules de transport en commun

Évaluation de la GRT

- L'aménagement de voies de dépassement ne répond pas aux besoins de capacité et n'a pas été retenu.
- Des voies à sens réversible peuvent être aménagées dans les secteurs urbains où la vitesse de circulation est faible, mais elles ne conviennent pas en milieu rural. La vitesse de circulation et la présence d'entrées augmentent les risques pour les automobilistes et les résidents. Cette mesure a été rejetée.
- Les autres mesures de GRT sont intégrées à une solution globale.

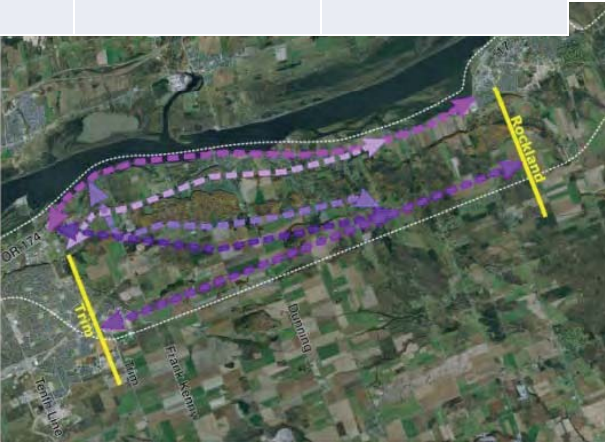
Sélection – De la jonction au chemin Trim

Élargissement de la route 174	Élargissement du chemin Innes (de six à huit voies)	Élargissement du boulevard Saint-Joseph	Aménagement d'un nouveau circuit
Répond au besoin tout en ayant des effets limités sur l'environnement par rapport aux autres options. Solution retenue.	N'est pas réalisable sur le plan opérationnel à l'ouest de la Ceinture de verdure. Solution rejetée.	Offre une moins grande capacité que l'élargissement de l'autoroute et a une grande incidence sur la collectivité d'Orléans. Solution rejetée.	N'est pas conforme aux politiques de la CCN et du Plan directeur de la Ceinture de verdure. Solution rejetée.
✓	✗	✗	✗



Sélection – Du chemin Trim à Rockland

Élargissement de la route 174/17	Élargissement du circuit entre le chemin Innes et le chemin Baseline ou aménagement d'un nouveau circuit	Élargissement du chemin Old Montreal entre le chemin Trim et la route 174 Est	Élargissement de la promenade Wihaven et du chemin Frank Kenny	Aménagement d'un nouveau circuit vers le chemin Baseline au sud de la promenade Wihaven
Répond aux besoins. Solution retenue.	Répond au besoin s'il y a aménagement d'une liaison adaptée vers la route 174. Solution retenue.	Rejeté en raison de l'utilisation des terrains adjacents et de la présence d'entrées, des aménagements prévus et de la configuration.	Rejeté en raison du piètre tracé en plan, du manque de continuité et de l'utilisation des terrains adjacents.	Répond au besoin s'il y a aménagement d'une liaison adaptée vers la route 174. Solution retenue.
✓	✓	✗	✗	✓



Sélection – Rockland

Amélioration de la route 17	Élargissement des voies urbaines de Rockland	Aménagement d'un nouveau circuit au sud de Rockland
Répond aux besoins et présente quelques effets sur l'environnement, liés aux milieux humides, à l'utilisation des terrains adjacents et aux cours d'eau. Solution retenue.	N'est réalisable sur aucune route en raison de l'utilisation des terrains adjacents et de l'aménagement (le chemin Baseline étant compris dans le segment 2). Solution rejetée.	Répond au besoin en offrant un autre itinéraire pour l'aménagement continu du secteur sud de Rockland. Solution retenue.
✓	✗	✓



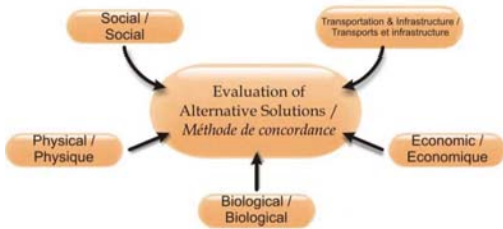
Solutions retenues

De la jonction au chemin Trim
Élargissement de la route 174 à six voies
Du chemin Trim à Rockland
Élargissement de la route 174/17 à quatre voies
Élargissement du circuit entre le chemin Innes et le chemin Baseline ou aménagement d'un nouveau circuit
Aménagement d'un nouveau circuit au sud de la promenade Wilhaven
Environs de Rockland
Amélioration de la route 17
Aménagement d'un nouveau circuit au sud de Rockland

- Nous allons peaufiner cette courte liste d'options et effectuer une évaluation plus détaillée des répercussions et des avantages de chaque solution retenue, afin de trouver la solution à privilégier.

Évaluation des solutions retenues

- La méthode utilisée pour l'évaluation des solutions retenues visera les objectifs suivants :
 - Utilisation de critères
 - Pondération ou classement
 - Analyse de sensibilité



Prochaines étapes

Critères

- Les critères d'évaluation sont des éléments particuliers utilisés aux fins de comparaison. La définition de ces critères doit être faite en fonction :
 - de la nature du projet;
 - des attentes des intervenants;
 - d'une bonne compréhension des conditions actuelles;
 - de la délimitation des secteurs;
 - des problèmes liés au projet proposé.
- L'équipe d'étude collaborera avec les intervenants afin de définir les critères qui seront utilisés dans le cadre de cette évaluation.
- Faites-nous part des éléments que vous trouvez importants et qui devraient être pris en compte dans la sélection de la solution.

Criteria Group Groupes de critères	Examples of Criteria Exemples de critères	
Biological Conditions Milieu biologique	• Wildlife • Fisheries • Watercourses • Woodlots • Vegetation	• Faune • Pêches • Cours d'eau • Boisés • Végétation
Social Conditions Milieu social	• Community Linkages • Archaeology • Economic Activity • Noise • Recreation • Land requirements/use	• Liens communautaires • Archéologie • Activité économique • Bruit • Loisirs • Exigences et utilisation des terres
Physical Conditions Milieu physique	• Soils • Bedrock • Groundwater • Drainage	• Sols • Fondation rocheuse • Eaux souterraines • Drainage
Transportation & Infrastructure Transports et infrastructure	• Transit Ridership • Network/System Integration • Level of Service • Geometry • Capacity • Operation	• Achalandage du transport en commun • Intégration du réseau ou du système • Niveau de service • Géométrie • Capacité • Opération
Economic Economique	• Capital Cost • Operational and Maintenance Costs • Business Impacts	• Coût en capital • Coûts de fonctionnement et d'entretien • Impact sur les entreprises

Consultation

- Rencontres avec le GCP, le GCE et le GCO
- Séances portes ouvertes, présentations, feuilles de commentaires, questionnaires et autres observations écrites
- Avis dans les journaux et lettres d'information envoyées aux organismes, aux intervenants, etc.
- Rencontres individuelles
- Séance de consultation pour les Premières nations
- Couverture médiatique et en ligne
 - www.ottawa.ca/etuderoute174et17
 - <http://www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure>
- Bulletins d'information
- Présentations aux comités et aux conseils des CUPR et d'Ottawa
- Examen public officiel du RÉE

Des questions?

Veuillez vous nommer et dire d'où
vous venez.

Welcome

Welcome to the Open House for the Ottawa Road 174 / Prescott-Russell County Road 17 Environmental Assessment Study.

Tonight you will have the opportunity to learn and comment on the:

- Environmental assessment process for the study;
- Existing conditions within the study area;
- Alternative Solutions;
- Short List of Alternative Solutions; and
- Next steps in the study process.

Representatives from the United Counties of Prescott-Russell, the City of Ottawa, and the consulting team are here to answer your questions.

Bienvenue

Bienvenue à cette séance portes ouvertes sur l'étude d'évaluation environnementale de la route 174 et de la route de comté 17 de Prescott-Russell.

Ce soir, vous aurez l'occasion d'en apprendre plus et d'émettre des commentaires à propos :

- Du processus d'évaluation environnementale dans le cadre de l'étude;
- Des conditions actuelles du secteur à l'étude;
- Des solutions possibles;
- De la liste restreinte des solutions de rechange retenues; et
- Des prochaines étapes du processus d'étude.

Plusieurs représentants des Comtés unis de Prescott et Russell et de la Ville d'Ottawa ainsi que des membres de l'équipe de consultants sont ici ce soir pour répondre à toutes vos questions.

Background

- OR 174 and CR 17 were formerly Provincial Highway 17 and were transferred to the municipalities in 1997
- City of Ottawa has planned 174 improvements:
 - 2009 to 2015 Widen from 5 to 6 lanes from Highway 417 to Blair Road
 - 2016 to 2022 Widen from 4 to 6 lanes from Blair to Jeanne d'Arc Blvd
- Challenges
 - Transportation problems (congestion and safety) have been identified for many years
 - Natural geographic constraints (e.g., Ottawa River and Mer Bleue Bog) limit the alternative east-west transportation routes
 - OR 174/CR17 is the only high capacity arterial in the area
 - Growth in Orléans, Cumberland and Rockland has continued to increase traffic volumes

The United Counties of Prescott and Russell, in partnership with the City of Ottawa, have initiated a Schedule C Class Environmental Assessment study for the roadway.

Contexte

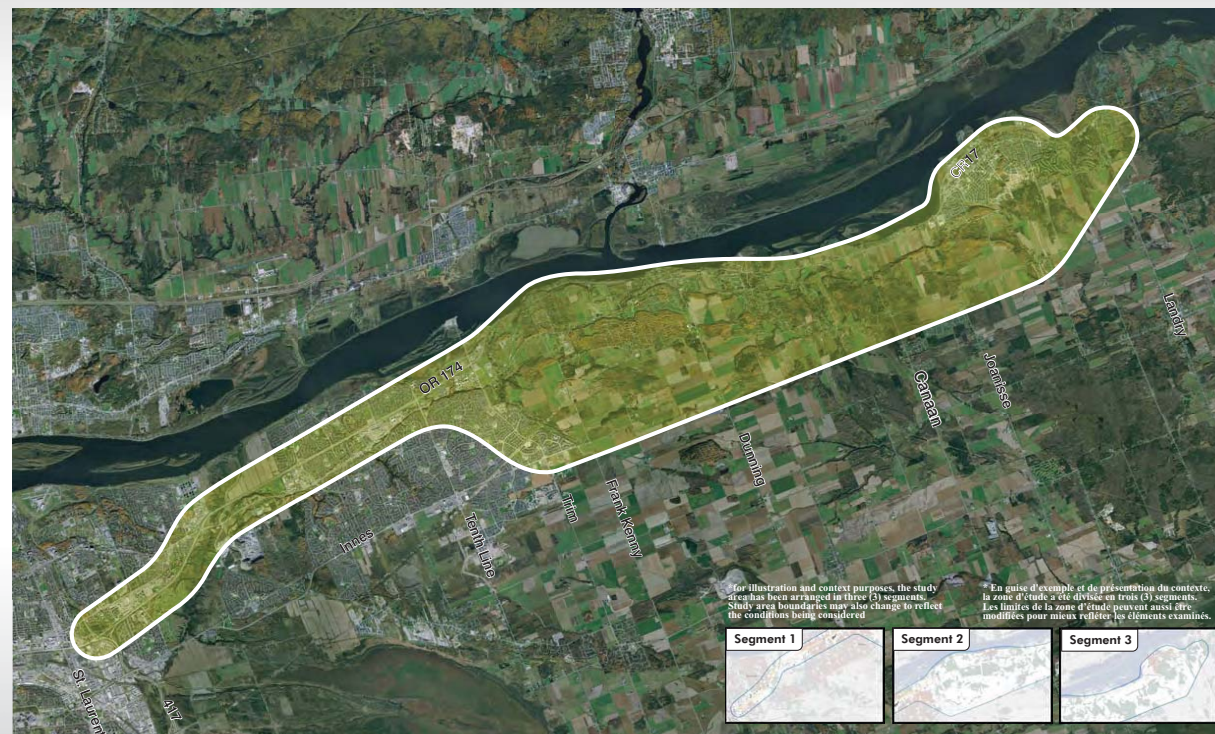
- Avant d'être transférées sous juridiction municipale en 1997, la route 174 et la route de comté 17 formaient autrefois la route provinciale 17
- La Ville d'Ottawa prévoit apporter les améliorations suivantes à la route 174 :
 - Élargissement de cinq à six voies de la portion située entre l'autoroute 417 et le chemin Blair (2009-2015)
 - Élargissement de quatre à six voies de la portion située entre le chemin Blair et le boulevard Jeanne d'Arc (2016-2022)
- Défis
 - Des problèmes de transport (liés à la congestion routière et à la sécurité) sont connus depuis de nombreuses années
 - Des contraintes géographiques naturelles (p. ex., la rivière des Outaouais et la tourbière de la Mer Bleue) limitent le nombre de voies de transport possibles sur l'axe est-ouest
 - La route 174 et la route de comté 17 constituent la seule artère de grande capacité dans le secteur
 - La croissance qu'ont connue Orléans, Cumberland et Rockland a entraîné une hausse du débit de circulation

Les Comtés unis de Prescott et Russell, en collaboration avec la Ville d'Ottawa, ont entrepris une étude d'évaluation environnementale de la route, conformément à l'annexe C du document sur les évaluations environnementales municipales de portée générale.



Study Area

Zone d'étude

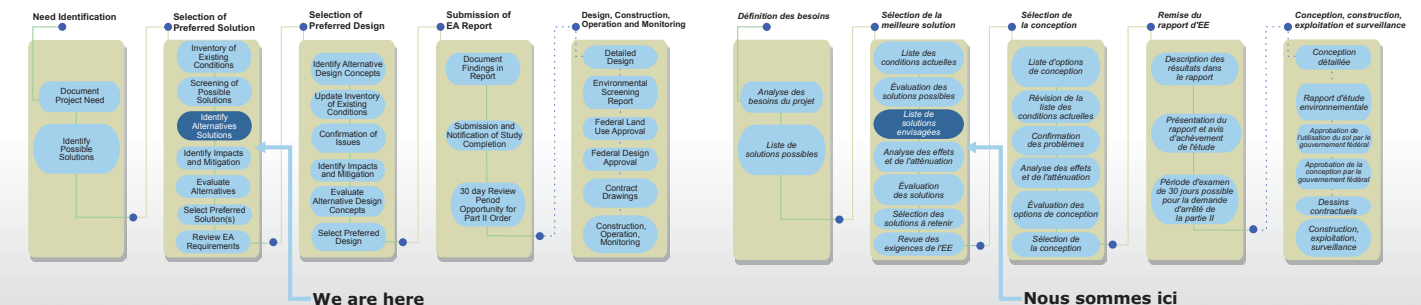


Planning and environmental assessment process

In accordance with the Municipal Class Environmental Assessment (2011), the widening of a road or construction of a new road/paved facility over \$2.3M is a Schedule "C" project. Schedule "C" projects require the preparation of an Environmental Study Report which will be made available for public review.

Processus de planification et d'évaluation environnementale

Selon l'évaluation environnementale municipale de portée générale (2011), l'élargissement d'une route ou la construction d'une nouvelle route ou chaussée revêtue, à un coût de plus de 2,3 M\$, est un projet visé par l'annexe C. Tout projet de l'annexe C requiert la préparation d'un rapport d'étude environnementale qui sera mis à la disposition du public aux fins d'examen.



Consultation

Effective consultation will play a key role in the success of this project. Three separate Consultation Groups representing various agency, business and public stakeholders, will provide direct input and feedback during the course of the study.

Consultation with the following groups is occurring throughout the study:

Agency Consultation Group (ACG), comprised of representatives from federal, provincial, municipal and other approval authorities and agencies.

Business Consultation Group (BCG), comprised of representatives from local businesses, business associations and institutions.

Public Consultation Group (PCG), comprised of representatives from city councillors, community associations and special interest groups.

General Public

The General Public will have the opportunity to build awareness, knowledge and understanding of the study through:

- Public Open Houses and Presentations
- Media Coverage
- Newspaper Notices
- Study Reports
- Comment Sheets-Questionnaires
- Written Submissions
- Study Website and Email

Consultation

La consultation efficace jouera un rôle déterminant dans la réussite de ce projet. Trois groupes de consultation représentant respectivement des groupes d'intervenants, différents organismes et différentes entreprises, interviendront et émettront des commentaires directement tout au long de l'étude.

Voici les trois groupes :

Groupe de consultation d'organismes (GCO), constitué de représentants provenant des sphères fédérale, provinciale et municipale et d'autres autorités et organismes responsables de l'approbation.

Groupe de consultation des entreprises (GCE), formé des représentants des entreprises, des associations d'entreprises et des institutions locales.

Groupe de consultation publique (GCP), composé de représentants des conseillers municipaux, des associations communautaires et des groupes d'intérêts spéciaux.

Grand public

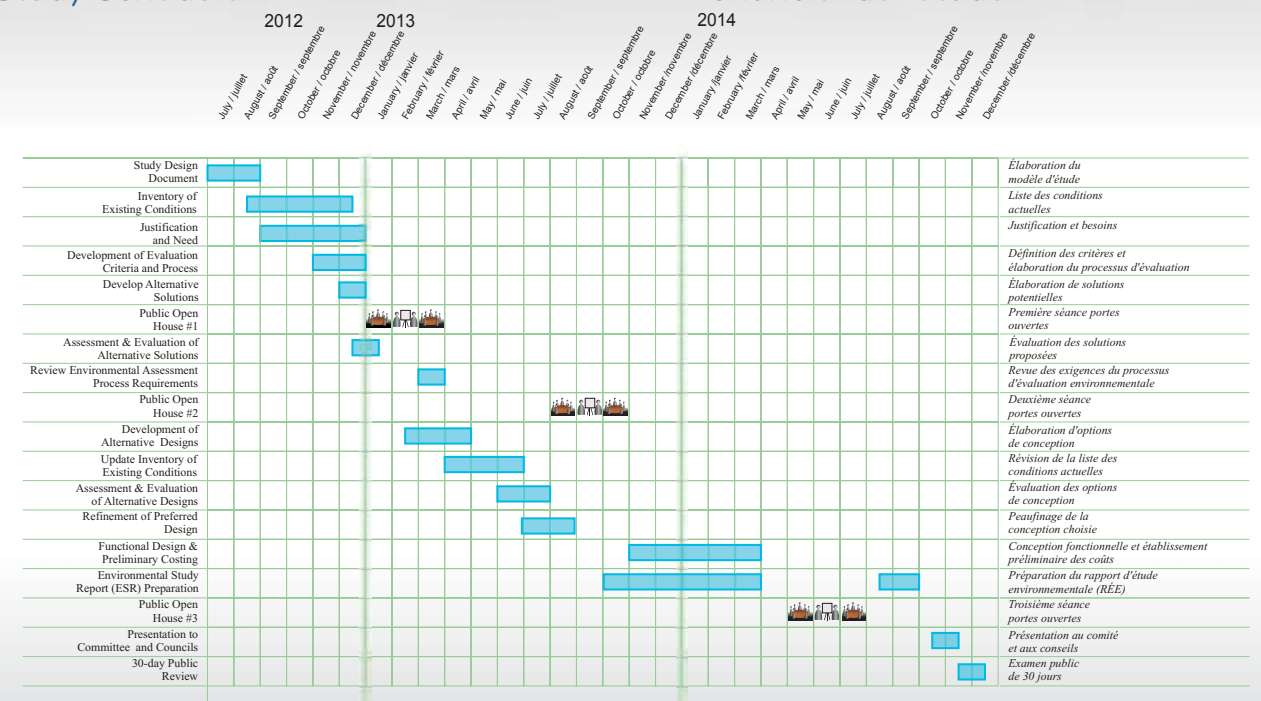
Le grand public aura l'occasion d'en apprendre plus sur le projet par les moyens suivants :

- Séances portes ouvertes et présentations
- Couverture médiatique
- Avis dans les journaux
- Rapports d'études
- Formulaires de commentaires
- Demandes écrites
- Site Web du projet et courriel



Study Schedule

Échancier de l'étude





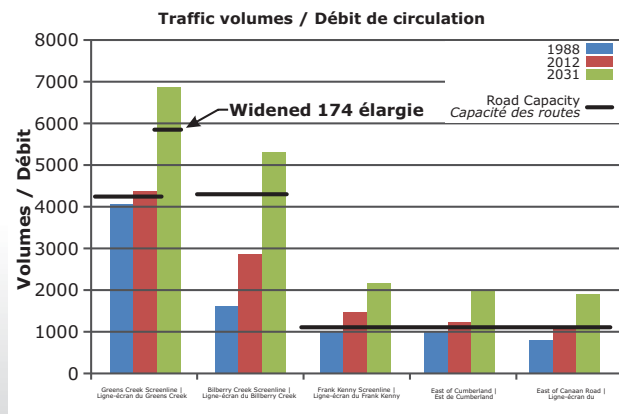
Needs Assessment Overview

Transportation problems in the area have been identified for many years. The Needs Assessment for this study has reviewed previous reports and utilized new information to assess:

- Existing and future transportation (road and transit) capacity
- Safety improvement opportunities
- Impacts of new developments and expanded urban areas

- Increasing traffic volumes between 1988 and 2031
- Capacity of the road has been exceeded when the volume bars are above the capacity lines

Screenline - a line that crosses major transportation facilities in a sector for the purpose of analyzing capacity. These lines are typically drawn along a feature (river or railway) that limits the number of crossing points available.



- On prévoit une augmentation du débit de circulation entre 1988 et 2031
- Le débit excède la capacité routière lorsque les colonnes dépassent la ligne représentant cette capacité

Ligne-écran - Ligne tracée en travers des grandes infrastructures de transport d'un secteur dans le but d'évaluer sa capacité. Ces lignes suivent habituellement le tracé d'une caractéristique géographique (une rivière ou un chemin de fer) qui limite le nombre de points d'intersection possibles.



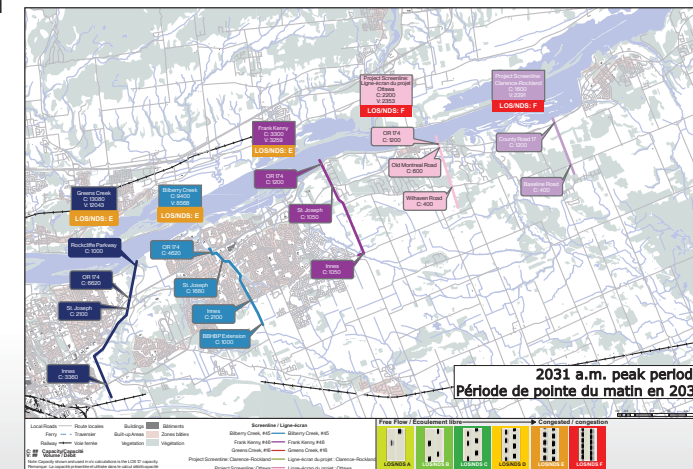
Needs Assessment - Capacity

A key component of the study was to evaluate the need to provide transportation infrastructure in the Study Area to meet the current and future transportation requirements.

The analysis considered:

- Existing travel conditions at key interchanges and specific screenlines
- Various travel modes across these screenlines
- Targets for transit and other non-auto travel modes in the relevant Official Plans
- Future travel conditions related to population growth targets
- Future vehicular volume capacities of the screenlines
- Strategies to address deficiencies in screenline capacity

There is a need to provide additional transportation capacity.



L'analyse a tenu compte :

- Des conditions actuelles de déplacement aux principaux échangeurs et à certaines lignes-écrans
- Des divers modes de déplacement qui traversent ces lignes-écrans;
- Des objectifs des Plans officiels pertinents en matière de transport en commun et de moyens de transport autres que l'automobile
- Des futures conditions de déplacement liées au nouveau niveau de croissance démographique
- De la capacité future de circulation des lignes-écrans
- Des stratégies pour résorber les déficits de la capacité des lignes-écrans

Il est nécessaire d'augmenter la capacité routière actuelle du secteur.



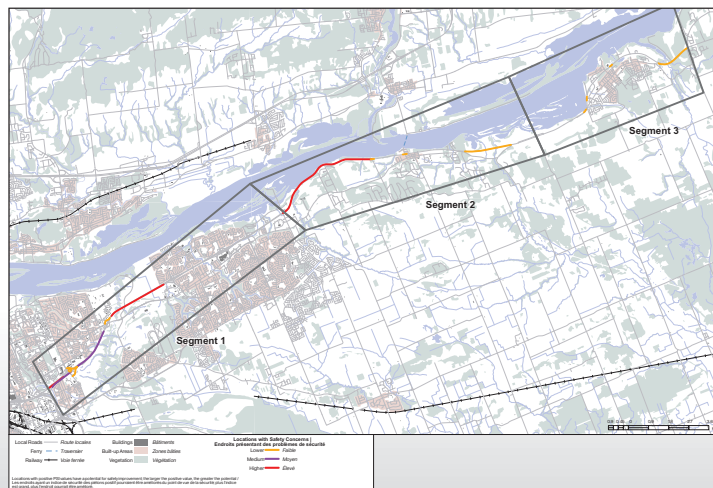
Needs Assessment - Safety

Another key component of the study was to evaluate safety concerns.

The analysis of 174/17 considered:

- Review of existing safety reviews and reports
 - Split to Trim - 1487 collisions
 - Trim to Canaan - 465 collisions
 - Canaan to Landry - 287 collisions
- Comparison of the predicted number of collisions for a similar type of roadway and the expected number of collisions based on actual collision experience and statistical analysis
- Significant contributors to collision experience:
 - Speed, roadside character, access, interchange and intersection geometry, lighting, positive guidance, passing opportunities, accommodating transit

There are opportunities for safety improvements in the OR 174-CR17 corridor.



L'analyse de la route 174/17 a tenu compte :

- De la revue des examens et rapports de sécurité existants :
 - Entre la jonction et le chemin Trim - 1 487 collisions
 - Entre le chemin Trim et le chemin Canaan - 465 collisions
 - Entre le chemin Canaan et le chemin Landry - 287 collisions
- De la comparaison entre le nombre de collisions prévu sur une route du même type et le nombre de collisions attendu en fonction des collisions qui ont eu lieu et de l'analyse statistique
- Des principales causes des collisions :
 - La vitesse, l'environnement paysager, l'accès, la configuration des intersections et des échangeurs, l'éclairage, l'orientation positive, les possibilités de dépassement et l'adaptation au transport en commun

Il existe des possibilités d'amélioration de la sécurité dans le couloir de la route 174/17.



Existing conditions overview

Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by a transportation facility. The information will be used to:

- Develop reasonable alternatives
- Identify impacts
- Evaluate alternatives

Information will be updated, as necessary, as the study progresses and more detailed information is required.

Survol des conditions actuelles

Les conditions environnementales de départ ont été documentées pour tenter d'évaluer l'effet sur l'environnement d'un service de transport.

Les renseignements seront utilisés pour :

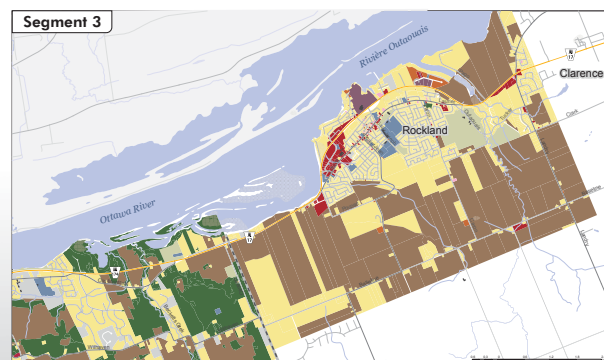
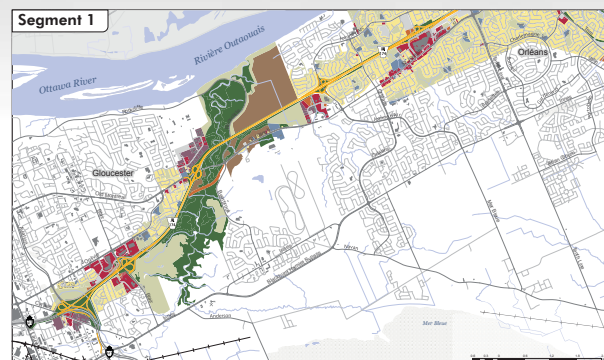
- Trouver des solutions de rechange raisonnables
- Estimer les effets
- Évaluer les solutions

Les renseignements seront mis à jour, au besoin, au fur et à mesure que l'étude avancera et que des renseignements détaillés seront nécessaires.

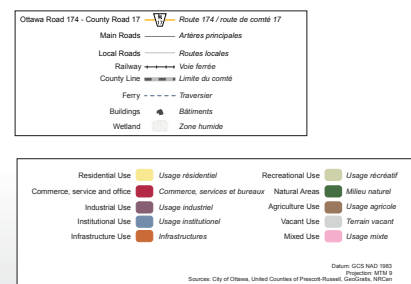
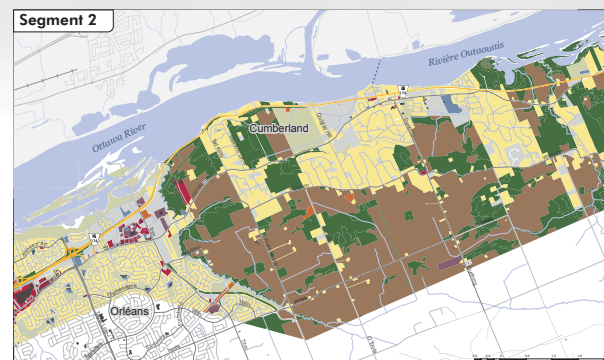




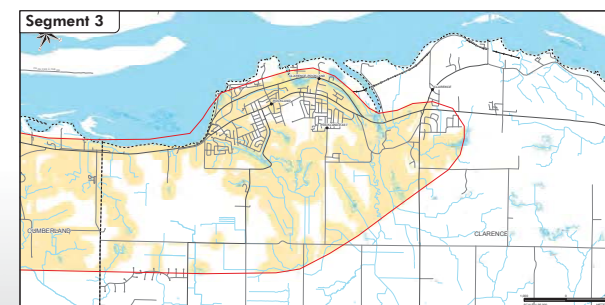
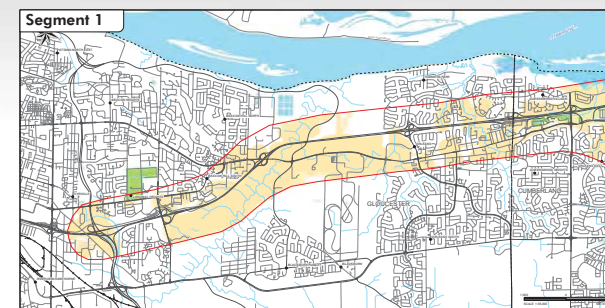
Social Environment - Land Use



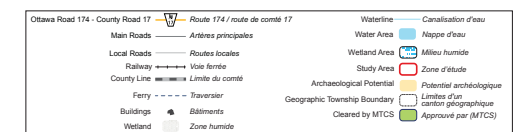
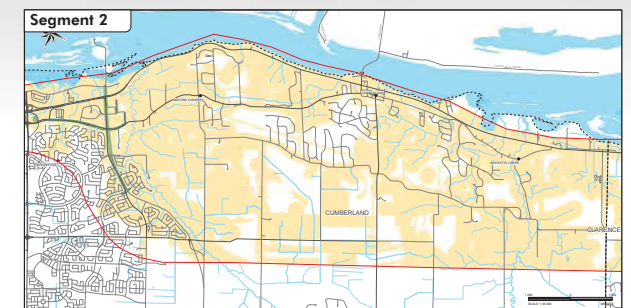
Milieu social - Utilisation du sol



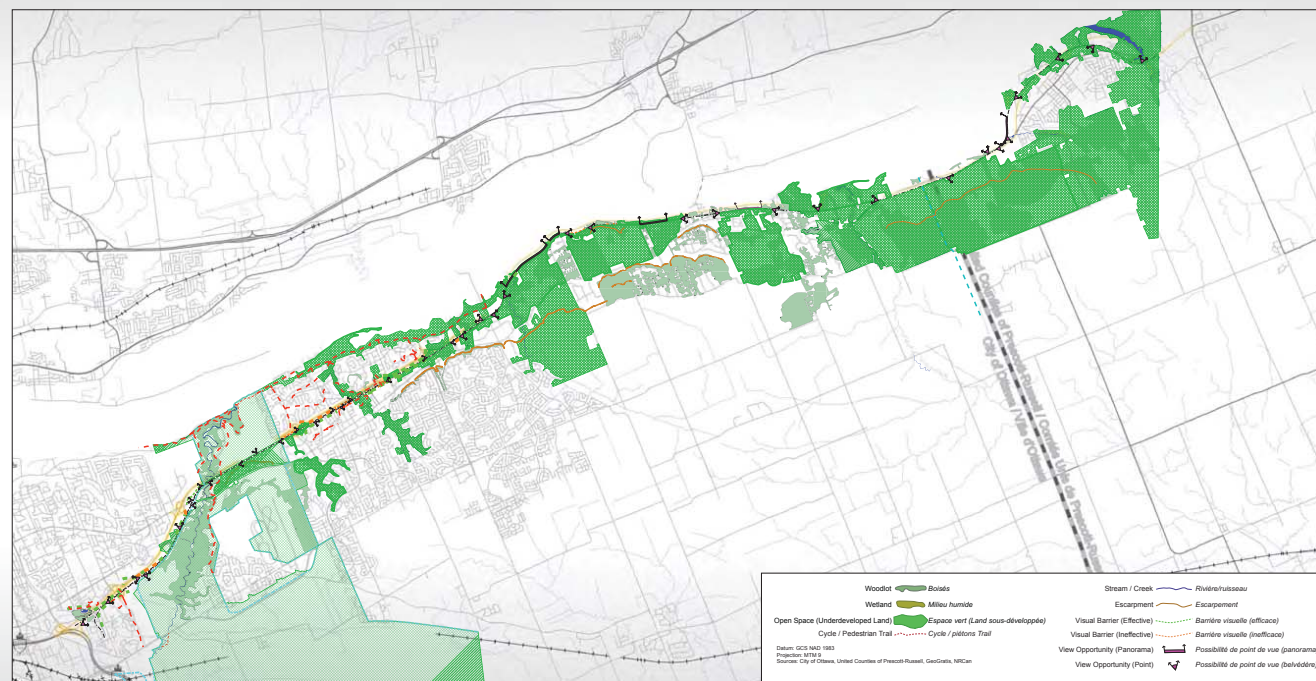
Social Environment - Archaeology Potential



Milieu social - Potentiel archéologique



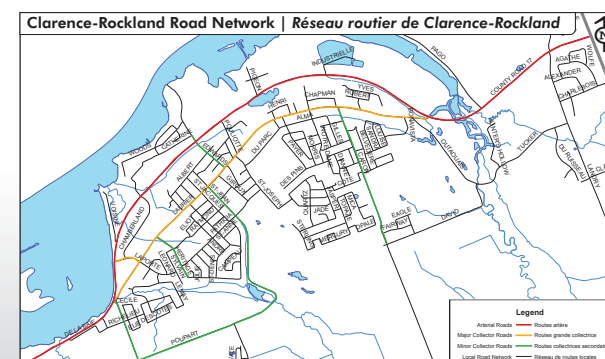
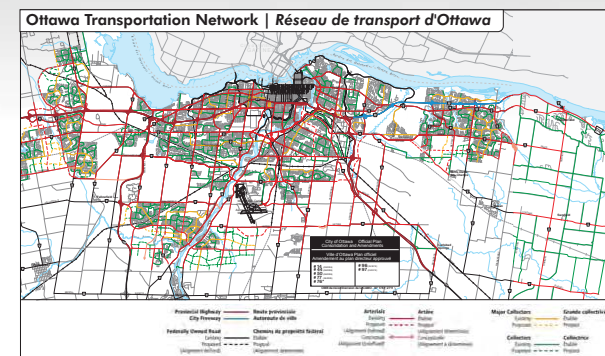
Social Environment - Views & Vistas



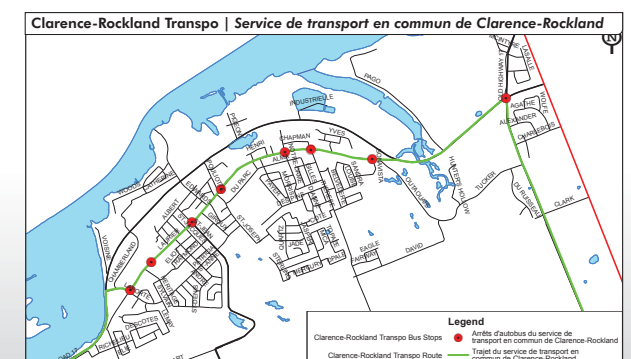
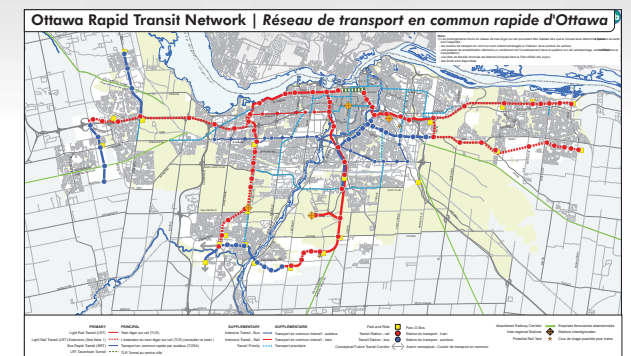
Milieu social - Belvédères et points de vue



Transportation Environment

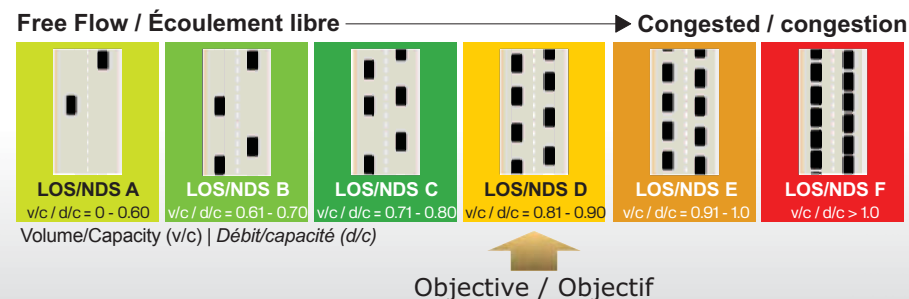
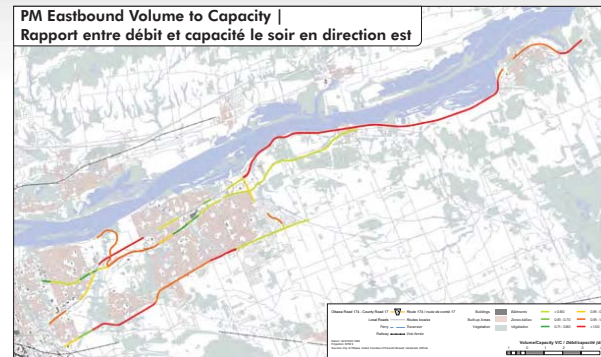
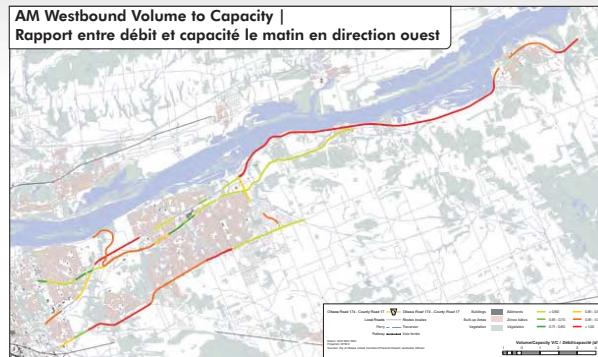


Milieu transport



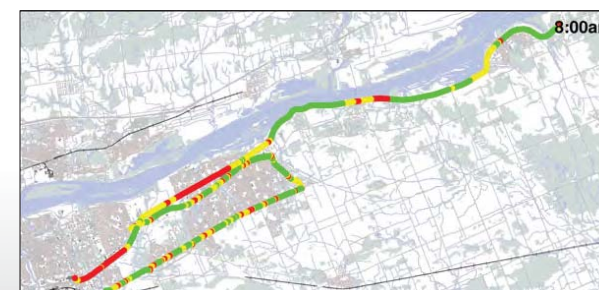
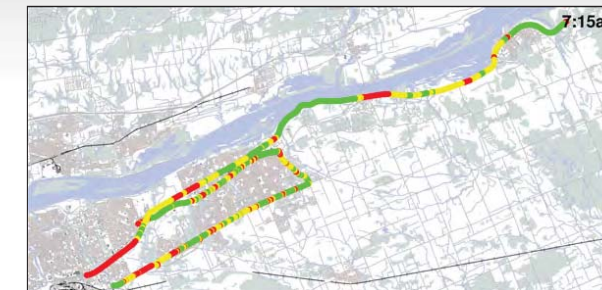
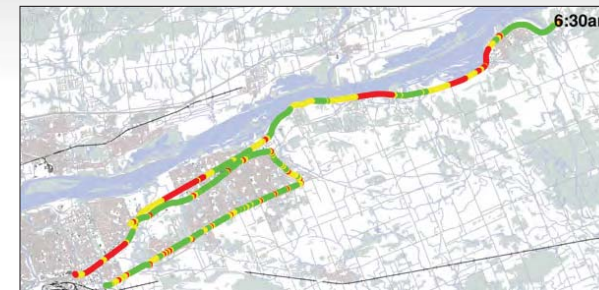
Transportation Environment Volume to Capacity

Milieu transport Rapport entre débit et capacité



Transportation Environment AM Westbound Speeds

Milieu transport - Vitesse le matin en direction ouest

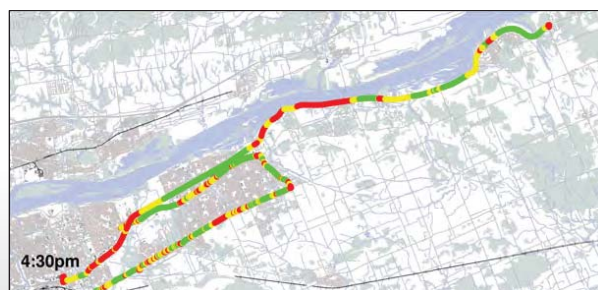
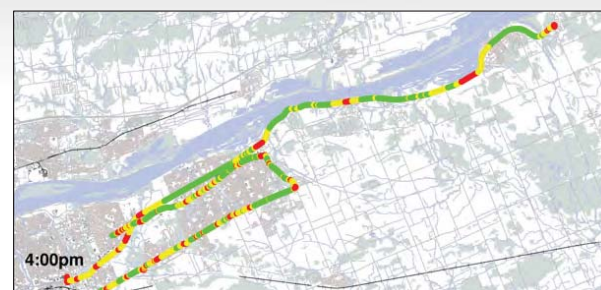
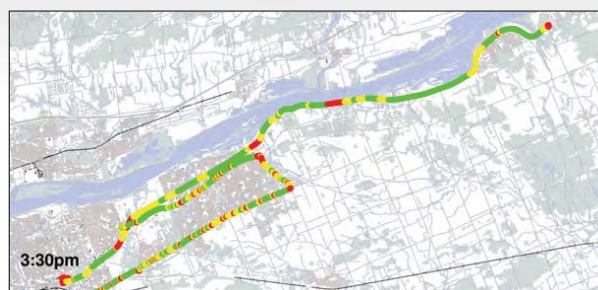


Speed Classification / Classification de la vitesse

Speed / Vitesse	Posted / Affichée 100 km/h	Posted / Affichée 80-90 km/h	Posted / Affichée 60-70 km/h
Slow / Lent	<= 60	<= 50	<= 40
Moderate / Modérée	60 - 100	50 - 80	40 - 60
Fast / Rapide	> 100	> 80	> 60

Transportation Environment PM Eastbound Speeds

Milieu transport - Vitesse le soir en direction est



Speed Classification / Classification de la vitesse

Speed / Vitesse	Posted / Affichée 100 km/h	Posted / Affichée 80-90 km/h	Posted / Affichée 60-70 km/h
Slow / Lent	<= 60	<= 50	<= 40
Moderate / Modérée	60 - 100	50 - 80	40 - 60
Fast / Rapide	> 100	> 80	> 60

Infrastructure and Utilities

- Watermains 300 - 1220 mm diameter
- Sanitary sewers 200 - 825 mm diameter
- Storm sewers >600 mm diameter
- Hydro One Stations
- Power transmissions
- Gas pipelines
- Gas mains > 100 mm diameter
- Fibre optic cables
- Culverts
- Bridges



Infrastructures et services

- Conduites d'eau principales d'un diamètre de 300 mm à 1 220 mm
- Égouts sanitaires d'un diamètre de 200 mm à 825 mm
- Égouts pluviaux d'un diamètre supérieur à 600 mm
- Stations d'Hydro One
- Transport d'électricité
- Gazoducs
- Conduites à gaz d'un diamètre supérieur à 100 mm
- Câbles à fibre optique
- Ponceaux
- Ponts



Economic Environment Business Condition Assessment

- The most prominent business type along the OR 174-CR 17 corridor is retail, followed by food, fuel and accommodation outlets
- There is a higher concentration of businesses in Gloucester and the City of Clarence Rockland than in Orléans and Cumberland
- Most prevalent business type:
 - Gloucester - Offices or Professional Services
 - Clarence Rockland – retail
- Only a small proportion of businesses are primarily served by passing traffic

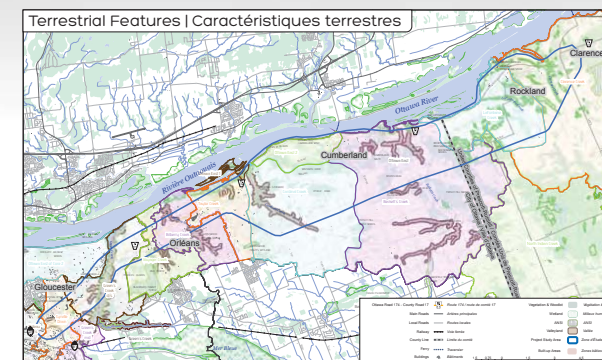
Business Type	Count
Food, Fuel and Accommodations including hotels, motels, gas stations, restaurants and fast food outlets	34
Retail Shops and malls	51
Offices/Professional Services large company offices or single service offices e.g., lawyers, accountants etc. Also including banks and financial services	44
Other any business not encompassed above, including recreation and entertainment facilities	20
Total	149

Environnement économique Évaluation de la conjecture économique

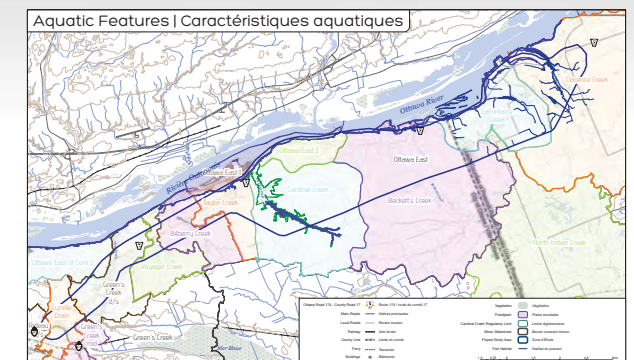
- La majorité des entreprises installées le long du couloir de la route 174/17 font partie du secteur du détail; suivent les secteurs de l'alimentation, du carburant et de l'hébergement*
- Il y a une plus grande concentration d'entreprises à Gloucester et à Clarence-Rockland qu'à Orléans et à Cumberland*
- Principal type d'entreprise :*
 - Gloucester – bureaux et services professionnels
 - Clarence-Rockland – commerces de détail
- Seule une petite fraction des entreprises profitent principalement de la circulation de passage*

Type d'entreprise	Nombre
Alimentation, carburant et hébergement Comprend les hôtels, les motels, les stations-service, les restaurants et les restaurants-minute	34
Détail Magasins et centres commerciaux	51
Bureaux et services professionnels Les bureaux de grandes entreprises ou les bureaux de service unique, p. ex., d'avocats ou de comptables. Cette catégorie englobe aussi les banques et les services financiers	44
Autre Toute entreprise qui n'appartient pas aux catégories mentionnées ci-dessus, notamment les installations de loisir ou de divertissement	20
Total	149

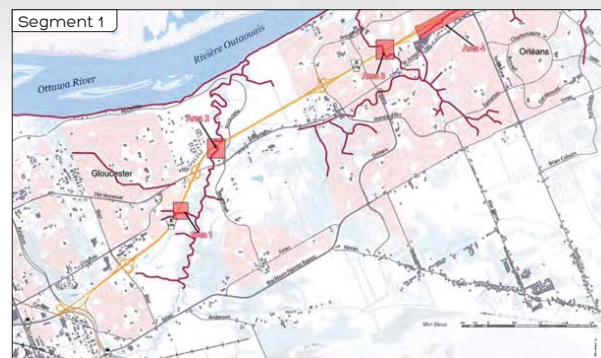
Natural Environment



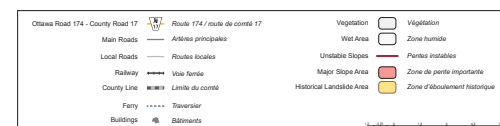
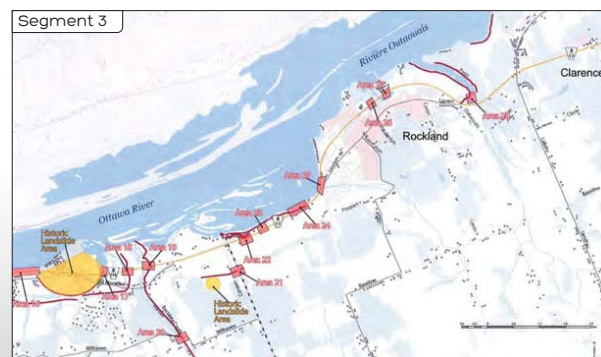
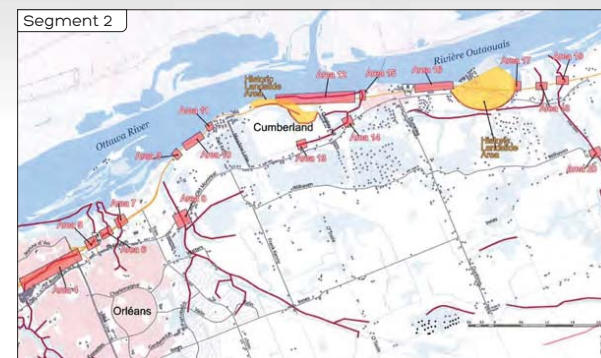
Milieu naturel



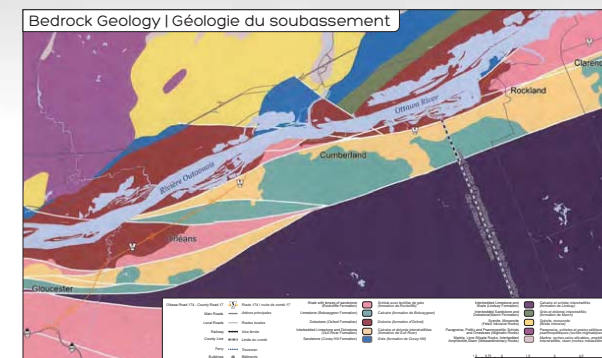
Physical Environment - Unstable Slopes



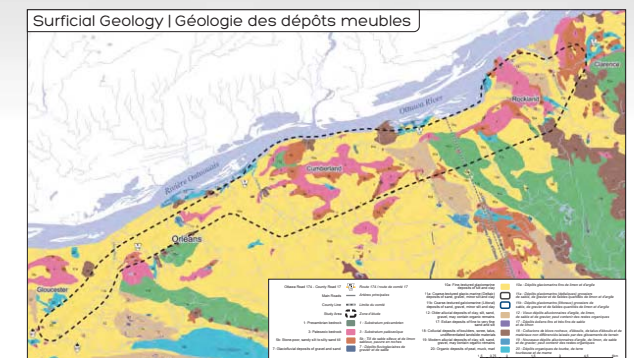
Milieu physique - Pente instables



Physical Environment



Milieu physique



Alternative Solutions

There are capacity, safety and operational deficiencies in the study area.

- Predicted traffic volumes are expected to exceed operational standards at the majority of key road crossings even with an increased transit ridership
- An arterial-type lane is required in each direction to provide additional capacity in 2031 from Highway 417 to Rockland
- There is potential for safety improvements in site-specific areas where higher than predicted collisions occur

In order to address these deficiencies a long list of alternative solutions was developed.

- Do nothing
- Enhance other modes and/or manage travel demand
- Improve the existing corridor with transportation systems management (TSM)
 - *Split to Trim Road*: add interchange at Trim Road
 - *Trim Road to Rockland*: improve transition zone cross-section; improve intersection at bottlenecks such as Cameron Street in Cumberland, Quigley Hill Rd, Old Montreal Rd, Canaan Rd, Laurier Street and Landry Road; consolidate private laneways to one or more service road(s) from Meadow Lane Road to Quigley Hill Road; add passing lanes and/or reversible lanes
- Provide additional road capacity
- Expand the 174/17 existing corridor
 - Split to Trim Road: widen freeway from 4 to 6 lanes
 - Trim Road to Rockland: widen arterial from 2 to 4 lanes
- Expand other east-west arterial roads such as Old Montreal Road, Wilhaven Drive, Frank Kenny Road, Trim Road and Innes Road
- Construct new east-west roads:
 - From Trim Road easterly up the escarpment to south of Wilhaven Drive to Baseline Road to Landry Road
 - At Rockland, southerly bypass of existing development
- Combination of the above solutions

Solutions possibles

La zone d'étude présente des lacunes en matière de capacité, de sécurité et de fonctionnement.

- *On prévoit que les débits de circulation dépasseront les normes opérationnelles à la plupart des principales traversées routières, et ce, malgré une augmentation du nombre d'usagers du transport en commun*
- *Il faudra construire une voie artérielle dans les deux directions d'ici 2031 pour augmenter la capacité de l'autoroute 417 à Rockland*
- *Il y a potentiel d'amélioration de la sécurité dans certains secteurs où survient un nombre de collisions plus grand que celui attendu*

Afin de remédier à ces lacunes, une longue liste de solutions possibles a été dressée.

- *Ne rien faire*
- *Améliorer les autres moyens de transport ou gérer la demande en transport*
- *Améliorer le couloir existant grâce à la gestion du système de transport (GST)*
 - *De la jonction au chemin Trim : ajouter un échangeur à l'intersection du chemin Trim*
 - *Du chemin Trim à Rockland : améliorer la zone de transition sur le plan transversal; améliorer les intersections aux goulots d'étranglement, comme ceux de la rue Cameron à Cumberland, du chemin Quigley Hill, du chemin Old Montreal, du chemin Canaan, de la rue Laurier et de la rue Landry; fusionner les allées privées pour en faire une ou plusieurs voies de desserte entre le chemin Meadow Lane et le chemin Quigley Hill; ajouter des voies de dépassement ou des voies à sens réversible*
- *Augmenter la capacité routière*
- *Élargir le couloir existant de la route 174/17*
 - *de la jonction au chemin Trim : élargir l'autoroute de quatre à six voies*
 - *du chemin Trim à Rockland : élargir l'artère de deux à quatre voies*
- *Élargir d'autres artères en direction est et ouest, comme le chemin Old Montreal, la promenade Wilhaven, le chemin Frank Kenny, le chemin Trim et le chemin Innes*
- *Construire de nouvelles routes en direction est et ouest*
 - *du chemin Trim à l'est en haut de l'escarpement, au sud de la promenade Wilhaven, au chemin Baseline et à la rue Landry*
 - *à Rockland, contourner l'aménagement existant par le sud*
- *Combiner les solutions ci-dessus*

Screening of Alternative Solutions

Screening criteria were developed to assess the merits of the long list of alternative solutions. The screening criteria are:

- **The ability of the alternative to address the project need.** Alternative solutions must provide transportation capacity and improve safety
- **Adherence to policies, regulations, and local standards of practice.** Alternatives should not contravene provincial/federal policies or municipal regulations or policies
- **Consideration of environmental impacts.** Alternatives were reviewed to determine their high level impacts on the various environmental conditions. The ability to avoid/reduce/minimize impacts was considered

Évaluation des solutions possibles

Des critères de sélection ont été établis pour évaluer les avantages de la longue liste de solutions possibles. Les critères de sélection sont les suivants :

- **La capacité de la solution de répondre aux besoins du projet.** Les solutions doivent offrir une meilleure capacité routière et améliorer la sécurité
- **Le respect des politiques, des règlements et des normes de pratique locales.** Les solutions doivent être conformes aux politiques provinciales et fédérales et aux politiques et règlements municipaux
- **Prise en compte des répercussions sur l'environnement.** Les solutions de rechange ont été examinées pour évaluer leurs répercussions globales sur les diverses conditions du milieu. Leur capacité d'éviter et de réduire les répercussions a été prise en considération

Screening of Alternative Solutions Strategies

Solution	Result
Do Nothing	Does not address the need
Transportation Demand Management (TDM)	Does not address the need
Transportation Systems Management (TSM)	Does not address the need
Additional road capacity	May address the need
Combination of alternatives	May address the need

TDM Screening

- Measures on their own do not address need
- TDM measures included as part of overall solution

TSM Screening

- Passing lane don't address capacity needs. Not carried forward
- Reversible lanes applicable to lower speed urban locations. Not appropriate for rural areas. Speeds and driveways create unsafe conditions for motorists and residents. Not carried forward
- Other TSM measures included as part of overall solution

Dépistage des solutions de rechange

Solution	Résultat
Ne rien faire	Ne répond pas aux besoins du projet
Gestion de la demande en transport (GDT)	Ne répond pas aux besoins du projet
Gestion du système de transport (GST)	Ne répond pas aux besoins du projet
Augmentation de la capacité routière	Peut répondre aux besoins du projet
Combinaison de solutions	Peut répondre aux besoins du projet

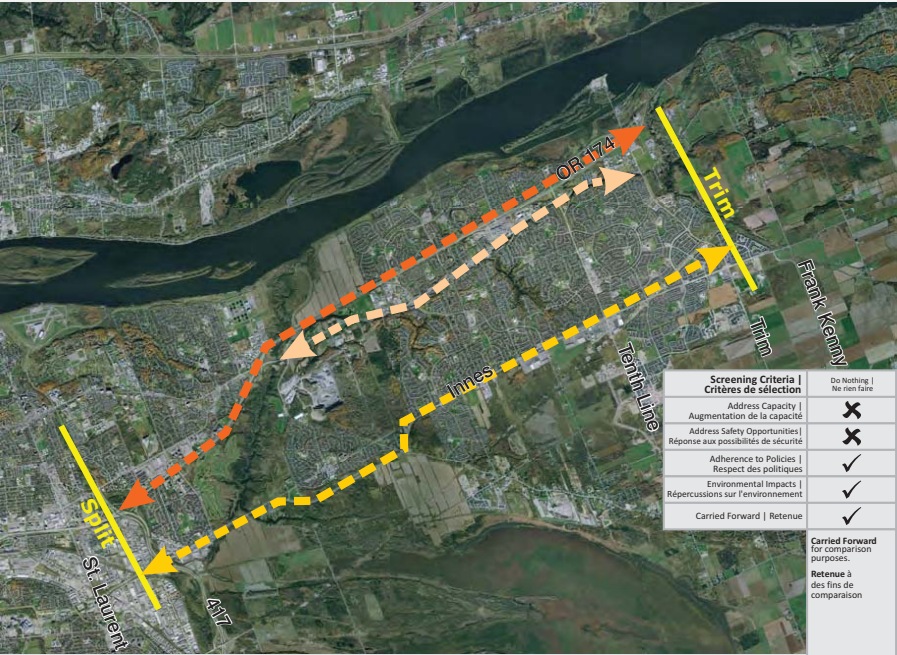
Évaluation de la GDT

- *Les mesures seules ne répondent pas aux besoins du projet*
- *Les mesures de GDT sont intégrées à une solution globale*

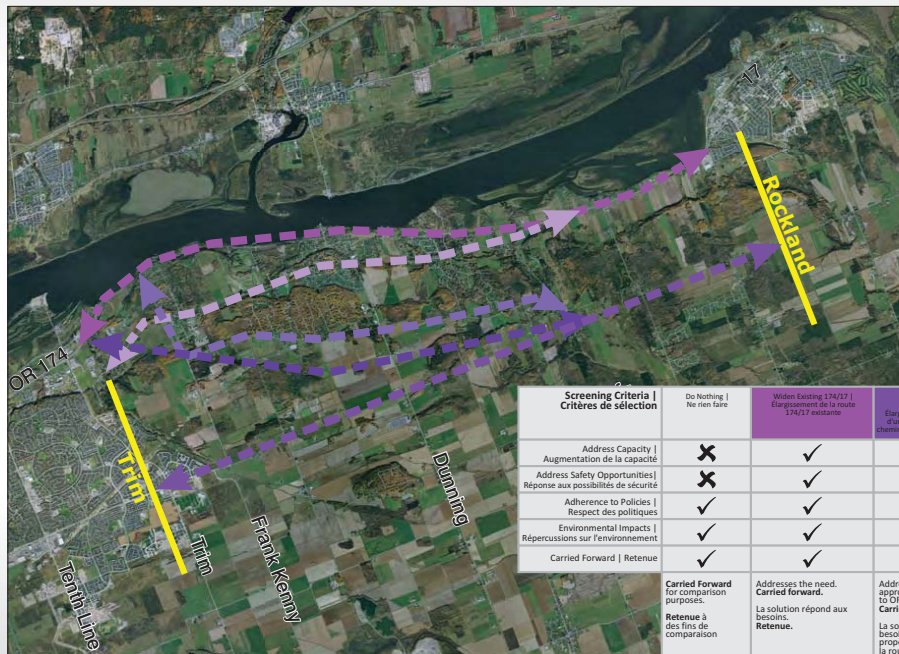
Évaluation de la GST

- *Les voies de dépassement ne répondent pas aux besoins en matière de capacité. La solution n'a pas été retenue*
- *Les voies à sens réversible conviennent aux secteurs urbains à basse vitesse, mais pas aux secteurs ruraux. La vitesse et les voies d'accès créent des conditions dangereuses pour les automobilistes et les résidents. La solution n'a pas été retenue*
- *D'autres mesures de GST sont intégrées à une solution globale*

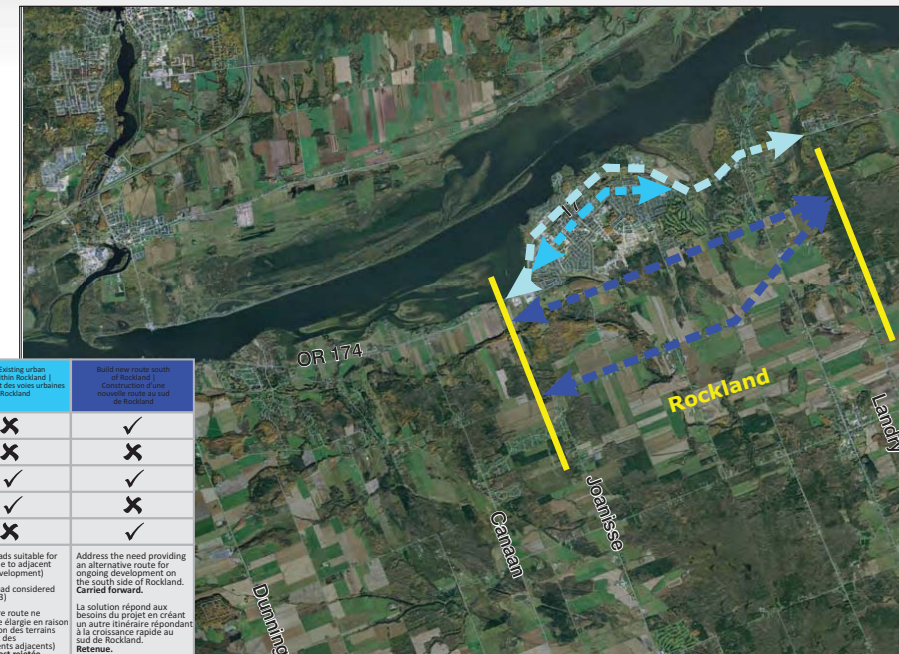
Screening of Alternative Solutions-Split to Trim Road



Screening Criteria Critères de sélection	Do Nothing Ne rien faire	Widen Existing 174/17 Élargissement de la route 174/17 existante	Widen Innes Road Élargissement de la route Innes	Widen St. Joseph Élargissement de la route St. Joseph	Build New Route Construction d'une nouvelle route
Address Capacity Augmentation de la capacité	✗	✓	✓	✓	✓
Address Safety Opportunities Réponse aux possibilités de sécurité	✗	✓	✗	✗	✗
Adherence to Policies Respect des politiques	✓	✓	✗	✗	✗
Environmental Impacts Répercussions sur l'environnement	✓	✓	✗	✗	✗
Carried Forward Retenue	✓	✓	✗	✗	✗
Carried Forward for comparison purposes. Retenue à des fins de comparaison	Addresses the need with expected limited environmental impacts when compared with other alternatives. Screen out. Carried forward.	Widening of Innes Road west of the Greenbelt from 6 to 8 lanes is not operationally feasible. Screen out.	Widening of St. Joseph Blvd. provides less capacity than freeway widening and will have significant impacts within the Orleans community. Screen out.	Widening of the boulevard Saint-Joseph offre une moins grande capacité que l'élargissement de l'autoroute et aurait une grande incidence sur la collectivité d'Orléans. La solution est rejetée.	New routes are not in keeping with the policies of the MCC and the Greenbelt Master Plan. Screen out.

Screening of Alternative Solutions-
Trim to RocklandÉvaluation des solutions de rechange -
du chemin Trim à Rockland

Screening Criteria Critères de sélection	Do Nothing Ne rien faire	Widen Existing 174/17 Élargissement de la route 174/17 existante	Widen/new route Innes-Baseline Road Élargissement ou construction d'une nouvelle route entre le chemin Innes et le chemin Baseline	Widen Old Montreal Road from Trim to 174 East Élargissement du chemin Old Montreal de Trim à 174 est	Widen Wilhaven Road and Frank Kenny Élargissement de la route Wilhaven et Frank Kenny	New route south of Wilhaven to Baseline Road Construction d'une nouvelle route au sud de la promenade Wilhaven
Address Capacity Augmentation de la capacité	✗	✓	✓	✗	✗	✓
Address Safety Opportunities Réponse aux possibilités de sécurité	✗	✓	✗	✗	✗	✗
Adherence to Policies Respect des politiques	✓	✓	✓	✓	✓	✓
Environmental Impacts Répercussions sur l'environnement	✓	✓	✓	✗	✗	✓
Carried Forward Retenue	✓	✓	✓	✗	✗	✓
Carried Forward for comparison purposes. Retenue à des fins de comparaison		Addresses the need. Carried forward. Retenue.	Addresses the need if appropriate connection to OR 174 can be developed. Carried forward.	Screen out due to adjacent land use and driveways, planned development and geometry. La solution est rejetée en raison de l'utilisation des terrains adjacents et de la présence d'entrées, des aménagement prévus et de la configuration. Retenue.	Screen out due to poor horizontal alignment, lack of continuity and adjacent land use. La solution est rejetée en raison du piètre tracé en plan, du manque de continuité et de l'utilisation des terrains adjacents. Retenue.	Addresses the need if appropriate connection to OR174 can be developed. Carried forward.

Screening of Alternative Solutions-
RocklandÉvaluation des solutions de rechange -
Rockland

Screening Criteria Critères de sélection	Do Nothing Ne rien faire	Improve 17 Amélioration de la route 17	Widen Existing urban streets within Rockland Élargissement des voies urbaines de Rockland	Build new route south of Rockland Construction d'une nouvelle route au sud de Rockland
Address Capacity Augmentation de la capacité	✗	✓	✗	✓
Address Safety Opportunities Réponse aux possibilités de sécurité	✗	✓	✗	✗
Adherence to Policies Respect des politiques	✓	✓	✓	✓
Environmental Impacts Répercussions sur l'environnement	✓	✓	✓	✗
Carried Forward Retenue	✓	✓	✗	✓
Carried Forward for comparison purposes. Retenue à des fins de comparaison		Addresses the need with some environmental impacts associated with wetlands, adjacent land use and water. Carried forward. La solution répond aux besoins du projet et présente quelques effets sur l'environnement, les notamment aux milieux humides, à l'utilisation des terrains adjacents et aux ruisseaux d'eau. Retenue.	No other roads suitable for widening due to adjacent land use/development. Screen out. Aucune autre route ne pourrait être élargie en raison de l'utilisation des terrains adjacents et des aménagement adjacents. La solution est rejetée. (Le chemin Baseline est pris en compte dans le segment 3)	Addresses the need providing an alternative route for ongoing development on the south side of Rockland. Carried forward. La solution répond aux besoins du projet en créant un autre itinéraire répondant à la croissance rapide au sud de Rockland. Retenue.

Shortlist of Alternative Solutions

Courte liste de solutions

We will refine the short list of alternatives and undertake a more detailed evaluation of the impacts and benefits of the alternative solutions carried forward to arrive at a preferred alternative solution.

Nous réduirons la courte liste de solutions et entreprendrons une évaluation plus détaillée des répercussions et des avantages des solutions retenues pour choisir la meilleure solution.

Split Trim
Widen existing 174 to 6 lanes
Trim to Rockland
Widen existing 174/17 to 4 lanes
Widen/new route Innes-Baseline
Build new route south of Wilhaven
Rockland vicinity
Improve existing 17
Build new route south of Rockland

De la jonction au chemin Trim
Élargissement de la route 174 existante à six voies
Du chemin Trim à Rockland
Élargissement de la route 174/17 existante à quatre voies
Élargissement ou construction d'une nouvelle route entre le chemin Innes et le chemin Baseline
Construction d'une nouvelle route au sud de la promenade Wilhaven
Environs de Rockland
Amélioration de la route 17 existante
Construction d'une nouvelle route au sud de Rockland

Evaluation of Alternative
Solutions Carried Forward

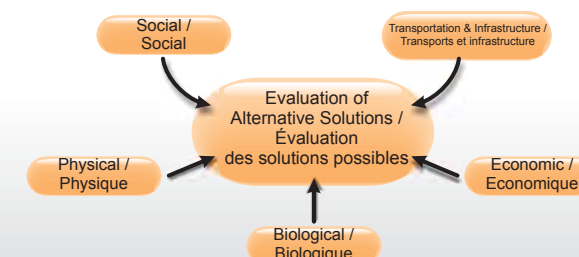
The methodology of evaluating alternative solutions will address the following objectives.

- Use of Criteria:** Criteria/indicators (i.e., features or considerations) should be used as the basis for the evaluation of the alternative solutions
- Weighting or Ranking of Criteria:** An ordering (i.e., weighting or ranking in terms of importance) of the criteria should be the basis for the evaluation
- Sensitivity Analysis:** Some form of sensitivity analysis should be completed as part of the evaluation process (i.e., to test the results of the evaluation and to verify the influential factors in the results of the evaluation)

Évaluation des solutions alternatives
répondant aux retenues

La méthode d'évaluation des solutions retenues visera les objectifs suivants.

- Utilisation des critères :** les critères ou indicateurs (c.-à-d. les particularités ou les éléments pris en compte) doivent servir de base à l'évaluation des solutions de rechange
- Pondération ou classement des critères :** un classement des critères (c.-à-d. pondération ou classement selon l'importance) devrait servir de base à l'évaluation
- Analyse de sensibilité :** une certaine analyse de sensibilité doit être effectuée au cours du processus d'évaluation (c.-à-d. afin de tester les résultats de l'évaluation et de vérifier les facteurs qui ont pu les influencer)



Examples of Evaluation Criteria

Evaluation criteria are specific factors or considerations which are used to compare alternatives. Factors that need to be considered in a development of criteria include:

- The nature of the project
- The expectations of stakeholders
- An understanding of the existing conditions
- The delineation of boundaries
- The issues related to the proposed project

The Study Team will develop the criteria to be used in this assessment with the stakeholders.

Let us know what you think is important and should be considered in the selection of an alternative.

Criteria Group Groupes de critères	Examples of Criteria Exemples de critères
Biological Conditions Milieu biologique	<ul style="list-style-type: none"> • Wildlife • Fisheries • Watercourses • Woodlots • Vegetation
Social Conditions Milieu social	<ul style="list-style-type: none"> • Community Linkages • Archaeology • Economic Activity • Noise • Recreation • Land requirements/use
Physical Conditions Milieu physique	<ul style="list-style-type: none"> • Soils • Bedrock • Groundwater • Drainage
Transportation & Infrastructure Transports et infrastructure	<ul style="list-style-type: none"> • Transit Ridership • Network/System Integration • Level of Service • Geometry • Capacity • Operation
Economic Économique	<ul style="list-style-type: none"> • Capital Cost • Operational and Maintenance Costs • Business Impacts

Exemples de critères d'évaluation

Les critères d'évaluation sont des facteurs ou des éléments pris en compte qui sont utilisés pour comparer les solutions possibles. Les facteurs qui doivent être pris en considération lors de l'établissement des critères sont les suivants :

- La nature du projet
- Les attentes des intervenants
- La compréhension des conditions actuelles
- Le tracé des limites
- Les enjeux liés au projet proposé

L'équipe chargée de l'étude établira les critères qui seront utilisés pour l'évaluation en collaboration avec les intervenants.

Dites-nous ce qui, à votre avis, est important et devrait être pris en compte dans la sélection de la solution.

Next Steps

The next steps in the study process are to:

- Evaluate the Alternative Solutions carried forward
- Identify a preliminary preferred solution
- Confirm the preferred solution and review the EA Requirements
- Develop Alternative Design for the preferred solution
- Update existing conditions, as required
- Continue to meet with the Consultation Groups

The results will be presented at Public Open House #2 scheduled for the fall of 2013.

Prochaines étapes

Les prochaines étapes de l'étude sont :

- D'évaluer les solutions retenues
- De choisir provisoirement la meilleure solution
- De confirmer la meilleure solution et de passer en revue les exigences en matière d'évaluation environnementale
- D'élaborer d'autres conceptions possibles pour la meilleure solution
- De mettre à jour les conditions, au besoin
- De continuer à rencontrer les groupes de consultation

Les résultats seront présentés lors de la deuxième séance portes ouvertes, qui aura lieu à l'automne 2013.

Thank You

Your involvement is valuable to the success of this study.

Please complete the Comment-Questionnaire and place it in the designated box before you leave, or mail or fax it to us by February 22 2013.

Merci

Votre opinion contribue au succès de ce projet.

Veuillez remplir le formulaire de commentaires et le déposer dans la boîte prévue à cet effet avant de partir, ou nous le faire parvenir par télécopieur ou par la poste avant le 22 février 2013.



Annex D-6

- Newsletter (English and French)

Overview

This Information Bulletin has been prepared to provide a common understanding of the Ottawa Road 174/ Prescott-Russell County Road 17 Environmental Assessment Study. It includes information on the project background, study process and schedule, existing conditions, description of consultation activities, screening of alternative solutions, evaluation methodology and next steps.



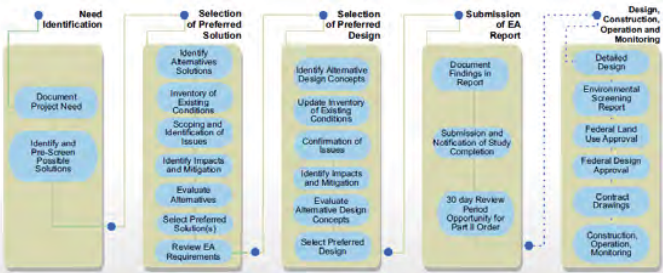
Project Background

Ottawa Road (OR) 174 and County Road (CR) 17 were formerly Provincial Highway 17, and were transferred to the City of Ottawa and the United Counties of Prescott and Russell in the 1990's. The OR174/CR17 corridor is the only high capacity arterial in the area. Transportation is constrained by the natural geography, including the Ottawa River and Mer Bleue Bog.

Transportation problems have been identified for many years, and growth in Orléans, Cumberland and Rockland has continued to increase traffic volumes. A Needs Assessment Study has indicated that even with increased transit and a broad range of travel-reducing strategies, there will be a need to provide additional transportation capacity within the study area to meet future transportation requirements. The study further demonstrated that there were opportunities for safety and operational improvements within the study corridor.

Study Process

In accordance with the Municipal Class Environmental Assessment (2011), the widening of a road or construction of a new road/paved facility over \$2.3 M is a Schedule “C” project. Schedule “C” projects require the preparation of an Environmental Study Report which will be made available for public review.



Schedule

Over the next two years, the study team will present and evaluate design alternatives and assess the impacts of the preferred alternative. The EA Report is planned to be provided for a 30-day legislated review period for all interested parties at the end of 2014.

Existing Conditions

Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by a transportation facility. The information will be used to: develop reasonable alternatives; identify impacts; and evaluate alternatives. Information will be updated, as necessary, as the study progresses and more detailed information is required..

Consultation

Consultation includes three invited Consultation Groups and Public Participation at Open Houses held during key points of the study.

Agency Consultation Group (ACG): formed to address the full range of technical issues and to comment on special studies as well as applicable procedures, legislation and policies. The ACG includes representatives from all levels of government and regulatory agencies.

Business Consultation Group (BCG): formed to review work-to-date and to provide comments on study activities, issues and concerns that reflect business interests and values. BCG members include business associations, institutions and large commercial establishments and large land owners within the Study Area.

Public Consultation Group (PCG): formed to enable the community and interest groups to provide direct input into the study. It includes representatives from City Wards adjacent to the corridor, interest groups, and representatives from several City advisory committees.

Public Open Houses

Three Public Open Houses are planned throughout the Study. The objectives of each of these are outlined below.

- Public Open House #1: Introduction of the project and presentation of the exiting conditions and the alternative design concepts
- Public Open House #2: Present the recommended alternatives including: design details; implementation plans; the assessment of effects and the proposed mitigation measures
- Public Open House #3: Present final technical details

Information on the EA is also available online:

www.prescott-russell.on.ca/en/public-works/major-projects

or

www.ottawa.ca/hwy174and17study

Alternative Solutions

There are capacity, safety and operational deficiencies in the study area. In order to address these deficiencies a long list of alternative solutions was developed. Screening criteria were developed to assess the merits of the long list of alternative solutions. The screening criteria are:

- **The ability of the alternative to address the project need.** Alternative solutions must provide transportation capacity and improve safety.
- **Adherence to policies, regulations, and local standards of practice.** Alternatives should not contravene provincial/federal policies or municipal regulations or policies.
- **Consideration of environmental impacts.** Alternatives were reviewed to determine their high level impacts on the various environmental conditions. The ability to avoid/reduce/minimize impacts was considered.

Screening of Alternative Solutions – Segment 1 Split to Trim Road

Widen Existing 174	Widen Innes Road	Widen St. Joseph	Build New Route
✓	✗	✗	✗

Screening of Alternative Solutions – Segment 2 Trim to Rockland

Widen Existing 174/17	Widen/new route Innes-Baseline Road	Widen Old Montreal Road from Trim to 174 East	Widen Wilhaven Road and Frank Kenny	New route south of Wilhaven to Baseline Road
✓	✓	✗	✗	✓

Screening of Alternative Solutions – Segment 3 Rockland

Improve 17	Widen Existing urban streets within Rockland	Build new route south of Rockland
✓	✗	✓

Solutions for Evaluation

We will refine the short list of alternatives and undertake a more detailed evaluation of the impacts and benefits of the alternative solutions carried forward to arrive at a preferred alternative solution.

Split to Trim

Widen existing 174 to 6 lanes

Trim to Rockland

Widen existing 174/17 to 4 lanes

Widen/new route Innes-Baseline

Build new route south of Willhaven

Rockland vicinity

Improve existing 17

Build new route south of Rockland

Evaluation Methodology

The characteristics of this EA suggest that the chosen methodology of evaluating alternative solutions should address the following objectives:

- **Use of Criteria:** Criteria/indicators (i.e. features or considerations) should be used as the basis for the evaluation of alternatives
- **Weighting or Ranking of Criteria:** an ordering (i.e. weighting or ranking in terms of importance) of the criteria should be the basis for the evaluation.
- **Sensitivity Analysis:** some form of sensitivity analysis should be completed as part of the evaluation process (i.e. to test the results of the evaluation and to verify the influential factors in the results of the evaluation).

Criteria and weighting will be developed in consultation with the study team experts and the advisory committees.

Next Steps

The next steps in the study process are to:

- Evaluate the Alternative Solutions carried forward
- Identify a preliminary preferred solution
- Confirm the preferred solution and review the EA requirements
- Develop Alternative Design for the preferred solution
- Update existing conditions, as required
- Continue to meet with the Consultation committees

The results of these steps will be presented at Public Open House #2 scheduled for the fall of 2013.

Your involvement is important to the success of this project

Questions or Comments

If you wish to provide us with more detailed comments, send written submissions, by February 22, 2013 to:
Valerie McGirr, P. Eng. Consultant Project Manager AECOM
302 - 1150 Morrison Drive
Ottawa , ON K2H 8S9
Phone: 613-820-8282 ext. 243
E-mail: Valerie.mcgirr@aecom.com

Survol

Le présent bulletin vise à expliquer en quoi consistera l'évaluation environnementale (ée) de la route 174 d'Ottawa et la route de comté 17, Prescott-Russell. Il donne de l'information sur le contexte du projet, le processus et le calendrier de l'étude, les conditions actuelles, les activités de consultation et les prochaines étapes..



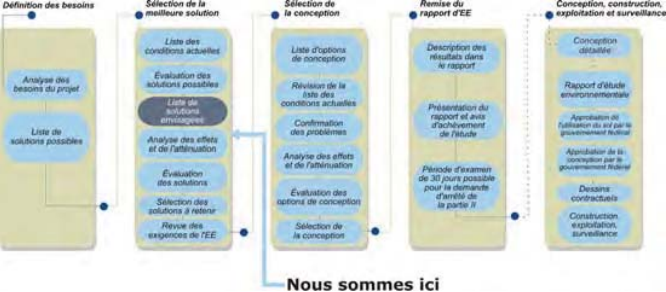
Contexte du projet

Avant d'être transférées sous juridiction municipale en 1997, la route 174 et la route de comté 17 formaient autrefois la route provinciale 17. Des contraintes géographiques naturelles (p. ex., la rivière des Outaouais et la tourbière de la Mer Bleue) limitent le nombre de voies de transport possibles sur l'axe est-ouest et la route 174 et la route de comté 17 constituent la seule artère de grande capacité dans le secteur.

Des problèmes de transport sont connus depuis de nombreuses années, et la croissance qu'ont connue Orléans, Cumberland et Rockland a entraîné une hausse du débit de circulation. L'évaluation des besoins de cette étude a indiqué que Même avec une utilisation accrue du transport en commun et la mise en œuvre d'autres stratégies de réduction des déplacements, une plus grande capacité sera requise à partir de la jonction jusqu'à Rockland. L'étude a aussi démontré qu'il est possible d'améliorer la sécurité du couloir de les routes 174 et 17

Processus de l'étude

Selon l'évaluation environnementale municipale de portée générale (2011), l'élargissement d'une route ou la construction d'une nouvelle route ou chaussée revêtue, à un coût de plus de 2,3 M\$, est un projet visé par l'annexe C. Tout projet de l'annexe C requiert la préparation d'un rapport d'étude environnementale qui sera mis à la disposition du public aux fins d'examen.



Échéancier de l'étude

Au cours des deux prochains années, l'équipe chargée de l'étude présentera et évaluera les options de conceptions et évaluera les impacts de l'option préférée. Le rapport d'EE devrait être accordé pour une période de 30 jours prévu par la loi examen de toutes les parties intéressées à la fin de 2014.

Les conditions environnementales

Les conditions environnementales de départ ont été documentées pour tenter d'évaluer l'effet sur l'environnement d'un service de transport. Les renseignements seront utilisés pour : trouver des solutions de rechange raisonnables; estimer les effets; et évaluer les solutions. Les renseignements seront mis à jour, au besoin, au fur et à mesure que l'étude avancera et que des renseignements détaillés seront nécessaires.

Consultation

La consultation se fera auprès de trois groupes consultatifs suivants, de même qu'auprès du grand public à l'occasion des séances « portes ouvertes » qui se tiendront aux moments clés de l'étude.

Le Groupe de consultation technique (GCT), chargé d'examiner l'ensemble des questions techniques et de formuler des commentaires sur les études spéciales ainsi que sur les procédures, lois et politiques applicables. Le GCT est composé de représentants de tous les ordres de gouvernement et des organismes de réglementation.

Le Groupe de consultation des gens d'affaires (GCGA), formé pour examiner les travaux réalisés et formuler des commentaires sur les activités, les questions et les préoccupations liées à l'étude qui reflètent les intérêts et les valeurs du milieu des d'affaires. Les membres du GCGA représentent les chambres de commerce, les associations commerciales, les institutions, les grands établissements commerciaux et les grands propriétaires fonciers à l'intérieur du secteur à l'étude.

Le Groupe de consultation publique (GCP), mis sur pied afin de permettre à la communauté et aux groupes d'intérêts de prendre une part directe à l'étude. Il comprend des représentants des quartiers adjacents au couloir, de groupes d'intérêts et de plusieurs comités consultatifs de la Ville.

Réunions publiques

Trois séances « portes ouvertes » sont prévues au cours de l'étude. Les objectifs de chacune sont décrits ci-après :

- 1^{re} séance « portes ouvertes » : présentation du projet et exposé des conditions actuelles et des conceptions alternatives;
- 2^e séance « portes ouvertes » :: exposé des solutions alternatives recommandées, et ldes informations sur les conditions mises à jour existantes
- 3^e séance « portes ouvertes » : exposé dernier détails techniques définitifs.

Informations sur l'EE est aussi disponible en ligne:

<http://www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure>
ou
www.ottawa.ca/etuderoute174et17

Solutions Possibles

La zone d'étude présente des lacunes en matière de capacité, de sécurité et de fonctionnement. Afin de remédier à ces lacunes, une longue liste de solutions possibles a été dressée. Les critères ci-dessous ont été définis afin d'évaluer la valeur de chacune des multiples solutions envisagées.

- Satisfaction des besoins du projet : la solution permet d'augmenter la capacité routière et la sécurité.
- Respect des politiques, règlements et normes de pratique locales : la solution n'enfreint ni les politiques provinciales et fédérales, ni les règlements et politiques municipaux.
- Prise en compte des répercussions sur l'environnement : les effets majeurs de chaque solution sur les diverses conditions environnementales ont été évalués. L'absence de répercussions ou la présence d'effets limités a été prise en compte.

Sélection – De la jonction au chemin Trim

Élargissement de la route 174	Élargissement du chemin Innes	Élargissement du boulevard Saint-Joseph	Aménagement d'un nouveau circuit
✓	✗	✗	✗

Sélection – Du chemin Trim à Rockland

Élargissement de la route 174 /17	Élargissement Innes/ ou aménagement d'un nouveau circuit	Élargissement du Old Montreal entre Trim et 174	Élargissement Wihaven et Frank Kenny	Aménagement d'un nouveau circuit vers le Baseline au sud de la de Wilhaven
✓	✓	✗	✗	✓

Sélection – Rockland

Amélioration de la route 17	Élargissement des voies urbaines de Rockland	Aménagement d'un nouveau circuit au sud de Rockland
✓	✗	✓

Solutions retunes

Nous allons peaufiner cette courte liste d'options et effectuer une évaluation plus détaillée des répercussions et des avantages de chaque solution retenue, afin de trouver la solution à privilégier.

De la jonction au chemin Trim
Élargissement de la route 174 à six voies
Du chemin Trim à Rockland
Élargissement de la route 174/17 à quatre voies
Élargissement du circuit entre le chemin Innes et le chemin Baseline ou aménagement d'un nouveau circuit

Aménagement d'un nouveau circuit au sud de la promenade Wilhaven
Environs de Rockland
Amélioration de la route 17
Aménagement d'un nouveau circuit au sud de Rockland

Méthodologie d'évaluation

- La méthode d'évaluation des solutions retenues visera les objectifs suivants.
- Utilisation des critères : les critères ou indicateurs (c.-à-d. les particularités ou les éléments pris en compte) doivent servir de base à l'évaluation des solutions de rechange
 - Pondération ou classement des critères : un classement des critères (c.-à-d. pondération ou classement selon l'importance) devrait servir de base à l'évaluation
 - Analyse de sensibilité : une certaine analyse de sensibilité doit être effectuée au cours du processus d'évaluation (c.-à-d. afin de tester les résultats de l'évaluation et de vérifier les facteurs qui ont pu les influencer)

L'équipe d'étude collaborera avec les intervenants afin de définir les critères qui seront utilisés dans le cadre de cette évaluation.

Prochaines étapes

- Les prochaines étapes de l'étude sont :
- D'évaluer les solutions retenues
 - De choisir provisoirement la meilleure solution
 - De confirmer la meilleure solution et de passer en revue les exigences en matière d'évaluation environnementale
 - D'élaborer d'autres conceptions possibles pour la meilleure solution
 - De mettre à jour les conditions, au besoin
 - De continuer à rencontrer les groupes de consultation

Les résultats seront présentés lors de la deuxième séance porte ouverte, qui aura lieu à l'automne 2013.

Votre participation compte pour beaucoup dans la réussite de ce projet

Des questions ou des commentaires?

Si vous souhaitez nous faire part de vos commentaires supplémentaires, transmettez-les par courrier, d'ici au 22 février 2013, à l'adresse suivante :

Valerie McGirr, P. Eng. Chargée de projet de firme de conseils
AECOM
302 - 1150 promenade Morrison
Ottawa , ON K2H 8S9
Téléphone : 613-820-8282 poste. 243
Courriel: Valerie.mcgirr@aecom.com

Appendix G

- Comment-Questionnaire
- Comments Submitted at and after the Public Open House Meetings (excluding personal information)

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name) _____
What specific interest do you have in this study? _____

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them. _____

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	er portant	o e hat portant	ess portant
ocial Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Ph sical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
cono ic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

If you wish, please provide	
Name:	_____
Representing	_____
Address:	_____ _____
Postal Code:	_____
Email:	_____

Thank you for your participation
If you wish to provide us with more detailed comments, send written submissions, by February 22, 2013 to: Valerie McGirr, P. Eng. Consultant Project Manager AECOM 302 - 1150 Morrison Drive Ottawa , ON K2H 8S9 Phone: 613-820-8282 ext. 243 E-mail: Valerie.mcgirr@aecom.com



ran ais au erso

Séance portes ouvertes no 1 –Fevrier 1
FE ILLE DE COMME TAIRE- ESTIO AIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaitre votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir notre Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

PROPOS DE Vo S :
Où habitez-vous? (nom de la rue) _____
En quoi cette étude vous intéresse-t-elle en particulier? _____

PROBL MES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnementale?

SOL TIO S DE REC A E

Avez-vous des commentaires ou des inquiétudes particulières quant aux les alternatives? Si oui, s'il vous plaît les décrire. _____

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	tr s i portant	peu i portant	oins i portant
Crit res sociau	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Crit res Transportation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Crit res biologique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Crit res ph sique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Crit res écono ique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y at-il d'autres critères qui, selon vous, devraient être utilisées dans l'évaluation? Si oui, s'il vous plaît décrire. _____

RO PES DE CO S LTATIO

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom _____ Organisme _____
Courriel _____ Téléphone _____

COMME TAIRE- É ÉRA

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☐ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Si vous le souhaitez , laissez -nous vos coordonnées	
Nom:	_____
Représentant	_____
(le cas échéant):	_____
Adresse:	_____ _____
Code postale:	_____
Courriel:	_____

Merci de votre participation
Si vous souhaitez nous faire part de vos commentaires supplémentaires, transmettez-les par courrier, d'ici au 22 février 2013, à l'adresse suivante : Valerie McGirr, P. Eng. Chargée de projet de firme de conseils AECOM 302 -1150 promenade Morrison Ottawa , ON K2H 8S9 Téléphone : 613-820-8282 poste. 243 Courriel: Valerie.mcgirr@aecom.com



nglish on re erse

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

KINSELLA

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

PLEASE e-mail me a list of all those with "833" numbers (Cumberland Residents) who express an interest in representing the community.

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

LOOKOUT Drive

OVERLOADING ON 174

Have lived more than 25 years in Cumberland - concern for future of the community.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

- COMPREHENSIVE & WELL PRESENTED
- Some gaps exist
- What about costs of alternatives.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

The 3 segments cannot be looked at in isolation. Seems if all starts at TRIM ROAD.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Costs - expropriation costs need to be considered

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☒ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Connections from Section to Section
Cost estimates of each alternative

Séance portes ouvertes no° 1 –Fevrier2013
FEUILLE DE COMMENTAIRES-QUESTIONNAIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaître votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir notre Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

À PROPOS DE VOUS :

Où habitez-vous? (nom de la rue) _____

En quoi cette étude vous intéresse-t-elle en particulier? _____

PROBLÈMES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnemer tale?

SOLUTIONS DE RECHANGE

Avez-vous des commentaires ou des inquiétudes particulières quant aux les alternatives? Si oui, s'il vous plaît les décrire _____

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	très important	peu important	Moins important
Critères sociaux	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères Transportation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères biologique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères physique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères économique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y at-il d'autres critères qui, selon vous, devraient être utilisées dans l'évaluation?
Si oui, s'il vous plaît décrire. _____

GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom _____ Organisme _____
Courriel _____ Téléphone _____

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☐ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Please keep us informed by mail. Thanks

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name) _____

What specific interest do you have in this study? _____

BECKETT'S CREEK
The southern route alternative would cut through my 72 acre property.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Extend your study area to an area that includes options further south than Paris Road - for example, go to French Hill Rd. A suggestion was made at the Cumberland consultation meeting, to possibly also look at Walkely Road as an optional route.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 –February 2013

COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

RR 174 CEMBRAND

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

NOT ENOUGH, NEED TO INCLUDE TRANSIT OPTIONS

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

ECONOMIC, SOCIAL

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name

Organization

Email

Phone:

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☒ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

I WOULD LIKE TO SEE TRANSIT OPTIONS ALONG WITH THE NEW ROAD OPTIONS.

Séance portes ouvertes no° 1 –Fevrier2013

FEUILLE DE COMMENTAIRES-QUESTIONNAIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaître votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir notre Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

À PROPOS DE VOUS :

Où habitez-vous? (nom de la rue)

Kevin CR.

En quoi cette étude vous intéresse-t-elle en particulier?

pour l'avenir du village et pour mon terrain en arrière de ma propriété

PROBLÈMES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnementale?

Je veux simplement vous faire savoir que je suis contre l'élargissement de la 174 pour les raisons suivantes → c'est la fin du village → impossible pour les gens habitant sur la 174 de sortir ou entrer chez eux

SOLUTIONS DE RECHANGE
Avez-vous des commentaires ou des inquiétudes particulières quant aux alternatives? Si oui, s'il vous plaît les décrire.→ ne règle pas le problème de circulation.
Je préfère de beaucoup les 2 autres alternatives

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	très important	peu important	Moins important
Critères sociaux	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères Transportation	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères biologique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères physique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères économique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y a-t-il d'autres critères qui, selon vous, devraient être utilisés dans l'évaluation?

Si oui, s'il vous plaît décrire.

GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom

Organisme

Courriel

Téléphone

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☐ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Dunning Rd vicinity

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

New Route

Route 174 - current to 417 @ Hunt Club

London or Ring Road

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Barnett Drive

Ensure sensible & sustainable solution
and ensure options are considered

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Ensure Cardinal Creek village development
can accommodate, and does not block any
option to link 174 - Pines Corridor

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Future development growth for the
City of Ottawa
expanding current 174 does not benefit city

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☒ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Overall, the EA process seems to want to
recommend expansion, and not options

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

On Highway 174

The widening between Trim and Cumberland Village

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

If a new route south is built, that would give users 2 routes to use. Only expanding the 174 still leaves one route. (Remember congestion during the sink hole)

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Should the current 174 be expanded the Village of Cumberland would be virtually decimated. Terrible social impact!!

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes☐ No☐ Somewhat

excellent, thorough presentation.

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

WEST ST.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

MY PROPERTY HAS FRONTAGE ON HWY 174, I AM WORRIED IF YOU TAKE SOME LAND FROM MY PROPERTY, THE HWY TRAFFIC WILL BE ENHANCED, I WOULDN'T BE ABLE TO LIVE IN MY HOUSE

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

WHY DON'T YOU USE OLD MONTREAL RD., IT IS THERE IT COULD BE FIXED TO USE IT

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

USE OLD MONTREAL RD.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes☐ No☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Maglady Rd.

Lifelong Cumberland Township residents

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

A lot of good detail

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Make sure the western end of the new or expanded
Innes Road option can accept the flow of traffic
and make proper connection to the 174.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Remember the longstanding residents of Cumberland Village
and their wishes in preference to protecting a block of
development land at the western end of this study area.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Old Montreal rd

what happens will impact our property

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

There was a chance before Cardinal Creek was
approved to make a sensible
access

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

South →

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

The future of a historic village

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization C.C.A.
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

RR 174

Concern about the effect of

widening highway 174

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

The background was quite good.
I believe greater emphasis on the impact to the
Ottawa river should be made

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

The current city transport plans to 2031 seem to come
to a crunching halt at tripp road. It seems to me some
minor or at least relatively small road building that connects
Innes/Blackburn bypass/Expanded Blackburn bypass would provide
superior alternative east
route

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Building roads for cars simply means more cars
until the route saturates.
Better transport public transportation capacity is
a longer term solution

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Séance portes ouvertes no° 1 -Fevrier2013
FEUILLE DE COMMENTAIRES-QUESTIONNAIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaître votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir notre Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

À PROPOS DE VOUS :

Où habitez-vous? (nom de la rue)

174

En quoi cette étude vous intéresse-t-elle en particulier?

Investissement Immobilier

PROBLÈMES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnemer tale?

Rien. Vous l'avez déjà fais votre étude environnementale.
Qu'attendez-vous pour aller de l'avant?

SOLUTIONS DE RECHANGE

Avez-vous des commentaires ou des inquiétudes particulières quant aux les alternatives? Si oui, s'il vous plaît les décrire

Le Innes extension ne servira pas la population général.
Aucun gain significatif...

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	très important	pas important	Moins important
Critères sociaux	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères Transportation	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères biologique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères physique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères économique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y a-t-il d'autres critères qui, selon vous, devraient être utilisées dans l'évaluation? Si oui, s'il vous plaît décrire.

GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom _____ Organisme _____
Courriel _____ Téléphone _____

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☒ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

A new four lane highway needs to be constructed south of Rockland and Cumberland Village. This ‘ring road’, which really should become the responsibility of the province, could extend east from Innes Road, or more in line with future growth east of Wall Road, as this will become an area of dense population and industry over the next fifty years.

No one wants to live adjacent to a super highway that has feeding lanes and overpasses. Many of us have been involved in highway widening discussions since the 1980s assuming that ‘we’ll believe it when we see it!’. Now, a decision has to be made.

My husband and I live at Regional Road 174 on land that has been in my family since 1862. My husband grew up on a farm by the river bordering Canaan Road on land that had been in his family from 1850-1957. Both of our families had land expropriated around 1908 for the construction of the Canadian Northern Railway line, then again in 1948 for the construction of the Trans Canada Highway. Property owners were then given entrances across the highway so that we could access the river, or cross our cattle from pastureland to the barn.

During the 1950s and 1960s, few dwellings other than summer cabins, existed along the river. We were one of the first residents to build a permanent home on cottage property in 1974 and were lucky to have highway access, as the Department of Highways did not grant new laneways onto the ‘killer strip’ from 1952-2000. Suddenly, with amalgamation, the City of Ottawa began to issue permits for private laneways onto the highway and were glad to have more tax revenue from large residences that were built to replace ramshackle cottages on riverfront property. Was the City not aware that studies and plans had been conducted to widen the highway?

Questions:

Financial:

Will it not be more cost effective to expropriate vacant land south of Innes Road rather than pay the current real estate value to property owners along the river?

How much land will need to be expropriated for overpasses at Quigley Hill Road (entrance to Barnett Park), at Cameron Street (access to the ferry) and at Village Boise, Kinsella, McTeer and at Canaan Road (access to McTeer) ?

What are the costs involved in moving the hydro-telephone-cable poles and lines along the highway. Will they be buried underground?

How do the riverside residents access their properties? Two feeding lanes will have to be built to allow people to access their property on either side of the widened 174.

Will existing houses be able to sustain added ‘shaking’ with the increase of more traffic, as well as the imposition of construction equipment noise etc. over a fair period of time?

Environmental:

What will happen to our wells and septic tank systems that will then be too close to the road?

How stable is the land, especially at narrow stretches along the shoreline where landslides have previously occurred-1922 and 1930 west of Cumberland Village and in 1973 just east. How stable is the rock cut near Petrie Island?

How will the storm drains that draw water from south of Cumberland Village into the river be rebuilt? How will you bridge large ravines that exist along the highway?

Can you justify pouring more rock into the sandy, clay shoreline to provide space for more lanes of traffic?

Social”

Why further decimate the viable community of Cumberland Village with additional traffic lanes. Will overpass walkways be built for pedestrian and bicycle traffic?

What of Heritage? Champlain opened this river highway corridor in 1613. Why not encourage tourism and scenic drives along a river parkway?

Will an archaeological study ensue? In 1957 my mother and I found a perfectly shaped Indian arrowhead at our shore and in 1962 my father and I discovered fish fossils near our shore and these artefacts now belong to the Victoria Museum in Ottawa.

Progress comes at a price. Your decisions must be made wisely in order to keep costs realistic and cause the least amount of disruption to existing homeowners along the proposed road widening area.

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

East Hares Road

Noise

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

-there will be an increase in noise to property
-will need to cut down trees in my back yard which
act as noise barrier

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

-NOISE

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

SEAWOIA DR

Decision on which route is chosen environmental
and social impact, specially as it affects my quality
of life

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

Excellent background information and apparently
well analyzed

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

My preferred alternative (Frank Rockland) is the route
South of Wilhaven

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

So for the criteria outlined seem
reasonable

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Séance portes ouvertes no° 1 –Fevrier2013
FEUILLE DE COMMENTAIRES-QUESTIONNAIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaître votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir notre Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

À PROPOS DE VOUS :

Où habitez-vous? (nom de la rue) Landry
En quoi cette étude vous intéresse-t-elle en particulier? Comment proche les routes seront de nous, l'augmentation du bruit, et du trafic sur les routes aux alentours.

PROBLÈMES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnementale?
trouver des solutions à court terme et à long terme

SOLUTIONS DE RECHANGE

Avez-vous des commentaires ou des inquiétudes particulières quant aux les alternatives? Si oui, s'il vous plaît les décrire.
Ne pas trop allonger en kms les routes
Ne pas prendre trop de temps - c'est maintenant pas d'en 20 ans

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	très important	peu important	Moins important
Critères sociaux	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères Transportation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères biologique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères physique	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères économique	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y a-t-il d'autres critères qui, selon vous, devraient être utilisées dans l'évaluation?
Si oui, s'il vous plaît décrire.

GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom _____ Organisme _____
Courriel _____ Téléphone _____

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☒ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Si vous le souhaitez, laissez-nous vos coordonnées

Merci de votre participation

Séance portes ouvertes no° 1 –Fevrier2013
FEUILLE DE COMMENTAIRES-QUESTIONNAIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaître votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir notre Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

À PROPOS DE VOUS :

Où habitez-vous? (nom de la rue) Orléans - Charest
En quoi cette étude vous intéresse-t-elle en particulier? l'agrandissement du 174 entre la 'split' et Trem.

PROBLÈMES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnementale?
la capacité pour les autos pendant l'heure de travail.

SOLUTIONS DE RECHANGE

Avez-vous des commentaires ou des inquiétudes particulières quant aux les alternatives? Si oui, s'il vous plaît les décrire. Est-ce qu'il y aura les réductions des voies pendant la construction du 'LRT'?

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	très important	peu important	Moins important
Critères sociaux	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères Transportation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères biologique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères physique	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Critères économique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y a-t-il d'autres critères qui, selon vous, devraient être utilisées dans l'évaluation?
Si oui, s'il vous plaît décrire. Aucun

GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom _____ Organisme _____
Courriel _____ Téléphone _____

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

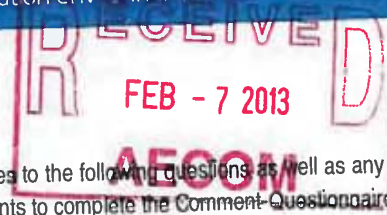
☒ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Expliquer la terme 'analyse de sensibilité' mieux sur les panneaux comme on a donné pendant la présentation.

Si vous le souhaitez, laissez-nous vos coordonnées

Merci de votre participation

Public Open House #1 - February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

- OTTAWA ROAD 174
- I LIVE ON THE 174 AND AM OPPOSED TO ITS WIDENING FROM TRIM TO CANAAN ROAD.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

THERE IS A PUBLIC RIGHT OF WAY FROM LILAC LANE ACROSS THE 174 AND THEN ON TO THE OTTAWA RIVER. HOW WILL THIS PUBLIC RIGHT OF WAY BE MAINTAINED?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them. THE TWO SOUTHERN OPTIONS ARE PREFERRED. THE KEY ISSUE IS THE CONNECTION TO THE 174. TRIM IS THE OBVIOUS CHOICE.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

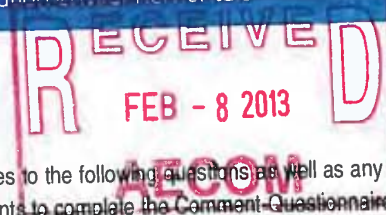
Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 - February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

des Trembles Ave Cumberland
The location of the four lanes is very important from a personal level and traffic flow affects quality of life, the environment and even more practically, the transportation of goods as well as people. We are involved in the transportation of goods.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

Excellent presentation - study information and well done to date. The issue I identified is the disconnect between the area from the split to them and the rest. With only one alternative being studied for the 'split to Trim' it will have great difficulty connecting to the alternative for the additional segments except widening 174.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Since the growth is south, the alternatives should be south as well. My recommendation is that an alternative to widening segment one is to build a new road connecting to the 417 at Wadley's Hunt Club.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

The focus has been on getting people to and from work - you need to broaden it to include commercial requirements.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

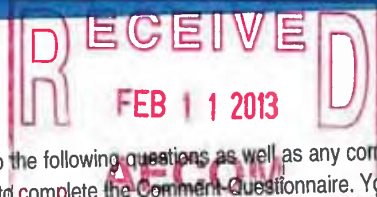
GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE



The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Old Montreal Rd

Future of Cumberland Village
Expropriation and devaluing my property

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

Widening #174 will damage and diminish our village. Expanding #174 seems to be the top choice by those who don't live here. Why expand #174 which eventually bottle-necks at the split.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Choose Innes Rd through Baseline from link it further west. I thought the new four lane Frank Kenny was to join Trim north to #174 in original plans.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

A new route to the south (Walkely/Heron) should be considered.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☒ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Representatives lacked important answers such as "is the Provincial money for expansion still available and is the money available for alternatives to work on #174"

Considering that Orleans has one of the highest if not THE highest public transit ridership, why is the LRT focusing on routes to the west and south only.

Public Open House #1 –February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

Putvin Ave Rockland

What specific interest do you have in this study?

Travel is Slow to Ottawa
and I have 3 NEW TOWN HOMES with 1 more just purchased
so I am bringing New Families to Rockland, so they need 4 lane Hwy.
need to Travel.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ITS all Great.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Costs to put
4 lane south of Rockland, versus 4 lanes from Orleans,
The Traffic needs to move same as Carleton Place
and Accompany with their New Highway.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Closing of Road entrances along 17 and at least
not put passing lanes.
The new Rd to the South of Rockland is the best,
but costs I figure would be higher.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

If you wish, please provide

Thank you for your participation

If you wish to provide us with more detailed comments, send written

Séance portes ouvertes no° 1 –Fevrier2013
FEUILLE DE COMMENTAIRES-QUESTIONNAIRE

Les Comtés unis de Prescott et Russell et la Ville d'Ottawa souhaite connaître votre opinion sur les sujets suivants et recueillir vos commentaires sur l'information présentée lors de cette séance portes ouvertes. Veuillez prendre quelques instants pour remplir noire Feuille de commentaires-questionnaire que vous pourrez ensuite déposer, avant de quitter les lieux, dans la boîte prévue à cette fin ou encore, transmettre par la poste à l'adresse mentionnée plus bas avant le 22 février 2013. Tous les renseignements recueillis sur ce formulaire feront partie du dossier public et ne serviront qu'à réaliser cette étude.

À PROPOS DE VOUS :

Où habitez-vous? (nom de la rue)

1150x St. Lawrence

En quoi cette étude vous intéresse-t-elle en particulier?

Revoir le Gds se rendent à
CITROÏI TRS LES JOURS IRLS LES GÉNÉRALIS INACCÉSSIBLES

PROBLÈMES PRIMAIRES :

Quels sont selon votre opinion, les problèmes primaires qui doivent être traités pendant l'Étude d'évaluation environnementale?

ADHÉRE DES ROUTES ENSTRANES
TRANSIT EN COMMUN

SOLUTIONS DE RECHANGE

Avez-vous des commentaires ou des inquiétudes particulières quant aux alternatives? Si oui, s'il vous plaît les décrire.

Plus les
CONSTITUÉS, RECHERCHES DES DIFFÉRENTS MILLIERS GOUVERNEMENTALES ET
LE PAYS DE RESSOURCES FINANCIÈRES, LE COUT.

Quelle importance accordez-vous aux groupes suivants les critères qui ont trait à l'étude 174/17?

	très important	peu important	Moins important
Critères sociaux	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères Transportation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères biologique	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Critères physique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Critères économique	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Critères préliminaires ont été élaborés pour évaluer des alternatives. Y a-t-il d'autres critères qui, selon vous, devraient être utilisées dans l'évaluation?

Si oui, s'il vous plaît décrire. _____

GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées

Nom _____ Organisme _____
Courriel _____ Téléphone _____

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☒ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Chartrand, Danielle

From: McGirr, Valerie
Sent: Thursday, February 14, 2013 7:38 AM
To: Chartrand, Danielle
Subject: FW: l'autoroute 417 à la route de comté 8 (chemin Landry).

fyi

Val
 Valerie McGirr, P. Eng.
 Manager, Ottawa Office
 D 613.820.8282 ext 243

AECOM
 302-1150 Morrison Drive, Ottawa ON K2H 8S9
 T 613.820.8282 F 613.820.8338
www.aecom.com

From:
Sent: Wednesday, February 13, 2013 11:24 PM
To: McGirr, Valerie
Subject: l'autoroute 417 à la route de comté 8 (chemin Landry).

Bonjour, j'ai vu la map qui va raccorder innes a baseline, et le probleme, la curve de baseline a landry, d'apres ce que je vois, ça passe directement dans ma maison, y va falloir ce parlé la dessus.

Feb. 7. 13. ^{26a} ✓

I attended the meeting at Maple Hall in Cumberland on Feb. 4. 13.

Extremely Disappointed - very little change from the last study + not as detailed -

My impressions -

1. Staff not well informed - not able to answer questions on funding
 Will it all be spent on studies
 as millions have been spent with no results in the past?

2. I had the feeling that widening of #174 has been decided already.

1. Not mention of service roads to accommodate all these homes.

Before the last study - no new access entrances were given but since then there are many - I wonder, but can guess, why?

3. What about all the heda clay

266
2.

^{520 which}
~~that~~ the road is built - #144 has
started to fall into the river
3 or 4 times in the past.

4. Why so much attention is given to the
north when all development which is
considerable^{is} in the south. No one of
the staff answered "We don't ^{want} to build
this highway through these new
developments with all these Puckey
Homes - what about the old
Homes + loyal + hard working ^{citizens} who
live here for years?

5. No mention of how you are going
to widen #144 at the cliff at the
end of Ted Kelly Lane -

6. No solutions + suggestions as
the cost of each option -

7. Any consideration to the increase
traffic from the ferry at Cumberland?

8. What about the overpasses at
Cumberland + Trim?

Suggestions -

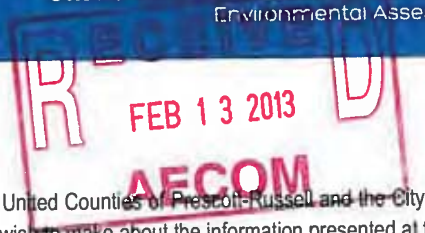
3/26C

As we were given no costs of each
option - Is that a consideration or
just the study - I'm sure now it will
be very expensive - much more than
years ago. Raising taxes + toll roads
perhaps.

1. Extension of Hunt Club ^{at Hawthorne} to Russell Rd.

2. Extending to Brian Gburn to Blackburn
by-pass

3. No more development before we
can accommodate 30,000 - 50,000 more
people.



Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Old Becketts Creek Rd.
my property is right beside
Wilhaven Dr.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

- For all 3 segments
- there should be an alternate bypass route to take
the overflow south of Innes across to baseline
- affecting the least homes & having limited on ramps
which holds up traffic flow - they have it
right in the
states & can't
get it right in
this city!!!!
- they just add
more on streets & lights
& tie up the traffic
more!!!!
All these consultants cost more money
just get it done!!!!

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

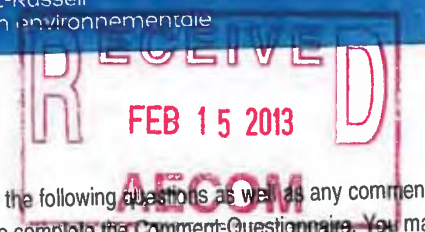
GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

quit wasting our tax dollars!!! you
will not please everyone



Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

O'Toole

if Innes is used will close off entry to our
concession therefore closing our farm business down
no more drive by access

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

It is not No sense in having the transit south yet Hwy North
No sense in expropriating thousands of century homes when
south of your study zone is vacant
No sense in adding to the current area... there is just no
space for such a project with many thousands being
offered

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Land south of your study is vacant
The Transit is off South... why not have Hwy South also

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

it is more the just those criteria... they are just
criteria for you + people in Rockford but for us
it is all a more... our lives, business, grand kids of
generations

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☒ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

because you were there just to give your presentation...
not there to hear us at all

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

Beckett's Creek Rd.

What specific interest do you have in this study?

Two of the five alternative solutions being considered for segment 2 could cross our property.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

The presentation was extremely well done. The information was very clear. I especially appreciate the Information Bulletin to help us remember what was presented. I agree with the issues: congestion + safety.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

Some elements of discarded alternatives could be retained. For example, widening could be slightly widened to add a bicycle route. Many cyclists already use this route between Dunning and Frank Kenny.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

I couldn't find the breakdown on the website

not sure of the categories for:
• noise
• aesthetics
• displaced wildlife
(moose, deer, coy, turke etc)

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Could short term and medium portions of the alternative solution be implemented to address the congestion and/or safety issues incrementally.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Other comment: what happens if the new route splits our property (i.e. access to both portions)?
• adding a priority turn signal to the traffic lights at Canaan Road would help in the short term.

Thank you for your participation

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

FEB 19 2013

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

Lookout Dr

What specific interest do you have in this study?

Daily commute to Ottawa, been living in Cumberland since 1989 and I care about the village

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

Yes - There is never much mention of the many previous studies done by different government levels

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

A widening of the 174 could be expensive and would not resolve the issue. There will not be an increase of traffic from Cumberland Village as very little land can be developed between Frank Kenny & the junction of Old Montreal and the 174. Growth is happening south, that's how road should be to avoid the 174

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

The future of Cumberland Village if the 174 is widened is dismal since no services exist, it will become a "no man's land." Too many access from backroads as it is.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☒ No ☒ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

It appears as if decisions have already been made and population has no say. The only way to get a true picture is to actually use the roads every day.

Thank you for your participation

P1

VALERIE MCGIRR P. ENG.
CONSULTANT PROJECT MANAGER
ARCOM
302-1150 MORRISON DR
OTTAWA

MY WIFE HAS FILLED OUT YOUR COMMENT QUESTIONNAIRE & I AGREE WITH MOST OF WHAT SHE HAS TO SAY, AND IT'S NOT JUST FROM CONDITIONING AFTER 20 YRS OF MARRIAGE. I WOULD LIKE TO ADD A FEW OBSERVATIONS

I) IT WOULD APPEAR THAT WHEN AN E.A. STUDY IS DONE, THAT A # OF FACTORS ARE CONSIDERED i.e. WILDLIFE, HABITAT, NATURAL FEATURES, EVEN ARCHAEOLOGICAL ITEM, SUCH AS PAST SIGNS OF NATIVE EXISTENCE OR PASSAGE. I AM SURE THAT ALL OF THIS IS FACTORED INTO SOME KIND OF EQUATION TO FORMULATE AN ANALYSIS & RESULT, WITH SOME FACTORS HAVING MORE WEIGHT THAN OTHERS. IN THIS BIG LOOSELY DEFINED EQUATION IT WOULD SEEM TO ME THAT THE SMALLEST FACTOR IS THE IMPACT OF WHAT IT WOULD HAVE ON THE RESIDENCES ACTUALLY ADJACENT TO THIS ROUTE "UPGRADE". AT THE MEETING IN CUMBERLAND, THE PRELIMINARY RESULTS OF SEGMENT #2 ALREADY HAS 2 OPTIONS CANCELLED OUT (PRIMARILY DUE TO GEOGRAPHICAL CONSTRAINTS AND NOT DUE TO THE FACT THAT THE # OF INHABITANTS AFFECTED WAS THE CAUSE)

II A QUICK COUNT WOULD INDICATE TO ME THAT THERE APPEARS TO BE A GREAT DISCREPANCY IN THE # OF RESIDENCE THAT WOULD BE IMPACTED BETWEEN THE EAST-WEST SECTION OF DUNNING RD AND FRANK KENNY. COMPARING THE 174 AND THE INNES ROAD ALTERNATIVE, THERE ARE APPROXIMATELY 45+ RESIDENCES THAT DIRECTLY ENTER ONTO THE 174 (THERE ARE ABOUT 20 MORE BEYOND CAMERON GOING TO CANNAN RD). INNES ROAD ON THE OTHER HAND HAS LESS THAN 10 HOUSES AND ONE OF THEM [INNES + DUNNING] IS A CORNER LOT WHICH WOULD ALLOW ACCESS ONTO DUNNING. INNES ROAD CLEARLY IMPACTS A LOT FEWER RESIDENCES.

III FROM A NATURAL FEATURES POINT OF VIEW THE WIDENING OF 174 NEARLY PUTS THIS OPTION BETWEEN A ROCK (ROCK CUTS INTO THE EXISTING ESCARPMENT) AND A HARD PLACE (ACTUALLY A WET PLACE) CALLED THE OTTAWA RIVER. INNES ROAD ON THE OTHER HAND IS A CARPENTER'S DREAM, ITS FLAT IS A BOARD AND STRAIGHT (RELATIVELY SPEAKING).

IV FUTURE GROWTH, IT WOULD APPEAR THAT FUTURE GROWTH IS NOT AN IMPORTANT FACTOR IN YOUR STUDY. PLEASE REMEMBER THAT ALL FUTURE GROWTH IN SEGMENT #2 WILL OCCURE SOUTH OF THE 174. THIS WILL

P2

V ALTERNATE ROUTE! ITS QUITE IRONIC THAT CURRENTLY THERE DOESN'T EXIST AN EAST-WEST ALTERNATE ROUTE IN SEGMENT #2 WHICH DOESN'T REQUIRE LARGE PERPENDICULAR OFFSETS OR BE ABLE TO EVEN HANDLE A FRACTION OF THE TRAFFIC THAT THE CURRENT 174 CAN. THE IMPLEMENTATION OF THE ALTERNATE ROUTE SOLUTION IN PLACE OF THE 174 WIDENING WOULD ALLIEVIATE THIS IRONY. PLEASE NOTE THAT AN ALTERNATE ROUTE CAN SOMETIMES BE YOUR ONLY ESCAPE ROUTE IN EMERGENCIES.

VI FUNDING. THERE WAS NO MENTION AS TO COSTING OR FUNDING OF THE PROJECT. I BROUGHT UP THE ISSUE AT THE MEETING AND ASKED IF THE FUNDING ANNOUNCED BY BOTH THE FEDERAL + PROVINCIAL GOVERNMENTS WAS FOR THE EXPANSION OF THE 174 OR DID IT EXTEND TO THE ALTERNATE ROUTE OPTION. NO ONE AT THE MEETING COULD ANSWER THE QUESTION, IF IT WAS ONLY FOR THE 174 EXPANSION, THAN ISN'T THIS E.A. STUDY MUTE! BY THE WAY AN ANNOUNCEMENT MEANS NOTHING. A SIGNED LETTER OF COMMITMENT CARRIES MORE WEIGHT. ANNOUNCEMENTS HAVE THE WEIGHT OF AIR

I REMAIN

P.S YOU CAN REACH ME ON MY CELL PHONE DURING BUSINESS HOURS.

I ANSWER

Chartrand, Danielle

From: McGirr, Valerie
Sent: Wednesday, February 20, 2013 7:38 AM
To: Chartrand, Danielle
Subject: FW: Proposed Highway 174 expansion

For our records

Val
Valerie McGirr, P. Eng.
Manager, Ottawa Office
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

From:
Sent: Tuesday, February 19, 2013 4:56 PM
To: McGirr, Valerie
Subject: Proposed Highway 174 expansion

Hello Ms McGirr

I live in the area affected by the proposed highway 174 expansion, but unfortunately I was out-of-town when the open house was held to discuss this. I understand the need for the expansion east of Trim Road. Wouldn't it make more sense to go after the provincial government to provide GO Transit for the commuters between Hawkesbury and Ottawa rather than expand the 174 between Trim Road and the 417/174 split?

Regards

Chartrand, Danielle

From: McGirr, Valerie
Sent: Wednesday, February 20, 2013 8:00 AM
To: Chartrand, Danielle
Subject: FW: Highway 174 Questionnaire
Attachments: HIGHWAY 174 -FIRST PUBLIC OPEN HOUSE 2013 FEB 07.doc

Follow Up Flag: Follow up
Flag Status: Completed

Another comment sheet

Val
Valerie McGirr, P. Eng.
Manager, Ottawa Office
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

From:
Sent: Wednesday, February 20, 2013 5:41 AM
To: McGirr, Valerie
Cc: Guy Felio [GMail]; Marcel Guibord
Subject: Highway 174 Questionnaire

Valerie McGirr,

Here are my comments on the presentation of Feb 7, 2013.

Yours,

PUBLIC OPEN HOUSE #1 -February 07, 2013
COMMENT QUESTIONNAIRE

ABOUT YOU:

My name is

and my address is

I have met with Angela Taylor and the Study Team at the Cumberland Town Hall on Oct 3, 2011 and presented an idea about the possibility of widening Highway 174 by adding ½ lane on each side of the existing pavement and repainting the resulting roadway as a 3-LANE Highway.

A detailed description of this idea was distributed to the members of the study team with a plan view of the roadway from Landry Street in Clarence Creek to Trim Rd in Orleans, showing all intersections and side roads and showing all the possible turns and crossings as normal and problem-free.

PRIMARY ISSUES:

The information given tonight was well presented and the method of analysing the problem seems adequate. However, the process as it is described is very slow and can extend to several years before any physical change occurs. The need to improve the road from Clarence to Ottawa is NOW, not in 10 or 15 years from now. There may be better final solutions in 10-15 years, but some tangible improvements could and should be introduced in a shorter time frame.

ALTERNATIVE SOLUTIONS:

The 3-LANE suggestion that I made is a significant alternative to a 4-LANE highway and is re-enforced by a similar suggestion for a 3-LANE highway by Rockland Councillor Guy Félio. My suggestion is for a centre lane that is a one-way to Ottawa in the morning and a one-way to Clarence Creek in the afternoon. This gives two one-way lanes to Ottawa in the morning and two one-way lanes to Clarence Creek in the

afternoon, effectively emulating a 4-LANE highway using only three lanes. Mr Félio’s suggestion is for an improved bus system that also uses a 3-LANE roadway. Combining both ideas is an ideal solution.

My concern is that this very relevant suggestion was totally omitted in the presentation. It should have been presented as a possible alternative and if considered unacceptable for whatever reasons, then the reasons for rejecting it should have been presented.

IMPORTANT CRITERIA:

	<u>Very important</u>	<u>Less important</u>
Social	X	
Transportation	X	
Biological		X
Physical	X	
Economic	X	

ADDITIONAL CRITERIA:

An additional criterion that should be used in the evaluation is the Time Frame for real improvement. If the chosen solution will take 10-15 years, then it is not the best solution. As an example, widening the existing road to 3 lanes could be accomplished in a very short time frame, with very low costs and with no need to buy land.

CONSULTATION GROUP:

Name: (as PCG)

Organisation: A concerned citizen who has worked in Ottawa and used this highway for 36 years, and who has heard all the political promises for improvement LOL.

E-Mail:

GENERAL COMMENTS: Yes (a better understanding of the study).

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)
What specific interest do you have in this study?

Kewin Cres.
Back onto the Hwy & also
A Realtor in the area.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

The information was presented in a way that it appeared the decision of road expansion was made. The background information from the study was very basic - assuming this is so because of early stage. There was no mention of research or a part of the study concerned residential concerns. It seems to me that residential or commercial owners should be consulted first before money is spent on studies.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

The widening of the Hwy (Tern to eastern areas) would be detrimental to the area of traffic flow (other than minor widening & straightening out). The current slow downs occur at Cameron Rd lights (due to heavy traffic from the ferry coming onto the highway going other direction - New construction on the road expansion. Growth is going south so should the road expansion. Ring Road around the City is important.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

- construction go out to the south of the city.
- I attended the Rockland meeting as well as the Cumberland meeting. I was unable to understand most of the crowd conversations. So I do not know their views. I think it is important for the Cumberland area & at least myself to know the views of the other areas.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

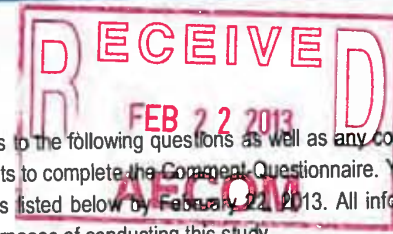
Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Thank you for your participation

Public Open House #1 -February 2013
COMMENT-QUESTIONNAIRE



The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)
What specific interest do you have in this study?

Sparkle St. Cumberland
Our back yard is on Hwy 174
our septic bed is at the back. Our garden is at the back, vegetables, raspberries, gooseberries, blueberries, apple trees. My wife's probably glad to be moved if 174 is widened.

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

development is going south. It would be better to give them a road west the developments.

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

New route south of Wilhaven doesn't give us any idea where that is.

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

septic tanks, wells, sewer main,

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☐ No ☒ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

Be more specific

Thank you for your participation

Chartrand, Danielle

From: McGirr, Valerie
Sent: Monday, February 25, 2013 7:37 AM
To: Chartrand, Danielle
Cc: Taylor, Angela (Angela.Taylor@ottawa.ca); Clermont, Marc; Phil Pawliuk; Dave Mcavoy
Subject: FW: New bridge

Follow Up Flag: Follow up
Flag Status: Flagged

Danielle:
Please add to our comments received.

Val
Valerie McGirr, P. Eng.
Manager, Ottawa Office
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

From: [REDACTED]
Sent: Friday, February 22, 2013 10:01 PM
To: McGirr, Valerie
Cc: Phil McNeely; Dalton McGuinty
Subject: New bridge

Hi:

I read the article in the Community News that mention writing to you for more detailed comments. Even though we have family in Rockland, I think and feel that we cannot accommodate more traffic from villages past Orleans until we get a ring road. The same goes for a bridge - the only option for the bridge being no bridge.

I spoke out at Phil McNeely's no-bridge meeting of a few months ago that his \$250 million to add a lane from the split to Nicolas will not alleviate the traffic problem. From 3:00 PM on, we can have a line-up of slow traffic down to a crawl from Blair to Pinecrest. We need two more lanes the full length of the Queensway and/or a long overdue ring road from Trim to the 416. Mr McNeely said that a ring road is not a solution from the results of a study done in 1996, 17 years ago (really!), because it would only be used by 10-20% of the commuters. Mr McNeely should quickly and urgently reconsider redoing that study and getting that ring road built as soon as possible, because there is a need for one. Just look at all the construction on the south side of Innis since 1996. Did McGuinty not see the need for one when he had a ridding in Ottawa? How blind can he be?

Meanwhile, most of the Queensway could easily accommodate an extra lane with a very low-cost solution, which is by reducing the width of the lanes by two feet and using the sides of the right lanes and passing lanes; this was done in larger US cities with success. This may required posting a lower speed limit during peak times. 90 km/hr is better than 30 km/hr or less with constant stops

that we go through now. Perhaps, the extra lane could be devoted to cars with 2 or more passengers.

I am coping Mr McNeely so that he seriously consider undertaking all that is required for that ring road. How come smaller cities in the states have alternate and belt roads and we don't. We just do studies... Meanwhile, the politicians should stop all construction in the East until a solution is found.

RSVP Mr McNeely. Excuses no longer acceptable. We need action.



GROUPES DE CONSULTATION

Nous recherchons des personnes intéressées à représenter leur communauté/association en tant que membres du Groupe de consultation des entreprises (GCE) et du Groupe de consultation publique (GCP). Si vous êtes intéressé(e), merci de nous indiquer vos coordonnées.

Nom: Claude H. Vachon Organisme: Société environnementale de Clarence-Rockland
 Courriel: augrgalop@bell.net Téléphone: 613-262-5609

COMMENTAIRES GÉNÉRAUX

Estimez-vous que les renseignements et le format dans lequel ces derniers ont été présentés à la séance portes ouvertes vous aident à mieux comprendre l'étude?

☒ Oui ☐ Non ☐ un peu

Si vous répondez non ou un peu, veuillez préciser les changements que nous pourrions apporter à la séance portes ouvertes ou les renseignements supplémentaires que vous souhaiteriez obtenir?

Si vous le souhaitez, laissez-nous vos coordonnées

Nom: Société environnementale de
 Représentant: Clarence-Rockland
 (le cas échéant): Claude H. Vachon
 Adresse: 2249 croissant Cécile
Rockland Ontario
 Code postale: K4K 1S4
 Courriel: augrgalop@bell.net

Merci de votre participation

Si vous souhaitez nous faire part de vos commentaires supplémentaires, transmettez-les par courrier, d'ici au 22 février 2013, à l'adresse suivante :

Valerie McGirr, P. Eng. Chargée de projet de firme de conseils
 AECOM
 302 -1150 promenade Morrison
 Ottawa, ON K2H 8S9
 Téléphone : 613-820-8282 poste. 243
 Courriel: Valerie.mcgirr@aecom.com

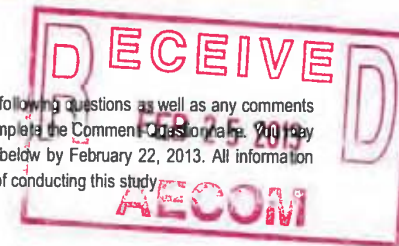


English on reverse

La Société environnementale de la Cité de Clarence-Rockland s'inquiète des travaux qui sont projetés pour la route 174 à partir du chemin Landry au chemin Trim à Orléans. La faune (cerfs et autres mammifères ainsi que les tortues) ont déjà de grandes difficultés à survivre à la circulation accrue de cette route que doivent emprunter chaque jour les travailleurs. Si nous devons absolument construire cette route, il faut absolument aménager des passages sous la route pour que ces animaux aient accès à la rivière des Outaouais. Il en va de leur survie et de notre patrimoine naturel. Des passages pour la faune sont nécessaires au niveau du ruisseau Tucker à Rockland. D'autres passages pour les tortues sont nécessaires juste à l'est de l'académie de hockey à Rockland. Un passage pour les cerfs de Virginie est nécessaire au ruisseau Beckett.

Nous croyons qu'une telle route ne règlera aucun des problèmes de circulation à long terme. Il y aura tout simplement plus de voitures en peu de temps et les embouteillages seront toujours présentes. La solution se trouve plutôt dans le transport en commun sous forme d'autobus ou de rail (train léger). Il nous faut construire une route au sud de Rockland qui rejoint le chemin Innes. Cette route serait ainsi au coeur de la Cité de Clarence-Rockland.

Public Open House #1 -February 2013
 COMMENT-QUESTIONNAIRE



The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

OWNER OF BUILDINGS and
land at Laporte / CHAMBERLAND STREETS AND
County Road 17 in Rockland ONTARIO

PRIMARY ISSUES:

Do you have any comments on the background information presented tonight?

I WOULD SUPPORT WIDENING County Road #17
TO PERMIT THE COMMERCIAL DEVELOPMENT ON
SOUTH SIDE & HIGH DENSITY RESIDENTIAL ON THE NORTH
SIDE FROM WALHART TO CANAAN ROAD

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

I WOULD LIKE TO SEE Canaan Road
COUNTY Rd. 17 having a carrefour giratoire
intersection

How important to you are the following criteria groups as they relate to the OR174/CR17 study?

	Very Important	Somewhat Important	Less Important
Social Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Transportation Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Physical Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

FUTURE DEVELOPMENT OF HIGHWAY COMMERCIAL
POTENTIAL WEST OF WALHART TO CANAAN ROAD

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name: _____ Organization: _____
 Email: _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☒ Yes ☐ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

If you wish, please provide

Name: _____
 Representing: _____
 Address: _____

Postal Code: _____
 Email: _____

Thank you for your participation

If you wish to provide us with more detailed comments, send written submissions, by February 22, 2013 to:

Valerie McGirr, P. Eng. Consultant Project Manager AECOM
 302 - 1150 Morrison Drive
 Ottawa, ON K2H 8S9
 Phone: 613-820-8282 ext. 243
 E-mail: Valerie.mcgirr@aecom.com



Français au verso

Public Open House #1 - February 2013
COMMENT-QUESTIONNAIRE

The United Counties of Prescott-Russell and the City of Ottawa would appreciate your responses to the following questions as well as any comments you wish to make about the information presented at this Open House. Please take a few moments to complete the Comment-Questionnaire. You may leave it in the designated box when you leave tonight's Open House, or send it to the address listed below by February 22, 2013. All information gathered in the Comment-Questionnaire will become part of the public record and used for the purposes of conducting this study.

ABOUT YOU:

Where do you live? (Street Name)

What specific interest do you have in this study?

Rtg. Rd #174
improving the community of Cumberland
and not desecrate the area for the purpose of the benefit of non-
City commuters who are looking to go from home to work as
quickly as possible. Cumberland (City tax payers) should not suffer from
PRIMARY ISSUES: non-city commuters.

Do you have any comments on the background information presented tonight?

Very concerned on the study, find the study has not taken a
larger area into consideration. 174 West of Town seems to be
only option and other routes that would turn up with either
Thist. Club / 417 or walkway 1417 have not been looked at. No
research has been given to the link from possible INNES RD to 174 connection

ALTERNATIVE SOLUTIONS:

Do you have any comments or specific concerns regarding the alternatives? If yes, please describe them.

The INNES RD. option
looks as the most suitable based on the reasons CCA has presented.
The city has allowed projects such as Cardinal Creek, and the town
road construction prior to this study being completed. With this
approach, concerned on finding a connecting route between Innes &
How important to you are the following criteria groups as they relate to the OR174/CR17 study? #174 not being possible -

	Very Important	Somewhat Important	Less Important
Social Criteria	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transportation Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Biological Criteria	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Physical Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Economic Criteria	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Preliminary criteria have been developed to evaluate alternative solutions. Are there additional criteria that you believe should be used in the evaluation? If yes, please describe.

Prescott-Russell residents interest of getting from point A to B as quickly
as possible should not take priority over the residents of Cumberland
and their community. This study should be looking at the long
term growth needs, which will lead to a road needed further
to the south where the growth in city will occur.

CONSULTATION GROUPS

We are looking for individuals to represent their communities/associations as members of the Business and Public Consultation Groups (BCG and PCG). If you are interested, please provide us with your contact information.

Name _____ Organization _____
Email _____ Phone: _____

GENERAL COMMENTS:

Do you feel that the information presented at this Open House has given you a better understanding of the study?

☐ Yes ☒ No ☐ Somewhat

If no, or somewhat, please describe what we could do differently, or what additional information you would like to have?

The study is not looking at a wide of an area for a solution, nor
looking at a long term perspective of the future growth and
traffic needs. Appears to be in a reactive mode to what Prescott-
Russell needs want

If you wish, please provide

Thank you for your participation

Consultation Round 2 Summary Report (Winter 2015)



AECOM
302 – 1150 Morrison Drive
Ottawa, ON, Canada K2H 8S9
www.aecom.com

613 820 8282 tel
613 820 8338 fax

City of Ottawa/ United Counties of Prescott and Russell

**Consultation Round #2 Summary Report
Confederation Line East LRT Extension/ Ottawa
Road 174/ Prescott Russell County Road 17
Widening**

Prepared by:

AECOM
302 – 1150 Morrison Drive
Ottawa, ON, Canada K2H 8S9
www.aecom.com

613 820 8282 tel
613 820 8338 fax

Project Number:

6023982

Date:

August 13, 2015

August 13, 2015

Marc R. Clermont, P. Eng.
Director of Public works
United Counties of Prescott and Russell
59 Court Street
L'Orignal, Ontario
K0B 1K0

Angela Taylor, P. Eng.
Senior Project Engineer
Planning & Growth Management Department
City of Ottawa
4th Floor, 110 Laurier Avenue West
Ottawa, Ontario
K1P 1J1

Dear Mr. Clermont and Ms. Taylor:

Project #s: 60270243 and 60323982
Regarding: Consultation Summary Report – Round 2 (January - February 2015)

The revised Consultation Report for the January-February 2015 round of consultation meetings for Confederation Line East LRT Extension/ Ottawa Road 174/ Prescott Russell County Road 17 Widening project is provided for your use. Comments received on our draft submission have been incorporated.

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Project Manager
AECOM

Encl.
cc: file

Distribution List


# of Hard Copies	PDF Required	Association / Company Name
0	1	Marc Clermont, United Counties of Prescott and Russell
0	1	Angela Taylor, City of Ottawa

Revision Log

Revision #	Revised By	Date	Issue / Revision Description
0	DK	March 2015	Draft for review
1	CG	August 2015	Final


AECOM Signatures

Report Prepared By:



David Kielstra
Consultation and Communications Coordinator,
AECOM

Report Reviewed By:



Valerie McGirr, P. Eng
Manager, Ottawa office, AECOM

Table of Contents

Letter of Transmittal
Distribution List

	Page
1	INTRODUCTION1
2	PROJECT WEBSITE.....3
3	CONSULTATION GROUP MEETINGS3
3.1	Agency Consultation Group (ACG) 3
3.2	Business Consultation Group (BCG)..... 7
3.3	Public Consultation Group (PCG)..... 9
3.4	Aboriginal Consultation..... 12
4	PUBLIC OPEN HOUSES13
4.1	Public Open House in Cumberland 14
4.2	Public Open House in Orléans 16
4.3	Public Open House in Rockland..... 19
4.4	Summary of Comments Submitted..... 24
4.4.1	Summary of Comments Submitted at the POHs 24
4.4.2	Comments Submitted after the POHs 25
5	CONCLUSION.....26

List of Figures

Figure 1	Study Area - East LRT and Highway 174/CR17 1
Figure 2	Study Area – Confederation Line East LRT Extension 2

List of Tables

Table 1.	Contacts for the Agency Consultation Group..... 4
Table 2.	Summary of Discussion at the Agency Consultation Group Meeting 5
Table 3	Contacts for the Business Consultation Group 7
Table 4.	Summary of Discussion at Business Consultation Group Meeting..... 8
Table 5.	Contacts for Public Consultation Group 10
Table 6.	Summary of Discussion at the Public Consultation Group Meeting 10
Table 7.	Summary of Questions & Answers at the Public Open House in Cumberland 14
Table 8.	Summary of Questions & Answers at the Public Open House in Orléans..... 17
Table 9.	Summary of Questions & Answers at the Public Open House in Rockland 20
Table 10.	Summary of Comments Submitted at Public Open House in Cumberland 24
Table 11.	Summary of Comments Submitted at Public Open House in Orléans 24
Table 12.	Summary of Comment Submitted at Public Open House in Rockland..... 25
Table 13.	Summary of Comments Submitted after the Public Open Houses 25

Appendices

- Appendix A.
- Consultation Notices
- Appendix B.
- Information Bulletins
- Appendix C.
- Open House Comment Log and Correspondence
- Appendix D.
- Frequently Asked Questions
- Appendix E.
- Presentations and Display Material

1 INTRODUCTION

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking an Environmental Assessment (EA) study for Ottawa Road 174 (OR 174) and Prescott-Russell County Road 17 (CR 17) corridor from Highway 417 to County Road 8 (Landry Road). This Study is being carried out in accordance with the requirements for a Schedule ‘C’ project under the Municipal Class EA process. The study area for the OR 174-CR 17 project is shown in **Figure 1**.

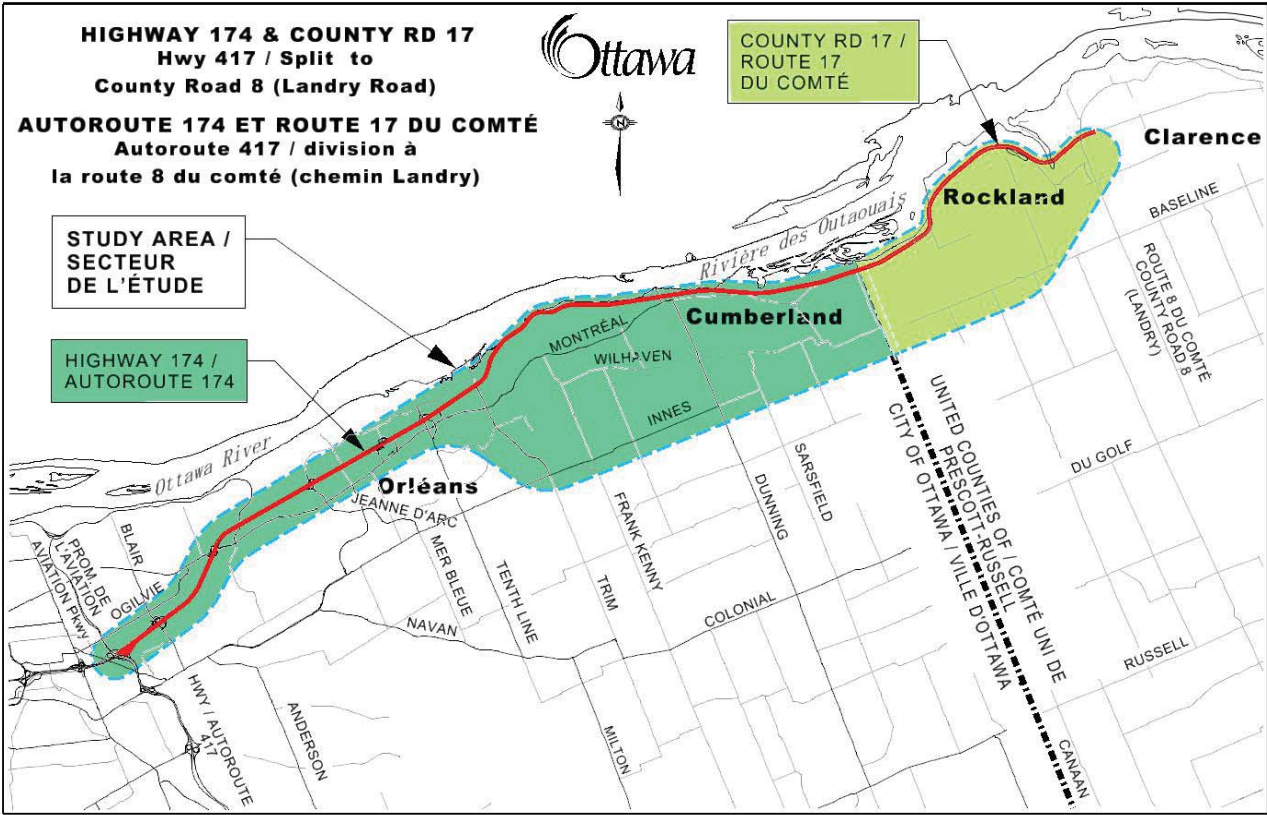


Figure 1 Study Area - East LRT and Highway 174/CR17

The City of Ottawa is also undertaking an EA Study for the extension of Light Rail Transit (LRT) from Blair Station to Trim Road along the Highway 174 corridor in accordance with the Transit Projects Assessment Process (TPAP). The ELRT line will provide fast, reliable service to the communities of Beacon Hill, Blackburn Hamlet, Orléans, and rural Cumberland. A total of eight stations are proposed between Blair Station and the Trim Park and Ride. The study area for the ELRT project is shown in **Figure 2**.

The Class EA and TPAP processes will involve developing, assessing, and evaluating alternatives, which will result in a recommended plan to be presented to the Committees/Councils of the United Counties of Prescott and Russell, (UCPR) and the City of Ottawa.

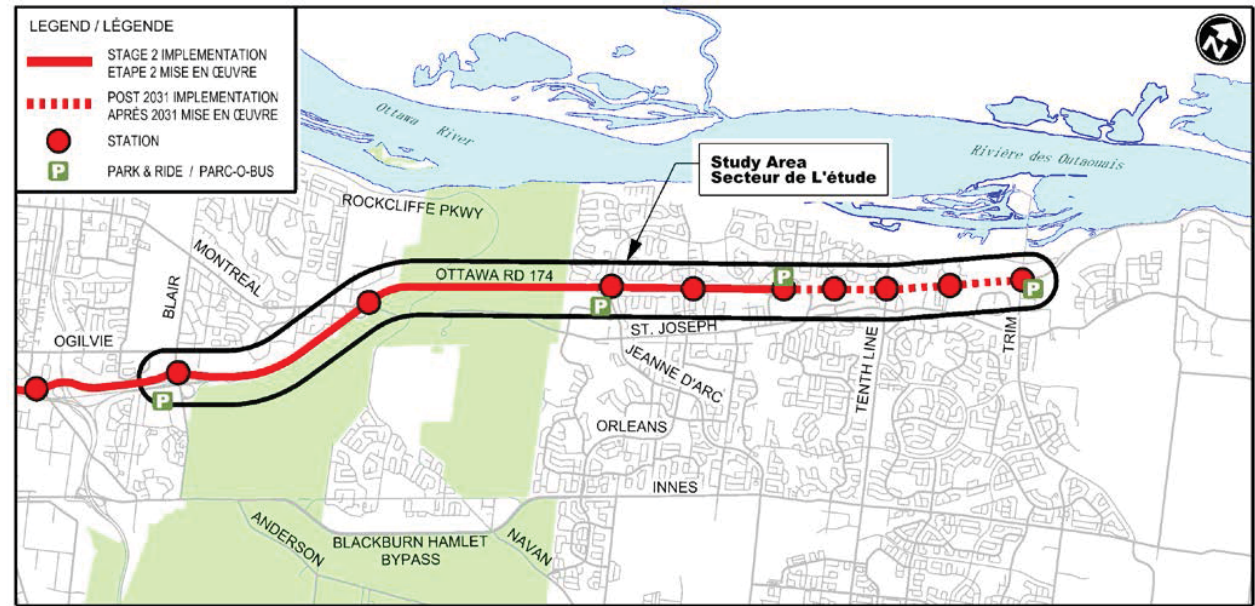


Figure 2 Study Area – Confederation Line East LRT Extension

This was the second round of public consultation for the OR174/CR17 EA study and the first round of public consultation for the East LRT Extension EA Study. Public consultation for the OR174/CR17 EA study was held in 2013 to introduce the need for alternative solutions to address problems on the OR174 and CR17 corridor. This consultation summary report covers the consultation that took place in January-February 2015.

The purpose of this second round of public consultation was to review the proposed alternatives, evaluation methodology and rankings, and to review the Preliminary Preferred Alternative for both west of Trim Road and east of Trim Road.

Consultation is an important part of each of the environmental assessment processes to effectively share project information and obtain feedback in a timely manner. The key activities during this round include:

- Consultation Group meetings:
- Agency Consultation Group – January 27, 2015
 - Business Consultation Group – January 29, 2015
 - Public Consultation Group – February 2, 2015

- Open Houses at three locations:
- Cumberland – February 3, 2015
 - Orleans – February 4, 2015
 - Clarence Rockland – February 5, 2015

2 PROJECT WEBSITE

Project information was posted on the web sites of the UCPR and the City of Ottawa at:

www.prescott-russell.on.ca/en/public-works/majorprojects
ottawa.ca/hwy174and17study
ottawa.ca/easternlrt
ottawa.ca/stage2

Public Open House notices, display material presented at the meetings and project updates will be available online throughout the study. Contact information is also provided on the web site to allow the public to comment throughout the study process.

3 CONSULTATION GROUP MEETINGS

Three separate Consultation Groups were formed to represent various agency, business and public stakeholders, which will provide direct input and feedback during the course of the study.

At each consultation group meeting, members were provided with an opportunity to:

- Obtain information on the process of the studies
- Review proposed alternatives, including:
 - Alternatives -- West of Trim Road
 - Alternative 1: LRT tracks and stations to the north of Highway 174
 - Alternative 2: LRT tracks and stations to the south of Highway 174
 - Alternative 3: LRT tracks and stations located in the median between the lanes of Highway 174
 - Alternatives – East of Trim Road
 - Alternative 1: Widen existing Highway 174/CR17 to 4 lanes
 - Alternative 2: Build a new route south of Wilhaven Drive to connect to Baseline Road
 - Alternative 3A: Improve existing Innes-Baseline Road with a new connection from Dunning to Canaan Road
 - Alternative 3B: The same as Alternative 3A, except connecting to Brian Coburn Drive at Frank Kenny Road.
- Review and comment on the evaluation methodology and rankings
- Review and comment on the Preliminary Preferred Alternative
 - West of Trim Road (Hybrid solution – north corridor before moving into the median)
 - East of Trim Road (Alternative 1 – widening Highway 174/CR17)
- Consider next steps of the project
- Provide questions or comments to the project team

Much of this information was later presented to the Public Open House (POH) sessions. Instead, the purpose of the Consultation Group meetings was to provide an alternate venue to engage members in dialogue regarding the project from their various perspectives as members of agencies, businesses or from the public.

3.1 Agency Consultation Group (ACG)

An Agency Consultation Group (ACG) was established for this project to address technical issues; comment on the special studies required to fully assess the various alternatives; and help ensure that the UCPR and City of Ottawa

follow procedures, legislation and appropriate policies. Members include federal, provincial, UCPR, City of Ottawa and other municipal representatives.

Prior to the round of Public Open Houses, a meeting was held with the ACG to obtain input. This meeting was held on January 27, 2015 from 9:00 am to 11:00 am in the Colonel By Room of Ottawa City Hall. The members of the study team in attendance included:

United Counties of Prescott and Russell	
Marc R. Clermont	Director of Public Works
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa – Project Management	
Angela Taylor	Project Manager
Jana Trembinski	Stage 2 Office

ACG Members	
Mike Michaud	Ann Selfe
David Jeanes	Steven Boyle
Arto Keklikian	David Atkinson
Florence Robinson	Michael Boughton
Mathieu Leblanc	Nick Stow
Richard Buchanan	Charles Warnock
Jeff McEwen	Michael Carneiro
Noreen Mazorra	Linda Carkner
Derek Washnuk	Cheryl McWilliams
Brent Winchcombe	Doug Rathwell

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Kelly Roberts	Environmental Lead, Parsons
David Hopper	LRT Lead, Parsons
Samantha Sabo	Environmental Planner, Parsons

Invitations to the ACG meeting were emailed to the agencies and utilities listed in **Table 1**.

Table 1. Contacts for the Agency Consultation Group

Invited Agencies to the ACG	
United Counties of Prescott and Russell:	City of Clarence-Rockland:
• Accessibility Advisory Committee	• Community Services (Recreation)
• Agricultural Advisory Committee	• Fire Department
• Economic Development and Tourism	• Physical Services
• Emergency Services	• Planning
• Planning & Forestry	Ottawa Catholic School Board
• Public Works	Catholic District School Board of Eastern Ontario
City of Ottawa:	Upper Canada District School Board
• Acquisition, Real Estate Partnership & Development Office	• Ottawa Student Transportation Authority
• Emergency & Protective Services	• Student Transportation for Eastern Ontario
• Planning & Growth Management	• Conseil des écoles catholiques du Centre-Est

Invited Agencies to the ACG	
• Construction Services	• Conseil des écoles publiques de l'Est de l'Ontario
• Design, Review and Implementation	• Conseil scolaire de district catholique de l'Est ontarien
• Development Review - East Unit	• Consortium de transport scolaire de l'Est
• Development Review - Outer Core Unit	Ministry of Infrastructure
• Development Review - Rural East Unit	Ministry of Natural Resources and Forestry
• Environmental Engineering/ Environmental Services	Ministry of Tourism, Culture and Sport
• Infrastructure Assessment - Transportation Assets	Ministry of Transportation
• Infrastructure Assessment - Water Resources Assets	Ministry of Aboriginal Affairs
• Land Use and Natural Systems	OPP
• Municipal Design & Construction East, Infrastructure Services	Rideau Valley Conservation Authority
• OC Transpo	South Nation Conservation Authority
• Planning and Infrastructure	Transport Action Canada
• Ottawa Police Services	National Capital Commission
• Rail Planning, Rail Implementation Office	National Research Council
• Right of Way Info & Approvals	Public Works and Government Services Canada
• Road Safety, Traffic Investigation & Surveys	RCMP
• Rural Affairs Office	Transport Canada
• Special Operations	Bell Aliant
• Traffic Operations	Enbridge Gas Distribution Inc.
• Traffic, Safety & Mobility	Hydro One Networks Inc.
• Transit Priority, Transit Services	Hydro Ottawa
• Transportation Strategic Planning	Rogers Communications Inc.
• Water Distribution	Videotron
• Operations - Paramedic Services, Emergency & Protective Services	

Angela Taylor welcomed ACG attendees and introduced the co-proponents of the study as well as the members of the Study Team. The ACG attendees also introduced themselves. The presentation was jointly given by Angela Taylor, Jana Trembinski, David Hopper and Valerie McGirr. A copy of the presentation is attached. A summary of the discussion is presented in **Table 2**. Stephen Blais, Councillor for Ward 19 (Cumberland) was also present.

Table 2. Summary of Discussion at the Agency Consultation Group Meeting

Summary of Discussion - ACG
SUMMARY OF COMMENTS – WEST OF TRIM ROAD
Q: Is protection for the bus rapid transit (BRT) at Blair Road being considered?
A: Yes, the Cumberland BRT corridor is protected. This corridor is faintly visible in the map that shows all of the LRT alignments.
Q: The station locations from the 2013 Transportation Master Plan (TMP) are conceptual. Have you evaluated all of these station locations?
A: All of these stations have been evaluated; some stations are more challenging to integrate than others. For example, the station at Tenth Line Road currently has no local bus access. There is also no station between Montreal Road and Blair, which can be considered.
Q: Have you considered station locations in terms of bus transfer and travel times?
A: Yes, we have taken these things into account. The functional design will include measures to make interchanges more urbanized to create a more pedestrian friendly environment.

Summary of Discussion - ACG
Q: Are you evaluating the alternatives on their own or can they be combined? <i>A: Yes, we have looked at hybrid alignments, which we will discuss shortly.</i>
Q: What are the approximate lengths of the pedestrian bridges? Is it similar to the Coventry Pedestrian Bridge? <i>A: The longest pedestrian bridge would be at Place D’Orléans. The bridges could add 2-5 minutes onto travel time, which is also influenced by the change in elevation (i.e. stairs or elevators). The bridge at Place D’Orléans will be a similar length as the Coventry Bridge.</i>
Q: Comparing to the Coventry Pedestrian Bridge isn’t appropriate, since the number of people using it will be very different than at Place D’Orléans. <i>A: These pedestrian bridges would be structurally similar. The peak load of the bridges will differ, and we will be taking that into account in their design.</i>
Q: Are these pedestrian bridges being designed for future capacity? <i>A: Yes, we are looking at the future loads, especially the “groups” of people that will use the bridges when they get off the train.</i>
Q: Does the number obtained for the median LRT from the concordance method take into account transit ridership? <i>A: Yes, ridership was one of the criteria we used to evaluate the alternatives, so it played a role in determining the outcome of the evaluation.</i>
Q: The concordance method is often considered controversial because the public doesn’t understand it. Is it possible to provide an explanation of the numbers? <i>A: A full evaluation goes behind the concordance method. At the public open houses we will be able to provide resources that explain the method.</i>
Q: Have you looked at hybrid segments? <i>A: Yes, we have looked at transition points between the alternatives. We will present the preferred hybrid alignment shortly.</i>
Q: How would access to the Canotek Business Park work? <i>A: We would provide a pedestrian walkway from the station to the north side of Montreal Road to facilitate access to the business park.</i>
SUMMARY OF COMMENTS – EAST OF TRIM ROAD
Q: If there are median barriers blocking the exits from properties along the 174, how would residents exit their properties? <i>A: They would have to turn right in and then right out of their property. There would be opportunities to perform a U-turn. We would also investigate the possibility of using roundabouts at intersections during the functional design stage of the environmental assessment.</i>
Q: Who would feel safe cycling on the shoulder on the side of the road like that? There needs to be off-road cycling opportunities as well. <i>A: We will look at alternatives for cycling and pedestrian that are separate from the road. We understand that not everyone would be comfortable cycling within the shoulder.</i>
Q: It seems like we would be able to use an urban-cross section for the majority of the stretch until Cumberland Village. We would only have to use the rural cross-section for a very small stretch. <i>A: During functional design we will select the cross-section that is appropriate for all sections, including Cumberland village.</i>
Q: Are you considering the realignment of the road to account for the mixed-use centre for Cardinal Creek? This realignment would allow for community amenities. This is an important discussion to have as it is a crucial factor for community acceptance. <i>A: At this time the OR 174 road right-of-way has been identified a minimum of 10 metres away from the riverfront, which will allow for a riverfront pathway.</i>
Q: We should focus on creating a “space and place” destination. We want the waterfront to be open and accessible. There should also be consideration for the alignment to allow waterfront access in front of Cumberland Museum. We should be focusing on these types of enhancements. <i>A: Thank you. We will take this into account during the functional design stage.</i>

3.2 Business Consultation Group (BCG)

A Business Consultation Group (BCG) was established for this project to provide input to the study and to advise and comment on local issues and concerns. Members include businesses, business associations, institutions and developers with interest in the area. Prior to the round of Public Open Houses, a meeting was held with the BCG to obtain input. This meeting was held on January 29, 2015 from 2:00 pm to 4:00 pm in the Chateauneuf Room at the Bob MacQuarrie Recreational Facility in Orléans.

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Marc Clermont	Director of Public Works
Phil Pawliuk	Project Manager
Dave McAvoy	Project Manager

City of Ottawa	
Angela Taylor	Project Manager
Jana Trembinski	Stage 2 Office
Chris Swail	Stage 2 Office

Consultant Team	
Valerie McGirr	Project Manager, AECOM
David Hopper	LRT Lead, Parsons
Kelly Roberts	Environmental Lead, Parsons
Samatha Sabo	Environmental Planner, Parsons
Patrick Déoux	Planner, MMM Group

Invitations to the BCG were emailed to individuals listed in **Table 3**. Organizations that did not respond were also called to provide them with information on the proposed work and encourage their attendance.

Table 3 Contacts for the Business Consultation Group

Invited Organizations to the BCG	
AVW - TELAV Audio Visual Solutions	Bernard Sanscartier Construction Ltd.
Brigil	C.H. Clément Construction
Mr. Gas Limited	Clarence-Rockland Construction Association
Traversiers Bourbonnais (Ferry)	Devcore Group
Swedco	DRB Development
IBI Group	Regional Development
Novatech Engineering Consultants	Taggart
Orléans Chamber of Commerce	Tamarack Homes, Cardinal Creek
Ottawa Chamber of Commerce	Spacebuilders Ottawa Ltd.
Clarence-Rockland New Chamber of Commerce	Colliers International
Greater Orleans Economic Development Council	Heart of Orleans BIA
Place d’Orléans Shopping Centre	Pro/Com Reality Corp.
Projek Design & Development Inc.	

Angela Taylor welcomed BCG attendees and introduced the co-proponents of the study as well as the members of the Study Team. The BCG attendees also introduced themselves. The presentation was jointly given by Angela Taylor, Jana Trembinski, David Hopper and Valerie McGirr.

A total of eight people attended the Business Consultation Group meeting in addition to the project team. Stephen Blais, Councillor for Ward 19 (Cumberland) and a member of his staff were also present. A summary of the discussion is summarized in **Table 4**.

Table 4. Summary of Discussion at Business Consultation Group Meeting

Summary of Discussion - BCG
Q: Are we looking at light rail transit (LRT) to Place D’Orléans or to Trim Road? <i>A: The environmental assessment study looks at the LRT all the way to Trim Road. Although the affordable network only has the LRT extending to Place D’Orléans, the ultimate transit network has LRT extending to Trim Road. We will be protecting the corridor for the eventual extension of LRT all the way to Trim Road.</i>
Q: Will there be any change to the park and ride at Place D’Orléans? <i>A: Yes, we will likely make some changes to the park and ride to accommodate additional volume. There is some land to the north of the park and ride where it could be expanded, or where transit oriented development will occur.</i>
Q: Is there provision for the storage of trains somewhere along this extension? <i>A: Yes, there will be a tail track at the end of the alignment in case a train requiring maintenance needs to be pushed out of the way until it can be taken back to the maintenance facility. Additional trains can also be stored here overnight to help build the morning peak service.</i>
Q: Why can’t the LRT go where the bus rapid transit (BRT) was supposed to be to the south of the Highway 174 corridor? <i>A: Since there are hydro poles to the south, it would interfere with the electrical system of the LRT. The hydro poles would need to be relocated, which would be time-consuming and costly.</i>
Q: Is there a discussion between the City of Ottawa and the United Counties of Prescott-Russell to extend the LRT all the way to Rockland? <i>A: Not at this time. Rockland does not presently have the ridership for this type of extension to occur.</i>
Q: Will the park and ride lot at Cumberland be expanded? <i>A: We are not explicitly looking at this for the environmental assessment study, but OC Transpo is likely investigating the potential for expansion in the long term.</i>
Q: It seems there is an issue of security at these stations. At rush hour it’s acceptable to use these services but at night security could be an issue. I know some women that will simply not take transit at night because they don’t feel safe. <i>A: We are taking into account the safety of users in station design. For example, the stations are designed to be open and have long vistas so that users can see who else is in the station with them. The stations are also designed so passengers can see the approaching trains from the pedestrian bridge level, allowing them multiple exits if they feel confined. We will be employing crime prevention through environmental design (CPTED) for the safety of transit users.</i>
Q: Orléans is presently the largest user of OC Transpo. Is this going to be maintained or increase in the future? <i>A: Orleans is predominantly a residential neighborhood so many of its residents choose to commute via transit. We are hoping that this investment will enhance ridership. Transit-oriented development will also occur, which will make transit even more attractive.</i>
Q: Will one of the lanes on Highway 174 be designated for high-occupancy vehicles (HOV)? Would rural transit be able to use these lanes? <i>A: We are protecting for HOV along the corridor but the decision to convert to HOV has not been made at this time. Buses from rural areas would be able to use the HOV lane but they will stop off at the easternmost LRT station, and the passengers would use the LRT to continue further west.</i>
Q: Are we looking at a four lane road all the way to Rockland? <i>A: Yes, alternative 1, which involves the widening of Highway 174 and County Road 17, would be widened to have 4 lanes total.</i>

Summary of Discussion - BCG
Q: Is Innes an option for LRT? <i>A: The Cumberland Transitway south of Innes is included in the City's long term plans as a bus option.</i>
Q: Wouldn’t it be better to have 3 lanes, and have one of those reversible? It seems like space would be an issue to widen to 4 lanes. <i>A: We performed an analysis to see whether having reversible lanes would be feasible along this portion of Highway 174. Based on a number of considerations, reversible lanes were not considered to be an acceptable solution in this context. Safety issues are a major consideration with reversible lanes. We would also not be able to provide physical separation in the form of a barrier.</i>
Q: Putting bike lanes and sidewalks on a highway like that does not appear safe. <i>A: These types of cross-sections would be used in more built-up/urban areas. Providing accommodations for pedestrians and cyclists is important in allowing for complete streets, which provides opportunities for more than just cars. We will be following this type of design in Cumberland as well as in Rockland.</i>
Q: What would the speed limit on this road be? I know the residents have been advocating for speeds to be reduced. A person biking on the shoulder of a highway doesn’t seem safe. <i>A: This road is being designed as an arterial. Rural arterials generally have a posted speed limit of 80 km/h. The paved shoulders improve conditions for commuter cyclists who are comfortable cycling under these conditions. During the detailed design phase we will investigate off-road cycling opportunities.</i>

3.3 Public Consultation Group (PCG)

A Public Consultation Group (PCG) was established for this project to provide input to the study and to advise and comment on local issues and concerns. Members include community associations and resident groups, Advisory Committees and advocacy groups.

Prior to the Public Open Houses a meeting was held with the PCG to obtain input. This meeting was held on February 2, 2015 at 6:30 pm in Room 350 at 255 Centrum Boulevard in Orléans.

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Marc R. Clermont	Director of Public Works
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager
Frank McKinney	Program Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Kelly Roberts	Environmental Planner, Parsons
David Hopper	Parsons
Samantha Sabo	Parsons

Letters of notification were mailed or emailed to community groups and interested parties listed in **Table 5**.

Table 5. Contacts for Public Consultation Group

Stakeholders Invited to the PCG	
Beacon Hill Community Association	Carson Grove and Rockcliffe Mews Residents Association
Blackburn Community Association	City of Ottawa, Cumberland Heritage Village Museum Board
Bradley Estate Community Association	Citizens for Safe Cycling
Cardinal Creek Community Association	Cycle Vision
Chapel Hill South Community Association	Ecology Ottawa
Cumberland Village Community Association	Federation of Citizens Association of Ottawa-Carleton
Fallingbrook Community Association	Greenspace Alliance of Canada's Capital
Navan Community Association	Heritage Ottawa
Sandpiper Cove Community Association	Ottawa Riverkeeper
Rothwell Heights Property Owners Association	Save the Parkway
Village Association (Chatelaine Village, Queenswood Village, Riverwalk)	Access Now
Bungalows of Notting Gate Community Association	Convent Glen / Orléans Wood Community Association (CGCWCA)
City of Ottawa, Accessibility Advisory Committee	Portobello South Community Development Association
City of Ottawa, Arts, Culture, Heritage and Recreation	Orléans Chaperal Community Centre
City of Ottawa, Community Services	Queenswood Heights Community Association
City of Ottawa, Cumberland Heritage Village Museum Board	Sarsfield Community Association
City of Ottawa, Environmental Stewardship	Société Environnementale de Clarence-Rockland
City of Ottawa, French Language Services	

Angela Taylor welcomed PCG attendees and introduced the co-proponents of the study as well as the members of the Study Team. The PCG attendees also introduced themselves. The presentation was jointly given by Angela Taylor, David Hopper and Valerie McGirr.

A total of 8 people attended the Public Consultation Group meeting held on February 2, 2015. In addition, Stephen Blais, Councillor for Ward 19 (Cumberland) was present. A summary of the discussion is summarized in **Table 6**.

Table 6. Summary of Discussion at the Public Consultation Group Meeting

SUMMARY OF COMMENTS – WEST OF TRIM ROAD
Q: Some of the stations will only have pedestrian access. Will there be bus access and/or an expansion of the park and rides? <i>A: The majority of stations will have pedestrian and cycling access. There will be bus access to the majority of stations. We are working with OC Transpo to develop the best bus access to stations. The park and ride at Place D'Orléans may be expanded, but no other park and rides are proposed at this time.</i>
Q: Are the criteria that you have listed here only for the light rail transit (LRT) component of the project? <i>A: No, for the section west of Trim Road we are looking at an integrated solution for the LRT and highway widening, and therefore these criteria are for both the LRT and the highway widening.</i>
Q: How would you park your bike at these stations? <i>A: There will be bicycle parking at all stations. We will be working with the City of Ottawa to design a sufficient number of bicycle parking spots to accommodate the volume at each station. Different stations will have different volumes, and will therefore have a different number of bicycle parking spots.</i>

Q: I understand that the criteria used for both the LRT and the highway widening is the same. How did you come up with the weights? <i>A: Members of the study team from both the City of Ottawa and the consultant teams assigned weights to each of the measurements. For example, among our specialists, there were biologists, transportation experts, urban planners, etc. These specialists each provided weights, and then these weights were averaged to obtain an overall blended weight for each category.</i>
Q: Is there input from the public for these weights? <i>A: Not at this time. If issues are raised by the public, these weights can be looked at in more detail. However, the sensitivity analysis we performed indicates that our analysis is robust in the sense that no matter which way we changed the weights in our sensitivity tests, the Median LRT was always preferred.</i>
Q: Is the cost of each of these stations the same no matter what? <i>A: Although each station will have similar components, there are local conditions that would influence the costs of the stations. For example, certain stations would require us to install retaining walls, which would increase the cost of that particular station. In the case of the underground station at Place D'Orléans proposed as part of the south alternative, an underground station is considerably more expensive than an aboveground station.</i>
Q: Were the locations of these stations selected to maximize use? <i>A: The 2013 Transportation Master Plan (TMP) conducted a preliminary screening of all stations, but we will be looking at the station locations in more detail as part of this environmental assessment. Some stations will have higher use than others. We use transportation models to assess the most suitable locations for stations.</i>
SUMMARY OF COMMENTS – EAST OF TRIM ROAD
Q: Why is the study area so confined to the south? Couldn't you have looked at routes even further to the south of Innes and Baseline? <i>A: We identified our study area so that our solution would serve existing urban areas and serve the predominant flow of traffic. Furthermore, we are constrained by the Mer Bleue bog to the south.</i>
Q: Would people from downtown take Innes Road out to Rockland? <i>A: The present tendency is for people to take Highway 174 eventually. Our assessment examined other alternatives than widening Highway 174 and County Road 17. Alternatives 3A and 3B, which involved providing a new connection from Innes to Baseline Road, were also part of our assessment.</i>
Q: Why haven't you thought of a three lane highway with reversible lanes from Trim Road to Landry Road? <i>A: We did a review and assessment of whether reversible lanes would be suitable in this context. Particular considerations such as safety and costs indicate that a reversible lane is not appropriate along Highway 174 and County Road 17. The costs of putting overhead signage that designate the direction of traffic in the centre lane would be very expensive. Furthermore, for safety reasons, we need to put a barrier between lanes of different directions. Using reversible lanes over long distances and at highway speeds is not a feasible option.</i>
Q: Why isn't Brian Coburn used as an arterial? The study area didn't include this. <i>A: Our Alternative 3B is a connection to Brian Coburn.</i>
Q: What is a HOV lane? <i>A: A HOV lane is a high-occupancy vehicle lane. It is a lane that is usable by taxis, buses, and vehicles with two people or more.</i>
Q: Can you explain what you mean by 4 lanes of capacity? <i>A: The first alternative involves widening Highway 174 and County Road 17 to 4 lanes (2 in each direction). All the other alternatives will be new 2 lane routes or new 2 lane connections to existing roads. The transportation demand management model indicates that we need a total of 4 lanes of capacity to serve the predominant flow of east-west traffic. In options 2, 3A and 3B, we would create an additional 2 lanes of capacity. We would then add these 2 lanes to the existing 2 lanes of capacity that we have on the existing Highway 174 to get a total of 4 lanes of capacity. These lanes of capacity are spread out among 2 different routes if we choose alternatives 2, 3A or 3B. If we choose alternative 1, all 4 lanes of capacity are along the same corridor.</i>
Q: If your preferred alternative is the widening of the 174/17, and there will be a barrier, how am I going to access my property? I presently live along the 174. <i>A: Due to the presence of the barriers, property owners would have "right-in-right-out" access. In other words, you would have to proceed to the nearest signalized intersection, roundabout, or U-turn location to change direction.</i>

Q: Why would anyone cycle down the side of a highway? <i>A: We are paving the shoulders to allow for commuter cyclists. We will also be looking at the best place to create multi-use pathways that are separate from the highway, for those people who do not feel comfortable cycling in the shoulder.</i>
Q: Is there room right now to build the 4 lanes on Highway 174 east of Trim Road? It seems like there are many locations where this cross-section you are showing us wouldn't fit. How wide would this cross-section be? <i>A: There is sufficient right-of-way available in many locations. There are some locations where additional right-of-way will be required. The cross-section we are showing is conceptual in nature, and will be refined during the detailed design stage of the environmental assessment. The cross-section is around 30 metres, but we are looking at ways to incorporate features that are important to the community, and to remove those that are not necessary.</i>
Q: If you have to modify an existing highway, do you have to follow standards as if you were building a new highway? <i>A: The construction of these roadways will be done to meet all safety standards. We are not able to compromise on matters surrounding the physics of highway design (e.g. curves, hills, etc.). The design will also consider social factors.</i>
Q: You are forcing people to live with the same highway we have had for the last few decades. The new rules shouldn't be followed if the new rules aren't right. <i>A: Your concerns will be taken into account and we will find ways to reduce dimensions where it will not compromise safety.</i>
Q: If the cross-section you are proposing doesn't work east of Trim Road, than is this corridor still going to work in the future? <i>A: All of our studies look at future growth and trends in transit and vehicle use. Our transportation models show that the road along the river is preferred and will be used to capacity first, followed by other routes as needed to address traffic demand.</i>
Q: Putting in roundabouts, signalized intersections and reducing speeds seems to conflict with the idea of giving Rockland a highway to downtown Ottawa. <i>A: Transit is going to be a big focus in the future. We are taking into account the potential for HOV lanes along the 174, which would further encourage transit. Rockland is becoming more urbanized, and their use of transit is likely to increase in the future.</i>
Q: How many people are going to lose their homes as a result of the widening? What kind of land is going to be expropriated? Every single person that lives along the highway is worried. <i>A: No homes will be required as a part of this widening. At the next stage of the environmental assessment, where we develop the functions design, we will be able to determine property requirements in more detail. We weighted social factors very highly in our analysis and these concerns are being taken into account in our design. Any property that would need to be purchased would be purchased at market value. Property owners would be compensated appropriately.</i>
Q: What would it take for your preferred alternative to be changed? <i>A: If there was significant opposition from the community to the evaluation then we would need to re-visit our selection.</i>
Q: The issues of these people need to be addressed. <i>A: Yes, we agree. We will have a dialogue with the people who would potentially be affected. We will have these answers for you in the late spring when we have developed a more detailed design.</i>
Q: How do you plan on moving the electrical substation and the houses along the escarpment? <i>A: The houses in this area are set back far enough that they will not be affected. We are in discussion with Hydro One regarding their requirements.</i>

3.4 Aboriginal Consultation

Aboriginal groups were invited to attend the Agency Consultation Group meeting on January 27, 2015 or to contact the study team to discuss an alternative consultation format that better suited their needs. Invitation letters were emailed or mailed to:

- Algonquins of Ontario Consultation Office
- Algonquins of Pikwakangan
- Kitigan Zibi Anishinabeg First Nation
- Métis Nation of Ontario

Note: following the August 2014 TPAP update notification, the Quebec Métis Nation indicated that it did not have an interest in the proposed project. As a result, no further notices have been provided.

The Study team met with representatives of the Algonquins of Ontario (AOO) on February 6, 2015 to discuss their interests. One of the parcels included in the land claim settlement of the Algonquins of Ontario is along OR 174, west of Quigley Hill Road. Access to the land parcel, which slopes towards the Ottawa River, was discussed, as well as the potential addition of a consolidated driveway should the parcel be sub-divided. The importance of thorough archaeological work was noted and the study team will be providing the AOO with all archaeological reports done as part of the study. AOO representatives were interested in exploring opportunities to incorporate linkages to their history through various means such as interpretive pathways, identifying trees and noted access to water was important to them. Consultation with the Algonquins of Ontario will continue throughout the study process.

Follow-up with other groups will be undertaken by the study team to obtain their input for this study.

4 PUBLIC OPEN HOUSES

One of the key objectives of the environmental planning process is to provide the public, interested parties and affected agencies with opportunities for meaningful input. To help meet this objective, public notification of the round of Public Open Houses (POHs) was undertaken. Newspaper notices were placed in local newspapers on Friday January 23 and 30, 2015. In addition, the notice was placed on the project web sites at:

www.prescott-russell.on.ca/en/public-works/majorprojects
ottawa.ca/hwy174and17study
ottawa.ca/easternlrt
ottawa.ca/stage2

The notice was also mailed or emailed to individuals identified on the study contact list. A copy of the English and French notice is provided in **Appendix A**.

Due to the size of the study area and the number of communities affected, Public Open House meetings at three different locations on separate nights were included in the process. The information presented and presentation format were generally the same at each meeting with an emphasis on the areas of interest of attendees. Bilingual Information bulletins (handouts) were available at each POH (provided in **Appendix B**).

The POH meetings were held from 6:30 p.m. to 9:00 p.m., with a presentation at 7:00 p.m. on:

Tuesday February 3, 2015	Wednesday February 4, 2015	Thursday February 5, 2015
R.J. Kennedy Memorial Community Centre 1115 Dunning Road, Cumberland	Bob MacQuarrie Recreation Complex 1490 Youville Drive, Orléans	Guy Faubert Hall 954 Giroux Street, Rockland

At each POH location there was an opportunity to:

- Obtain information on the process of the studies
- Review proposed alternatives, including:
 - Alternatives -- West of Trim Road
 - Alternative 1: LRT tracks and stations to the north of Highway 174
 - Alternative 2: LRT tracks and stations to the south of Highway 174
 - Alternative 3: LRT tracks and stations located in the median between the lanes of Highway 174

- Alternatives – East of Trim Road
 - Alternative 1: Widen existing Highway 174/CR17 to 4 lanes
 - Alternative 2: Build a new route south of Wilhaven Drive to connect to Baseline Road
 - Alternative 3A: Improve existing Innes-Baseline Road with a new connection from Dunning to Canaan Road
 - Alternative 3B: The same as Alternative 3A, except connecting to Brian Coburn Drive at Frank Kenny Road.
- Review and comment on the evaluation methodology and rankings
- Review and comment on the Preliminary Preferred Alternative
 - West of Trim Road (Hybrid solution – north corridor before moving into the median)
 - East of Trim Road (Alternative 1 – widening Highway 174/CR17)
- Consider next steps of the project
- Provide questions or comments to the project team

Display panels were prepared to describe the progress of the studies and to highlight key issues. The Open House information and presentation was posted to the project website. Project staff members were on hand to answer comments and questions, and to encourage individuals to provide their input in writing on available comment sheets or by email until February 13, 2015.

Open House Comments

The following table provides a summary of open house comments expressed by attendees of the February 2015 Open Houses. Comments provided by email or on comment sheets left at the POH are summarized in later sections.

4.1 Public Open House in Cumberland

A total of 139 people signed the registration sheet at the POH held in Cumberland. Before the formal presentation, attendees were encouraged to review the bilingual display material and discuss the studies with staff.

During the formal presentation Angela Taylor, City of Ottawa, welcomed attendees and introduced the Study Team. The presentation was given jointly by Angela Taylor, Jana Trembinski, David Hopper and Valerie McGirr. Patrick Déoux provided a summary of the presentation in French and was available to assist attendees with their questions in French. A question and answer period followed the presentation and the discussions are summarized below in Table 7.

Table 7. Summary of Questions & Answers at the Public Open House in Cumberland

Summary of Questions & Answers – POH in Cumberland
SUMMARY OF QUESTIONS & ANSWERS – WEST OF TRIM ROAD
Q: Will there be additional lighting along Highway 174? <i>A: Lighting is one of the many components that we will look at during the functional design stage, which is the next phase of the environmental assessment. What we can tell you at this time is that the lighting that will be incorporated in accordance with road design standards.</i>
Q: What does HOV mean? <i>A: HOV stands for high-occupancy vehicle. A lane that is designated for HOV would be a lane that is designated for use by only taxis, buses, and other vehicles with 2 or more people in the car.</i>
Q: Did you only look at putting the light rail transit (LRT) along only the Highway 174 corridor? <i>A: The Transportation Master Plan (TMP) looked at putting the LRT along the Cumberland Transitway. Based on operating costs, it was determined that putting the LRT along Highway 174 was a more feasible option.</i>

Summary of Questions & Answers – POH in Cumberland
Q: There will be more traffic between the LRT and along the connecting arterials. Will these arterials have the capacity to handle the additional traffic? <i>A: Our transportation models look at future demand and based on this information, we believe that there will still be some congestion during the peak period, but these arterials will function as required. We are looking at making modifications near the stations to provide the required access.</i>
Q: How would you access the LRT in an emergency? How would ambulances get to passengers on the train? <i>A: There will be a walkway created along the length of the tracks so that in the very unlikely event that the train was stopped between stations, passengers would be able to exit and use the walkway to access the nearest station. Ambulances would directed to the station entrances. If there were a significant event, emergency services would be able to access the tracks from the highway.</i>
Q: If there are no more crossovers along the highway due to the LRT being there, then emergency vehicles will take longer to change directions if they are already on the highway. <i>A: We will be working closely with emergency services to identify quick and reliable access along the highway corridor using the interchanges for emergency vehicles.</i>
SUMMARY OF QUESTIONS & ANSWERS – EAST OF TRIM ROAD
Q: When would you ever put a bike lane next to a highway? <i>A: It is part of the City of Ottawa policy to pave shoulders. The cyclists that would use these paved shoulders are usually commuter cyclists. We are also considering multi-use pathways away from the road for users who would not feel comfortable cycling on the shoulder.</i>
Q: How much of our property will be taken for these bike paths? <i>A: The location and width of our cross-sections are determined in the functional design stage, which is the next phase of the environmental assessment. During this next stage we would be able to provide you with more information on whether any property would be required.</i>
Q: You have provided us with an example of a median LRT. Can you provide us with an example of a 4 lane highway next to a river? <i>A: Examples in the City include portions of Riverside Drive, Sir John A. MacDonald Parkway, as well as Sir George Etienne Cartier Parkway.</i>
Q: What is the difference between a 4 lane arterial and a 4 lane freeway? <i>A: The difference is that an arterial would have signalized intersections and driveway access, whereas a freeway would have interchanges and ramps to provide access with no direct access from private property.</i>
Q: When will you widen the highway? <i>A: Since the widening of Highway 174 and County Road 17 is not in Ottawa’s affordable network as outlined by the Transportation Master Plan (TMP), the widening is presently expected to occur after 2031.</i>
Q: What is the date by which a concrete decision will be made regarding the widening of Highway 174? <i>A: The project needs to complete the environmental assessment process. The final report will be presented to committees/council in Ottawa and Prescott and Russell prior to public review of the documents. The exact date for these meetings has not been determined at this time, but is likely to occur sometime this year.</i>
Q: What would constitute significant opposition to this project? <i>A: We understand that there may be concerns regarding this project. We will take into account the comments submitted as part of this consultation process. Please submit your concerns on the comment sheet or send us an email.</i>
Q: How have you made a selection of a preferred route without considering cost? <i>A: Costs were one factor in our evaluation. . At a high level, costs of each of these routes have been taken into account.</i>
Q: Why don’t you build a road and make it connect south to Hunt Club Road? It is ridiculous that you haven’t included these options to the south. <i>A: As part of the environmental assessment process, we are looking to address a particular problem. In this case, we have to look at the demand along the whole corridor to determine the best possible solution. A connection south to Hunt Club Road would not serve the predominant direction of traffic, which is to and from downtown Ottawa. There is, however, a new connection from</i>

Summary of Questions & Answers – POH in Cumberland
<i>Innes Road to Hunt Club Road at Highway 417 that is also included in the ultimate transportation network of the City of Ottawa but not the affordable network. Both projects are needed. The internationally-recognized Mer Bleue Bog is a major constraint to the identification of alternative routes between Highway 417 and Orléans.</i>
Q: How much traffic does Cumberland generate? <i>A: As part of our analysis we have performed traffic counts. Please leave this question in a comment sheet or send it to us by email so we can provide you with the numbers you are looking for.</i>
Q: Why haven’t you considered a ring road? Every other major city has a ring road, and it seems stupid that you have not considered this in your analysis. <i>A: The 2013 Transportation Master Plan looked at the creation of a ring road. The transportation models we carry out look at where people are coming from and where people want to go. The models look at which areas will grow and how residents in communities get where they need to go. We are not considering a ring road at this time because it would not provide a solution to our current transportation needs. Ring roads need to be paid for by the province, and Ontario is not considering the creation of a ring road at this time.</i>
Q: Why would you build a road with only access on one side, since the Ottawa River is the north? (Comment)
Q: Are you saying that the expropriation of homes is acceptable, but the expropriation of farmland is not? <i>A: No homes will be needed as part of the proposed highway widening. During the functional design stage of the environmental assessment we will investigate the widths of property frontage required. At that time, we would be able to provide you with more information on whether any property would be required.</i>
Q: What do your traffic projections show east of Canaan Road? Can the widening Highway 174 even handle this? <i>A: Please record this question in a comment sheet or send us an email so that we can provide you with this information.</i>
Q: Will there ever be a bridge where the ferry is? <i>A: A bridge will not be considered at the ferry.</i>
Q: I don’t understand why these options need to be mutually exclusive. Why can’t we upgrade the highway and then build one of the other alternatives? Was this even considered? <i>A: Unfortunately there are insufficient funds to widen Highway 174 to 4 lanes and to build one of the other 2 lane alternative routes we are proposing. Funding is often a constraining factor in our infrastructure projects so we need to prioritize.</i>
Q: How much money has been wasted on these environmental assessments over the years? <i>A: I do not know the cost of the previous studies that have been performed on this corridor, but \$5 million was provided by the Province of Ontario for the current EA study.</i>
Q: What will be the speed limit along this road? <i>A: The speed limit has not been set at this time. We expect that speed limits will be lower in urban areas (50-60 km/h).</i>
Q: How do I turn out of my property if I am located on the south side and want to go westbound on the 174? <i>A: Residents with driveways along the 174 will have right-in right-out access. We will be looking to provide U-turn opportunities, and will also be developing whether there will be signalized intersections or roundabouts.</i>
Q: Making a U-turn on a highway isn’t safe. <i>A: Safety is a priority in designing access and we will provide u-turn locations where this movement can be completed in a safe manner.</i>
Q: What happens if I have a septic system near the road and you will need to expropriate that? <i>A: These types of needs will be addressed on a case by case basis if any land is required. We understand that every situation is different and we would be taking into account your individual needs.</i>

4.2 Public Open House in Orléans

A total of 151 people signed the registration sheet at the Public Open House meeting held in Orléans, although a head count at the event identified 164 individuals present indicating some may not have signed in. Before the formal presentation, attendees were encouraged to review the bilingual display material and discuss the studies with staff.

Angela Taylor, City of Ottawa, welcomed attendees and introduced the Study Team. A presentation was given jointly by Angela Taylor, Jana Trembinski, David Hopper and Valerie McGirr. Patrick Déoux provided a summary of the presentation in French and was available to assist attendees with their questions in French.

A question and answer period followed the presentation and discussions are summarized below in **Table 8**.

Table 8. Summary of Questions & Answers at the Public Open House in Orléans

Summary of Questions & Answers – POH in Orléans
SUMMARY OF QUESTIONS & ANSWERS – WEST OF TRIM ROAD
Q: Where will the parking be for all of the proposed light rail stations? <i>A: There are existing park and rides at Place D’Orléans and at Trim Road. Some expansion of these park and rides may occur, but no additional park and rides are proposed at this time.</i>
Q: Will there be bus access to all of the stations? <i>A: Yes, there is existing bus service along the majority of routes where stations are proposed. We are also in discussions with OC Transpo regarding the addition of new routes or alteration of existing routes, if required.</i>
Q: Won’t the interchange piers be in the way if you put the LRT station in the median? <i>A: These piers won’t pose an issue because there is sufficient width for the stations and the platforms to be built around them.</i>
Q: I heard that development on the north side of Place D’Orléans will be taking over some of the land presently used for the park and ride. <i>A: I am not familiar with this plan. Transit-oriented development is being proposed in the large vacant lot adjacent to the park and ride. We are considering the potential need to create structured multi-level parking in this area.</i>
Q: Is there any talk of giving Highway 174 back to the province? <i>A: Ownership and jurisdictional discussions have occurred and will likely continue to occur. However, at this time, no transfer of ownership to the Province of Ontario is planned.</i>
Q: Will all of the buses that come from Orléans, Cumberland and further east still go downtown once the LRT is built? <i>A: No, our plan is to have all passengers riding the bus in Orléans to transfer to the LRT to continue west to downtown.</i>
Q: So we will all have to transfer to the LRT? <i>A: Yes, but we believe that although passengers will have to transfer, there will be some time savings since the LRT will provide a faster, smoother and more reliable trip into the downtown core.</i>
Q: Is there going to be increased bus service on the north-south arterials in Orléans? <i>A: We expect to have increased bus service to allow more frequent access to the LRT stations. The trip frequency will also be increased over time based on demand.</i>
Q: Why do we have 10 different buses in downtown Ottawa? Why couldn’t there just be one? <i>A: The LRT located downtown will replace much of the bus service in this area.</i>
Q: Assuming the trains hold 600 people or however much they hold, how will all the buses at the stations handle all of these people transferring? <i>A: Each station will be sized appropriately based on usage. We will have enough bus capacity for all people transferring. Not all 600 passengers would be getting off at Blair to go east into Orléans. Many passengers would get off at other stations, and there will still be multiple routes serving the Orléans area.</i>
Q: There is an issue in Beacon Hill related to vibration. The City told us that there is no money to fix the vibration, which was found to be two times over the threshold! <i>A: The LRT will actually cause less vibration than a bus would, since the weight of the train is dispersed among many wheels on the train. We will still investigate vibration and noise, and will include mitigation measures if any impacts are identified.</i>
Q: How does the community in Blackburn Hamlet fit in? <i>A: There would be bus service providing access to nearby stations. We will likely still have the 98 bus to Blair Station. We will be working closely with OC Transpo to determine how the bus service will fit in with the stations we are proposing.</i>
Q: Will you be putting in sound barriers on the northwest side of Jeanne D’Arc?

Summary of Questions & Answers – POH in Orléans
A: We will be investigating noise impacts in greater detail and provide mitigation measures if they are required. Also, since the LRT is located within the median, the concrete barriers present to both sides of the LRT will also help to attenuate any noise that originates from the wheels of the train.
Q: Will you be putting double tracks all the way to Place D’Orléans?
A: Yes, there will also be locations for crossovers to allow the trains to change direction and travel back into the City.
Q: Will any of the trains be stored along this area?
A: We will have a tail track to the east of the terminal station that will allow for an extra train or two to be stored for the morning peak service. Our train storage will be at Belfast yard, but we are also looking at whether this yard can handle the capacity we need.
Q: Since you are looking at both the LRT and the highway study, why don’t you design the road bed to absorb vibrations?
A: We will document the issue of vibration and identify mitigation measures.
Q: Will all of your stations have that brutalist red steel design that OC Transpo has been using everywhere else?
A: We will be looking to update the design of the stations. The Confederation Line station concepts are likely to be applied.
Q: What will be the frequency of trains?
A: At rush hour the trains will come every 3 to 4 minutes, during mid-day it would be on the order of 6 to 10 minutes, and would likely be spaced even more so in the evening. The frequency is based on demand and if there is demand, there will be more frequent trains.
Q: When will the LRT be opening?
A: We are aiming for 2023 at this time.
Q: I really think that your cost analysis should extend to the Trim Road park and ride, since it is already over capacity.
A: Unfortunately all of our projects have a limit of affordability. We will be looking at protecting for future stations, but deferring their construction until we have money to construct them. OC Transpo will likely be looking at expansion to the Trim Road park and ride.
Q: Are you assuming that building the LRT will improve ridership?
A: Yes, based on our analysis, the ridership is likely to increase.
Q: What if the LRT doesn’t work, and people don’t use it?
A: I can say that in all my years of experience with light rail, I can think of only 1 or 2 examples of there being poor ridership. The reasons these examples failed is because they built the train in areas where people wouldn’t use them. It has truly been successful for the majority of examples that I can think of. Orléans already has very high transit ridership and we know that as we add more service ridership goes up. Planning is key to designing a system that works for the passengers.
Q: Why do we need the LRT if you are widening the highway?
A: Transit is Ottawa’s first priority and is needed to increase ridership to achieve the objectives of the Transportation master Plan. The LRT will be built before widening of the highway. Transit has always done very well in Orléans, and we believe that providing the LRT will make it even more attractive. But LRT and transit can only accommodate a portion of the overall demand for travel and ultimately a widening of the highway is also needed to meet demand.
Q: How sensitive are your cost estimates to demand, ridership, and the number of trains required?
A: The transportation model we use looks at travel patterns and behaviour. We believe that we have estimated the number of trains effectively based on our projections. However, the City can go back to the manufacturer and buy more trains at a guaranteed price if there is demand.
Q: I have had cases where I am taking the bus at Ottawa U campus and the buses are full and drive by me. Is there going to be room at these train stations?
A: Unlike buses, the train will have to stop at every station, so there will be an opportunity for you to get on. We will be monitoring how many users there are and if we need to add more trains, we will add more trains. OC Transpo has a loading standard that will have to be met by the train operator.
Q: Will you put in a HOV lane if you are widening on the 174?
A: We are planning a third lane that can be designated as a high occupancy vehicle (HOV) lane. We are protecting for the extra

Summary of Questions & Answers – POH in Orléans
width that would be required to have a buffer between the HOV lane and general purpose lanes. The actual decision on whether it is operated as a general or HOV lane will be made later.
Q: What kind of noise is created by the train? I couldn’t stand having a train chugging along in addition to the highway noise.
A: Since the LRT will have a continuously welded track and is electrically driven, the train will be a lot quieter than the typical train noise you are thinking of. They are not at all like the VIA trains or even as noisy as the Trillium Line trains.
SUMMARY OF QUESTIONS & ANSWERS – EAST OF TRIM ROAD
Q: How wide would the bike lanes be?
A: Multiuse pathways would be approximately 3 metres, or 10 ft in width. The paved shoulders are 2.5m in width
Q: You should have widened the highway from the split to Trim Road 10 years ago. Are you saying we have to wait until after 2031 for any improvements to be made?
A: Some of the widening may occur when the LRT is installed. A good portion of the highway is already 3 lanes, including the shoulder bus lanes. With the LRT, we won’t need the bus lanes anymore.
Q: Growth along the river will be too much for the highway to handle even if you widen it.
A: Every 5 years, the Transportation Master Plan (TMP) is reviewed and updated. All of these plans look at projected growth, and where this growth will happen. We believe that a widened 174 and 17 can handle the planned growth.
Q: How will residents access their homes if you put a median barrier?
A: Residents would have right-in right-out access into their driveways. We will also be looking at providing median breaks where residents can make U-turns. We are also looking at providing signalized intersections and/or roundabouts to facilitate the process.
Q: Beyond Trim Road a lot of us don’t have private services. Will you be looking at mitigation measures to protect owners of wells from salt related issues?
A: Impacts to wells is one of the criteria that we looked at as part of evaluation. We will document this issue so that it is addressed in design.
Q: How many people will die along this corridor until you make it safer?
A: Safety is one of our major considerations in this project. If the City of Ottawa had money to proceed with this project, they would proceed with it as quickly as possible. We understand that this has been a problematic route in the past.
Q: We are the only city in Ontario without an alternate route. I wonder what MPP Marie-France Lalonde has to say about this. We are a capital city and our infrastructure can’t keep up.
A: (This question answered by MPP Marie-France Lalonde). There is some money from the province to spend outside of the Greater Toronto Area and we will be looking at how this money will be allocated in the Ottawa-Orléans region.
Q: Will the widening of the highway increase noise?
A: We will be conducted noise modeling to determine the impacts of noise, and we will implement mitigation measures if they are required.

4.3 Public Open House in Rockland

A total of 51 people signed the registration sheet at the Public Open House meeting in Rockland. Before the formal presentation, attendees were encouraged to review the bilingual display material and discuss the studies with staff.

Marc Clermont, United Counties of Prescott and Russell, welcomed attendees and introduced the Study Team. A presentation was given jointly by Patrick Déoux, David Hopper, and Valerie McGirr. A question and answer period followed the presentation and discussions are summarized below in **Table 9**.

Table 9. Summary of Questions & Answers at the Public Open House in Rockland

Summary of Questions & Answers – POH in Rockland
SUMMARY OF QUESTIONS & ANSWERS – WEST OF TRIM ROAD
Q: It seems like all of the infrastructure updates have been going on in the west in Kanata. Why haven’t the communities in east Ottawa gotten anything? <i>A: Because Orléans has such a high transit usage, it will be getting the LRT. That is what our presentation tonight is about.</i>
Q: Why did you stop the light rail at Trim Road? <i>A: We don’t have the ridership beyond Trim road to extend the LRT. We are protecting for a bus rapid transit corridor east of Trim Road.</i>
Q: What is the shelf life of your environmental assessment (EA)? If we don’t start the project until after 2031, what is the point of doing the EA now? <i>A: Under the Municipal Class Environmental Assessment, the shelf life is 10 years. Beyond this, if no work on any element of the project has been started, we would have to update existing conditions to see whether anything has changed and whether the impacts of the project would be different. We would not have to start over from the beginning. Also, since the Transportation Master Plan (TMP) is updated every 5 years, it is possible this will be implemented sooner. We also may do some of the widening west of Trim Road in conjunction with the LRT to facilitate construction.</i>
Q: If the LRT is in the median, how will emergency vehicles access the train in the event of an emergency? What if a major disaster were to happen? <i>A: There will be a walkway created along the entirety of the tracks so that in the very unlikely event that the train was stopped between stations, passengers would be able to exit and use the walkway to access the nearest station. Ambulances would be directed to the station entrances. If there were a significant event, emergency services would be able to access the tracks from the highway.</i>
Q: Can you please add a westbound off-ramp onto the Rockcliffe Parkway? I have to make a major detour to get on the parkway. <i>A: Rockcliffe Parkway (now Sir George-Etienne Cartier Parkway) is federally owned under the control of the National Capital Commission.</i>
Q: What kind of parking will be available? <i>A: There are existing park and rides at Place D’Orléans and at Trim Road. Some expansion of these park and rides may occur, but no additional park and rides are proposed at this time.</i>
Q: How will it work for Clarence Rockland Transpo (CRT)? What kind of fares are we looking at? <i>A: While CRT operations will be worked out in the future, CRT buses will likely stop at the LRT and you would transfer. The transfer point would be first at Blair Station when the Confederation Line is completed in 2018, and then at Place D’Orléans Station when it is constructed. This would allow the CRT buses to make more trips in the same amount of time because they do not have to continue to downtown. No fares have been decided at this time.</i>
Q: Will the frequency of the trains east and west of Blair Station be the same? <i>A: The frequency of trains will be based on demand. During peak hours, there will likely be the same frequency. In the off-peak periods, the frequency will be based on ridership.</i>
SUMMARY OF QUESTIONS & ANSWERS – EAST OF TRIM ROAD
Q: Why can’t you continue from Alternative 2 and then go along Canaan Road to the north? <i>A: As part of our development of alternatives, we considered data from our traffic models, locations of growth and the direction of traffic flow.</i>
Q: Is there going to be space for 4 lanes in the other options? <i>A: At this time there is no expectation to build these other options to 4 lanes.</i>
Q: Are you going to be putting updated signalized intersections or roundabouts? <i>A: These types of details will be elaborated on in the next phase of the environmental assessment. At this time we have not determined whether there will be signalized intersections or roundabouts.</i>

Summary of Questions & Answers – POH in Rockland
Q: Why are you only proposing two lanes for the options to the south? <i>A: Our transportation model looked at what kind of transportation capacity we would need in this area in the future. Our model shows that we need a total of 4 lanes of capacity to move the predominant flow of traffic. Since Highway 174 already has 2 lanes, these southern routes only need to be 2 lanes to give us the capacity we need.</i>
Q: It would be useful if you showed where the pink alternative (alternatives 3A and 3B) would connect to the 417 west of Frank Kenny. <i>A: Upgrades to other roads would be required if these were the selected alternatives, but they are not shown here. For example, a widening of some of Trim Road to 6 lanes would also be required.</i>
Q: If you widen County Road 17, you will focus even more traffic along this corridor. If you send them along back roads, it will disperse the traffic to other routes. Every morning I head west and use Highway 174. I get stuck at Jeanne D’Arc, I don’t get stuck here! <i>A: One of our other criteria is downstream effects. It is true that we are running out of places to widen roads to the west of the split and downtown. That is why we are trying to encourage transit to alleviate some of this congestion.</i>
Q: If you proceed with the widening, you will be changing traffic patterns. We would have to leave 5 minutes later. Have you even taken this into account? <i>A: We understand that widening the highway will have implications for downstream effects. We have taken this into account in our analysis.</i>
Q: What will be happening with the Cardinal Creek development, and other developments in this area? <i>A: Our transportation models did take into account projected future growth and where these areas of growth will occur. Our analysis has taken into account the additional volume that would occur as a result of the Cardinal Creek development.</i>
Q: Why haven’t you looked at a road east off of the 417? <i>A: The transportation master plan (TMP) looks at roads at a more regional level, and this type of road was examined. It will likely be considered in the future, but this type of road would not help to solve the current problem. Our models look at growth and where transportation infrastructure is needed, and they point in the direction of the alternatives you see here.</i>
Q: With respect to growth, have you taken into account the services that will be required in the area between Orléans and Rockland? There will be great demand. <i>A: We understand that there will be growth in this area and our solution looks to provide the best way forward to accommodate this growth.</i>
Q: Why can’t we do a three lane highway with reversible lanes? <i>A: We did a review and assessment of whether reversible lanes would be suitable in this context. Particular considerations such as safety and costs indicate that a reversible lane is not appropriate along Highway 174 and County Road 17. The costs of putting overhead signage that indicate the direction of traffic in the centre lane would be very expensive. Furthermore, for safety reasons, we need to put a barrier between lanes of different directions. Using reversible lanes over long distances and at highway speeds is not a feasible option.</i>
Q: If there is an accident, the road gets blocked. Does your criteria even look at safety? <i>A: Yes, safety is included under our transportation criteria. We address safety and traffic flow. Furthermore, one of the reasons we are recommending the widening of Highway 174 and County Road 17 as our preferred alternative is because traffic would still be able to manoeuvre even if one lane of traffic is blocked.</i>
Q: There is no alternative road in the case of an emergency. If you build one of the alternatives to the south, you will have your alternative route. <i>A: Network considerations such as redundancy were included in the evaluation.</i>
Q: Who participated in making alternative 1 the best? We were not consulted. <i>A: What we are recommending is a preliminary preferred alternative. We are here to listen to you and take into account your comments. We determined that alternative 1 is the best solution through our evaluation. We performed a sensitivity analysis to test the evaluation results. In 11 out of 17 of the sensitivity tests, alternative 1 ranked first.</i>
Q: It seems like Alternative 1 was chosen by administration and the public seems to have no impact on the decision. At

Summary of Questions & Answers – POH in Rockland
the last meeting, 100% of the people said that they did not want a widening of the 174 and 17. <i>A: Your comments are important to us. Please submit a formal comment by email or on your comment sheet and we will look at your concerns.</i>
Q: Based on your concordance method, there doesn’t seem to be a very big difference between the numbers of Alternative 1 and the numbers of Alternative 3A. Did you even look at the economic impacts on residents? <i>A: Yes, potential impacts on landowners were considered as one of the criteria as part of our social evaluation.</i>
Q: Which one of these alternatives is the most expensive? Why would you choose the most expensive option? <i>A: The widening of the 174 and 17 was the most expensive option. Costs were included as one criteria in our evaluation of the alternatives.</i>
Q: For the 3A alternative, have you considered at all what would occur west of Trim Road? It is a parking lot on Innes Road right now. You need to look at what occurs west of there in the peak hours. <i>A: Yes, the downstream effects have been taken into account. We use our forecasting models to determine how the future conditions would be and what would need to change to make these roads efficient.</i>
Q: Do you take into account where people are going for employment? <i>A: Yes, our transportation forecasting model looks at where people live and work.</i>
Q: Is there any chance at all to change the preferred alternative? <i>A: The most important thing you can do this evening is to fill out a comment sheet and let us know what your concerns are, in particular with the evaluation process. We look at this feedback and it helps us shape our solution.</i>
Q: When you say that more details are provided in your report... Can we get these reports? How do we get this information? <i>A: Some of these reports are available at the resource table tonight for your review and if you submit us a written request we can make this information available to you.</i>
Q: Can you please provide a microphone for the public so they can be heard when they ask questions? <i>A: Yes, absolutely. We will take this into account for future public open houses.</i>
Q: What is the time frame for widening? <i>A: Right now the widening of the 174 and 17 is not in Ottawa’s affordable network, so it would occur after 2031. However, the Transportation Master Plan (TMP) where priorities are established is updated every 5 years, with another update in 2018. Also if a source of funding becomes available for this project, the timeline may change.</i>
Q: I would like to see a detailed breakdown of costs of the alternatives. Are you telling me that you put only 14% of the weight on costs? What makes up the capital costs? <i>A: At this time we do a preliminary comparison of capital costs. We start with the major components, such as asphalt, and concrete and estimate costs around that. The more detailed our design becomes, the more precise our cost estimates become.</i>
Q: Are there plans to stop issuing permits to build houses along the river? <i>A: At present road authorities have designated this corridor as controlled access where there is no further creation of access. The City and the Counties are not permitting severances of property each with a new access to OR 174-CR 17.</i>
Q: Are you going to buy back the houses of the people that you let build houses along the river? <i>A: If we need property for the widening, only then would we be looking at purchasing property. There are many places along the corridor where we have sufficient right-of-way (ROW). In Rockland along the 17 we have a ROW of 50 metres.</i>
Q: You are telling me that we have sufficient right of way along the 17? Your road would be in my kitchen if what you say is true. You don’t have that kind of space. <i>A: I do not know where exactly you live, but in many portions we have a sufficient right-of-way. That is not to say we have sufficient right-of-way everywhere, but if there are portions where property is required then we will have a discussion with those residents who will be impacted.</i>
Q: What about noise impacts? <i>A: If there are noise impacts, we will look to install mitigation measures such as noise barriers.</i>
Q: There is a bus stop on the 174 near Cumberland that needs to be removed. There are numerous accidents and when

Summary of Questions & Answers – POH in Rockland
the bus stops it impedes traffic. There was also an issue with snowplows and passengers waiting on the side of the road to take the bus. This is a major safety issue that needs to be addressed. <i>A: Thank you. We will look into this.</i>
Q: The bottleneck along the 174 is not in Rockland or Cumberland. It is at Jeanne D’Arc. Opening routes like they did along the 417 at Hunt Club was great. It provided another way into the city. Baseline brings me into town faster than taking the 174. <i>A: We did look at congestion as part of our existing conditions study. With the Innes-Baseline route, we know that there will be downstream effects on other roads. Congestion is a major issue and so we are building transit like the LRT to encourage sustainable travel choices. We are protecting for a HOV lane along the 174 so that this can be changed in the future.</i>
Q: The real issue is how you treat the residents living along the 174 and the 17. It takes me a lot longer to get into town now than it did 10 years ago. We haven’t been consulted. <i>A: The purpose of the environmental assessment process is to create a dialogue with those that are potentially affected.</i>
Q: When you increased the bus service at the park and rides, the ridership increased substantially. <i>A: Yes. We will be looking at improving the existing park and rides in order to further increase ridership.</i>
Q: Would you ever create a service road? <i>A: We are looking at ways to consolidate driveways to the extent possible. This will depend on the ROW that we have available. We know that the ROW that we have is narrow in places. If we were able to consolidate some of these driveways, it would reduce the number of access points to the highway.</i>
Q: What about the land along the railway corridor? Does Rockland own it? Could we use it in some way? <i>A: This ROW is presently on lease to the Counties for their pedestrian pathway. We tried to purchase it from VIA Rail, but we weren’t successful. Our policy in Prescott-Russell now is to pave shoulders to facilitate cycling.</i>
Q: What exactly is stopping us from doing Alternative 2? Isn’t this right where we want to go? <i>A: There are many factors that we have taken into account. Based on our evaluation, Alternative 2 has more impacts to the natural and social environment, in particular.</i>
Q: If you look at other 4 lane highways, they didn’t build them right in the middle of the community. <i>A: Many of the highways in the province are under provincial jurisdiction, but OR174-CR 17 are under municipal jurisdiction and are considered arterial roads.</i>
Q: The City of Ottawa isn’t actually interested in looking at an alternative to widening the 174 and the 17. They want everyone to get to the LRT. This alternative is the most expensive so why are we going with this? <i>A: We take into account many factors in our evaluation, cost are just one of many factors.</i>
Q: It seems like no matter which alternative you choose, you are going to have issues. What is the point of doing any of these if you are only going to have even more issues later!
Q: Come and spend time out here in the East. You will see how we have been left behind. Every morning, every night, it is the same thing. We live with these issues every day.
Q: Why not just extend the LRT further east? <i>A: In the downtown centre, there is high density and many users want transit. While the upfront cost is important, the cost to operate it is even higher. You need about 8-10 thousand people per hour that want to take transit to make LRT feasible. We need to build ridership before we talk about trains. Rail is expensive to build and operate. Here in Rockland, we should focus on bus transit priority. That is where there is benefit to having 2 lanes in each direction.</i>
Q: What about the areas where future growth will occur? <i>A: A study similar to the Greater Toronto Area’s “Places to Grow” is being considered for Eastern Ontario, which would look at where development should occur. There is a lot of good agricultural land in this area, and if we build a road, there would be pressure to develop the surrounding areas and we would lose this agricultural land.</i>
Q: Have you ever farmed along here? This is not good agricultural land.
Q: Would you ever bring a train back out to Rockland? <i>A: There are many studies that look at whether Ottawa needs a regional rail service. At this time the LRT will not be brought to</i>

Summary of Questions & Answers – POH in Rockland
<i>Rockland because they don't have the ridership. That is not to say that this demand will not occur eventually, but in the foreseeable future, the LRT will not be brought to Rockland. We are instead looking at bus transit priority in Rockland.</i>

4.4 Summary of Comments Submitted

4.4.1 Summary of Comments Submitted at the POHs

Copies of the submissions, excluding personal information, are provided in **Appendix C** of this report. The eight comments submitted at the Cumberland POH on February 3, 2015 are summarized in **Table 10**.

Table 10. Summary of Comments Submitted at Public Open House in Cumberland

Description of Comments	Comment Sheet #	Number of Comments by Topic
Opposed to widening Ottawa Road 174	3,5	2
Perception is that decision is already made to widen Ottawa Road 174	8,6	2
Supports widening Ottawa Road 174	1	1
Environmental issues – agricultural, City of Ottawa	1,3	2
Transit – LRT station placement, number of stations	2,7	2
Transit – Local routes and transfers to LRT	2	1
Parking – park and ride facilities	2	1
Open House – general comments (venue, presentation ,etc.)	3,4,6	3
Cycling/ Pedestrian issues	5	1
Supports Light Rail Transit (LRT) proposed alignment	7	1
Transit – Support for extending to Trim Road and/or beyond	7	1
Future capacity/ Growth projections	8	1
Property Impacts and expropriation concerns	5	1

The fifteen comments submitted at the Orléans POH on February 6, 2013 are summarized in **Table 11**.

Table 11. Summary of Comments Submitted at Public Open House in Orléans

Description of Comments	Comment Sheet #	Number of Comments by Topic
Supports widening Ottawa Road 174	9,12,13,15,21,22	6
Noise & vibration, property access, wells & septic, archaeology/heritage are issues	16	1
Transit – Local routes and transfers to LRT	10	1
Parking – existing capacity at park and ride facilities is inadequate	11,21	2
Cycling/ Pedestrian issues	14,21,22,23	4
Supports Light Rail Transit (LRT) proposed alignment	9,12,13,14,18, 19,21,22,23	9
Transit – Support for extending to Trim Road and/or beyond	10,14,15,16,18	5
Accessibility – priority seating	10	1
Support for alternative - LRT South side of Highway 174	17	1
Traffic impact concerns from volume and construction on roads already at capacity off of Highway 174.	17,19	2

Description of Comments	Comment Sheet #	Number of Comments by Topic
LRT maintenance	19,20,22	3

One comment was submitted at the Rockland POH meeting on February 5, 2015 and is summarized in **Table 12**.

Table 12. Summary of Comment Submitted at Public Open House in Rockland

Description of Comments	Comment Sheet #	Number of Comments by Topic
Supports widening Ottawa Road 174	24	1

4.4.2 Comments Submitted after the POHs

An additional 42 comments were received by mail or email following the POHs. A summary of these comments is provided in **Table 13** and copies of the submissions, excluding personal information, are provided in **Appendix C** of this report.

Table 13. Summary of Comments Submitted after the Public Open Houses

Description of Comments	Number of Comments Received by Topic
Open House – general comments (venue, presentation ,etc.)	10
Costs should be identified	7
Noise & vibration, property access, wells & septic tanks, archaeology/heritage are issues	6
Environmental issues – agricultural, City of Ottawa	6
Support for other alternatives	7
Comments about evaluation criteria sensitivity/ weightings/ Performance Metrics	6
Property Impacts and expropriation concerns	5
Perception is that decision is already made to widen Ottawa Road 174	4
Supports widening Ottawa Road 174	3
Build new road south of Ottawa Road 174 / ring road where development is occurring	3
Opposed to widening Ottawa Road 174/route	3
Supports Light Rail Transit (LRT) proposed alignment	3
Transit – Local routes and transfers to LRT	3
Transit – Support for extending to Trim Road and/or beyond	3
Future capacity/ Growth projections	3
Property impacts are an issue	2
Traffic bottleneck points identified, lack of existing capacity	2
Transit – LRT station placement, number of stations	2
Existing bus system is adequate	2
Parking – existing capacity at park and ride facilities is inadequate	2
Cycling/ Pedestrian issues	2
Safety/ Traffic Accidents resulting from the LRT in the median	2
Widening would add more traffic / congestion onto Highway 174	1
Accessibility – priority seating, public washrooms	1
Traffic impact concerns from volume and construction on roads already at capacity off of Highway 174.	1
Speed Enforcement/ Possible designation as a parkway	1

Description of Comments	Number of Comments Received by Topic
Transit – Bridge infrastructure replacement needed for stations	1
Emergency turnarounds needed on Highway 174	1

5 CONCLUSION

The overall consensus was that current traffic conditions/congestion on Ottawa Road 174 / County Road 17 require a solution.

For the area from Highway 417 to Trim Road, most participants were happy to have LRT extended to Orléans and supported its construction all the way to Trim Road. Residents along the highway asked for noise walls to be built as soon as possible in areas not currently served by noise walls. Existing and future vibration levels were also an issue in some areas.

For the area east of Trim Road, some participants supported the results of the evaluation that identified the widening of OR 174/CR 17 as preferred while others felt that another alternative would be better. Many residents voiced concern about impacts to their properties from increased noise and vibration levels due to increased traffic volume and widening of the highway. Cost of the project was also a concern for many. Residents with driveways along OR 174 and CR 17 were concerned about the out-of-way travel required due to the change to their access to right in-right out only.

Residents in Cumberland Village opposed the widening of OR 174 indicating that the community would be severely impacted. They supported construction of a new route to the south.

Comments from residents were carried forward to the development of the functional design.

APPENDIX A: CONSULTATION NOTICES

Prolongement vers l'est de la Ligne de la Confédération du système de train léger rapide (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell
Études d'impact environnemental
Séances portes ouvertes

Mardi 3 février 2015
Centre communautaire commémoratif R. J. Kennedy
Salles A et B
1115, chemin Dunning, Cumberland
18 h à 21 h, présentation à 19 h

Mercredi 4 février 2015
Complexe récréatif Bob MacQuarrie
1490, promenade Youville, Orléans
18 h à 21 h, présentation à 19 h

Jeudi 5 février 2015
Salle Guy Faubert
954, rue Giroux, Rockland
18 h à 21 h, présentation à 19 h

Prolongement vers l'est de la Ligne de la Confédération du TLR

La Ville d'Ottawa entreprend une étude d'évaluation environnementale (ÉE) portant sur le prolongement du TLR de la station Blair jusqu'au chemin Trim, le long du couloir de la route 174 d'Ottawa (OR174), conformément au processus d'évaluation des projets de transport en commun. Cette ligne du TLR assurera un service rapide et fiable aux collectivités de Beacon Hill, Blackburn Hamlet et Orléans, ainsi qu'à la localité rurale de Cumberland. On propose un total de huit stations entre la station Blair et le parc-o-bus du chemin Trim.

Élargissement de la route 174/du chemin de comté 17

Les Comtés unis de Prescott et Russell, en partenariat avec la ville d'Ottawa, entreprennent une étude d'ÉE de catégorie A portant sur le couloir de la route 174 d'Ottawa (OR174) et du chemin de comté 17 (CR17) de Prescott-Russell à partir de l'autoroute 417 jusqu'au chemin de comté 8 (chemin Landry). Cette étude est réalisée conformément aux exigences de l'annexe « C » du document sur les évaluations environnementales municipales de portée générale.

Des consultations ont eu lieu en 2013 afin de présenter la nécessité de solutions de rechange aux problèmes se présentant dans le couloir de la route 174 d'Ottawa et de la route de comté 17.

Les séances portes ouvertes permettront aux résidents d'obtenir de l'information et de faire part de leurs commentaires sur ce qui suit :

- une mise à jour générale du projet, y compris l'intégration du TLR et du couloir de la route 174 de la station Blair jusqu'au chemin Trim;
- l'élaboration et l'évaluation des solutions de rechange concernant l'intégration du tracé du TLR et de l'élargissement de la route 174, y compris le plan privilégié à l'ouest du chemin Trim;
- l'élaboration et l'évaluation de solutions de rechange concernant les couloir routiers entre le chemin Trim et le chemin Landry, y compris le couloir routier privilégié.

Il s'agit de la deuxième série de consultations publiques relativement à l'ÉE concernant l'élargissement de la route 174 et du chemin de comté 17, et la première série de consultations publiques au sujet de l'ÉE portant sur le prolongement du TLR vers l'est, afin d'étudier et de discuter du projet avec l'équipe chargée de l'étude et de recueillir des commentaires.

Le prolongement du TLR vers l'est est l'une de trois études d'évaluation environnementale en cours dans le cadre de la 2e étape, qui porte sur le plan de la Ville d'apporter les avantages du transport par train léger aux résidents des secteurs se trouvant plus à l'est, à l'ouest et au sud, qui ajoutera 19 nouvelles stations et 30 kilomètres de voies à notre réseau de TLR.

L'accessibilité a de l'importance pour la ville d'Ottawa. Si vous avez besoin de dispositions spéciales, veuillez composer le 3-1-1 ou envoyer un courriel à la responsable de projet indiquée ci-dessous avant la tenue de la séance.

Si vous n'êtes pas en mesure d'assister à la séance portes ouvertes, ou si vous désirez des informations additionnelles, veuillez consulter le site Web de l'étude, à <http://ottawa.ca/fr/hotel-de-ville/consultations-publiques/le-couloir-est-du-train-leger-sur-rail> ou <http://www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure> ou <http://ottawa.ca/fr/hotel-de-ville/consultations-publiques/etude-de-la-route-174-dottawa-et-la-route-de-comte-17> ou faire parvenir vos observations et questions à la personne dont le nom paraît ci-dessous. Le matériel de présentation de la séance portes ouvertes sera affiché dans le site Web de l'étude. Pour obtenir de plus amples informations en cette matière ou pour faire ajouter votre nom à notre liste d'envoi, rendez-vous au site Web ou communiquez avec :

Prolongement vers l'est du TLR et élargissement de la route 174 et du chemin de comté 17

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca



Élargissement de la route 174 et du chemin de comté 17

Marc R. Clermont, ing..
 Directeur des Travaux publics
 Comtés unis de Prescott et Russell
 613-675-4661, poste 3100
 Courriel : MClermont@prescott-russell.on.ca

Le financement de l'étude d'ÉE concernant l'élargissement de l'autoroute 174 / du chemin de comté 17 est assuré par le gouvernement de l'Ontario.
N° Pub 2015-01-7003-25620-S

**Confederation Line East Light Rail Transit (LRT) Extension
Ottawa Road 174 / Prescott-Russell County Road 17 Widening
Environmental Assessment Studies
Open Houses**

Tuesday, February 3, 2015
R. J. Kennedy Memorial Community Center
Hall A & B
1115 Dunning Road, Cumberland
6 to 9 p.m., presentation at 7 p.m.

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans
6 to 9 p.m., presentation at 7 p.m.

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland
6 to 9 p.m., presentation at 7 p.m.

Confederation Line East LRT Extension

The City of Ottawa is undertaking an Environmental Assessment (EA) Study for the extension of LRT from Blair Station to Trim Road along the Ottawa Road 174 (OR174) corridor in accordance with the Transit Projects Assessment Process. This LRT line will provide fast, reliable service to the communities of Beacon Hill, Blackburn Hamlet, Orleans, and rural Cumberland. A total of eight stations are proposed between Blair Station and the Trim Park and Ride.

Highway 174/County Road 17 Widening

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class EA study for the OR174 and Prescott-Russell County Road 17 (CR17) corridor from Highway 417 to County Road 8 (Landry Road). This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class EA process.

Consultation was held in 2013 to introduce the need for alternative solutions to address problems on the OR174 and CR17 corridor.

The Open Houses will provide residents with information and feedback opportunities on the following:

- A general project update, including the integration of LRT in the OR174 corridor from Blair Station to Trim Road
- The development and evaluation of the integrated alignment alternatives for the LRT and OR174 widening including the preferred plan West of Trim Road
- The development and evaluation of alternative road corridors from Trim Road to Landry Road, including the preferred road corridor

This is the second round of public consultation the OR174/CR17 widening EA study and the first round of public consultation for the East LRT Extension EA Study to review and discuss the project with the study team and solicit feedback.

The East LRT extension is one of three environmental assessment studies underway as part of Stage 2, the City's plan to extend the benefits of rail to residents further east, west and south that will add 19 new stations and 30 kilometres to our LRT network.

Accessibility is an important consideration for the City of Ottawa. If you require special accommodation, please call 3-1-1 or e-mail the project lead below before the event.

If you are not available to attend the Open House or would like additional information, please visit the study web site at ottawa.ca/easternlr or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study or direct your comments and questions to the contact person below. The Open House presentation material will be posted to the study web site. For further information on this project, or to be added to our mailing list, visit the web site or contact:

East LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

**OR174/CR17 Widening**

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Funding for the Highway 174/County Road 17 Widening EA Study is being provided by the Government of Ontario.
Ad # 2015-01-7003-25620-S

http://ottawa.ca/en/news/city-seeks-public-input-lrt-east-preferred-alignment

City seeks public input on LRT East Preferred Alignment
January 26, 2015

News Release

Ottawa - The City of Ottawa is looking for public feedback on its preferred alignment for the Confederation Line East Light Rail Transit (LRT) extension at a series of open houses in early February. For the most part, the preferred alignment for the rail line is in the median that runs in the middle of Highway 174 and is being co-ordinated with the eventual widening of the highway.

“This is a great project that will improve the quality of life for people living in the east part of the city,” Mayor Jim Watson said. “This eastern rail extension is an important part of our Stage 2 package of future rail projects that will help us build a truly City-wide transit system to serve generations of residents.”

The City of Ottawa is co-ordinating this environmental assessment study for the extension of LRT from Blair Station to Trim Road with the EA study on the widening of the Highway 174 and Highway 17.

At the open houses, residents will have the opportunity to learn more about and provide their feedback on the integration of LRT in the highway corridor from Blair Station to Trim Road and the plan for highway widening west of Trim Road.

“Transit use in our City is highest in the east, and this project will provide a seamless, high-quality connection to downtown and the rest of the City,” said Councillor Keith Egli, Chair of the City’s Transportation Committee. “It’s important that people let us know what they think of our plans.”

The project will add 10 kilometres of rail from Blair Station to Place d’Orléans and four new stations to the City’s overall transit network. A potential extension to Trim Road could add up to four more stations and 3.5 kilometres of rail.

“It’s an exciting time for east-end residents,” said Chair of the Transit Commission and Cumberland Councillor Stephen Blais. “Through this project, we’re getting better transit and road connections with enormous economic development potential along the highway corridor. These are big improvements that will make a real difference in people’s lives.”

The Confederation Line East LRT extension is one of three environmental assessment studies underway as part of Stage 2, the City’s plan to extend the benefits of rail to residents further east, west and south. Stage 2 will build on existing rail projects by adding 19 new stations and 30 kilometres to our LRT network.

More information on the East LRT and Highway 174 widening project can be found at ottawa.ca.

The open house dates are as follows:

Tuesday, February 3
R.J Kennedy Memorial Community Centre
Halls A & B
1115 Dunning Road Cumberland
6 to 9 p.m., presentation at 7 p.m.

Wednesday, February 4
Bob MacQuarrie Orléans Recreation Complex

1490 Youville Drive, Orléans
6 to 9 p.m., presentation at 7 p.m.

Thursday, February 5
Guy Faubert Hall
954 Giroux Street, Rockland
6 to 9 p.m., presentation at 7 p.m.

Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening Environmental Assessment Studies

Overview

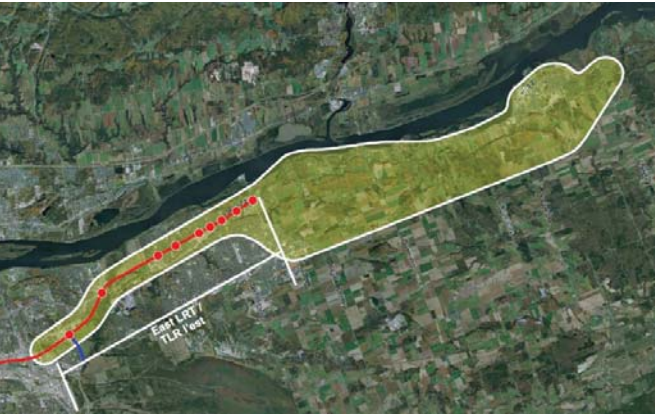
This Information Bulletin has been prepared to provide an understanding of the Confederation Line East Light Rail Transit Extension and the Highway 174 / Prescott-Russell County Road 17 widening environmental assessment studies. It includes information on the background of these projects, how their studies are being integrated, the study process, an overview of the existing conditions, the alternatives for each of these projects, the evaluation methodology used to select a preliminary preferred alternative, and the next steps in the environmental assessment process.

Confederation Line East Light Rail Transit Extension Project Overview

The City of Ottawa is undertaking an Environmental Assessment (EA) Study for the extension of Light Rail Transit (LRT) from Blair Station to Trim Road along the Highway 174 corridor in accordance with the Transit Projects Assessment Process. The East LRT (ELRT) extension is one of three EA studies underway as part of Stage 2, the City's plan to extend the benefits of rail to residents further east, west and south that will add 19 new stations and 35 kilometres to the LRT network. The ELRT line will provide fast, reliable service to the communities of Beacon Hill, Blackburn Hamlet, Orléans, and rural Cumberland. A total of eight stations are proposed between Blair Station and the Trim Park and Ride. These station locations have been designed to connect to existing and future transit services and park and rides in the area. The study area for this project extends from Blair Road in the west and continues along the Highway 174 corridor to Trim Road in the east.

Highway 174 and County Road 17 Widening Project Overview

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class EA study for Highway 174 and Prescott-Russell County Road 17 (CR17) corridor from Highway 417 to County Road 8 (Landry Road). This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class EA process. The study area for the Highway 174/CR17 project extends from the split with Highway 417 in the west and continues along the Highway 174 corridor until Trim Road, where the study area expands to include several alternative routes. The study area for the ELRT overlaps with the portion of Highway 174 that is located to the west of Trim Road.



Study Area for the ELRT and Highway 174/CR17

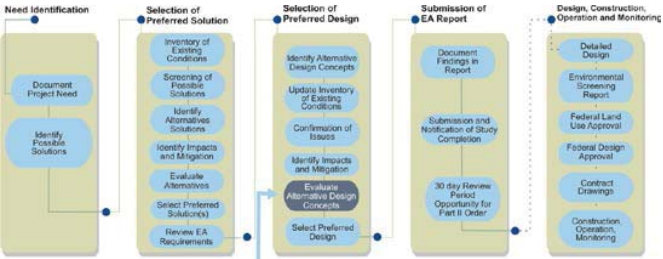
A Coordinated Approach to Environmental Assessment

In order to reach an optimal solution for these two projects whose study areas overlap, a coordinated approach to these EAs is being undertaken. Co-ordinated approach to both EA studies is needed to:

- Maximize the use of the existing road corridor
- Minimize impacts on the environment
- Minimize construction cost
- Build on existing study information
- Integrate stakeholder consultation activities
- Streamline EA process
- Share information, technical design work, and stakeholder consultation

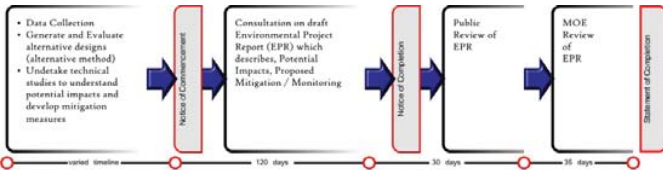
EA Study Process

In accordance with the Municipal Class EA process, the widening of a road or construction of a new road/paved facility over \$2.3 M is a Schedule "C" project. Schedule "C" projects require the preparation of an Environmental Study Report which will be made available for public review.



Municipal Class EA Process for a Schedule C Project

In accordance with the Transit Projects Assessment Process (TPAP), which is the EA process being followed for the ELRT, we are presently in the pre-planning and consultation phase. This phase involves generating and evaluating alternatives. This open house presents the selection of preferred alternative alignment for the ELRT within the pre-planning and consultation phase.



Transit Projects Assessment Process

Existing Conditions

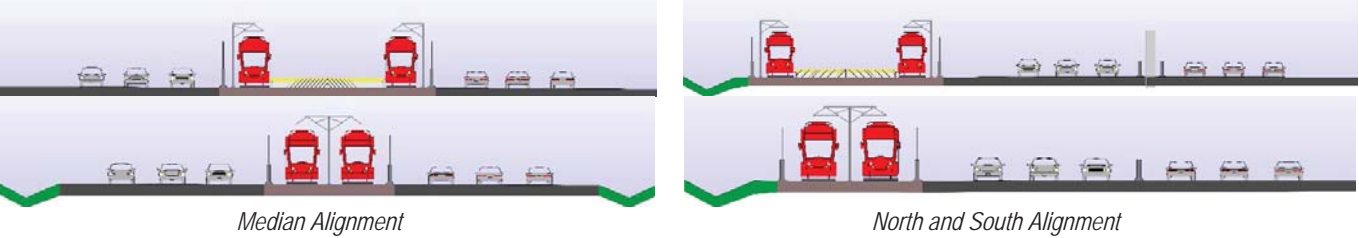
Baseline environmental conditions were documented to provide a foundation for assessing how the environment might be affected by both of these projects. The information has been used to: develop reasonable alternatives, identify impacts, and evaluate the alternatives.

APPENDIX B: Information Bulletins

Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening
Environmental Assessment Studies

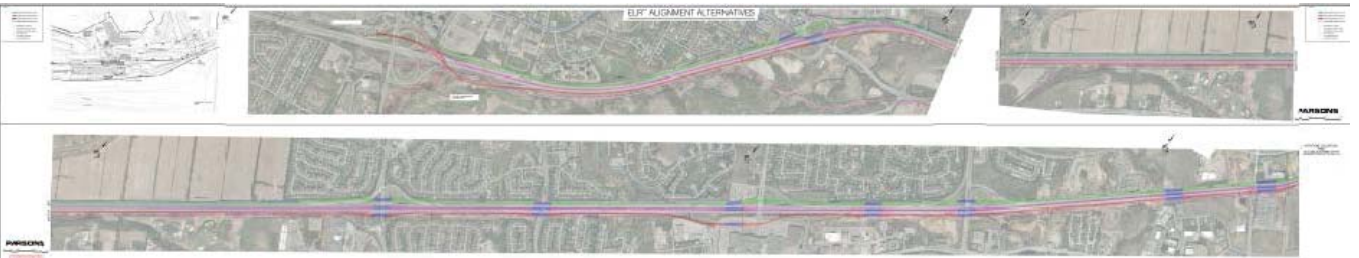
Alternatives – West of Trim Road

To the west of Trim Road, the roadway cross-section includes: 3 lanes in each direction, protection for incorporation of high-occupancy vehicle (HOV) lanes, paved shoulder, concrete median barrier, and a continuous concrete barrier and chain link fence between Highway 174 and the ELRT. The LRT cross-section includes: 2 tracks, space for overhead catenary poles, space for walkways, and platforms at stations. Stations will be designed to be convenient, comfortable, and accessible for all users, including pedestrians and cyclists.



The following 3 alternative alignments for the LRT from Blair Station to Trim Road were developed:

- **Alternative 1:** LRT tracks and stations to the north of Highway 174.
- **Alternative 2:** LRT tracks and stations to the south of Highway 174.
- **Alternative 3:** LRT tracks and stations located in the median between the lanes of Highway 174.



LRT to the north of Highway 174 in green, to the south of Highway 174 in red, and within the median of Highway 174 in pink. Station locations are in dark blue.

Alternatives – East of Trim Road

East of Trim Road, all alternatives will have paved shoulders, and will have pedestrian and cyclist accommodations. Alternative 1 will have a median separating the directions of traffic, while the other alternatives will have no dividing median. The median barrier type will be determined during the detailed design stage of the EA process.

The following 4 alternatives were developed:

- **Alternative 1:** Widen existing Highway 174/CR 17 to 4 lanes.
- **Alternative 2:** Build a new route south of Wilhaven Drive to connect to Baseline Road.
- **Alternative 3A:** Improve existing Innes-Baseline Road with a new connection from Dunning to Canaan Road.
- **Alternative 3B:** The same as Alternative 3A except connecting to Brian Coburn Drive at Frank Kenny Road.

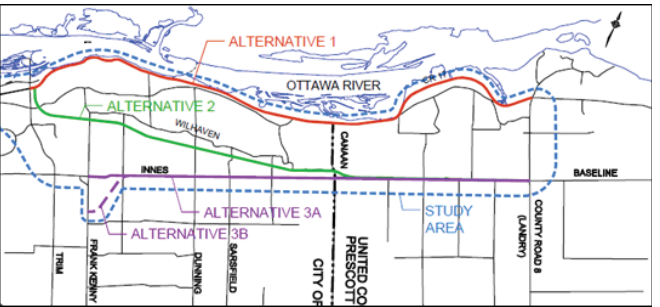
Alternative 1, would be a four lane divided rural roadway.



The second cross-section represents alternatives 2, 3A, and 3B, which involve a two lane undivided rural roadway.



The location of each of the alternatives can be found in the image below.



Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening
Environmental Assessment Studies

Evaluation Methodology

The methodology chosen to evaluate alternative solutions addresses the following objectives:

- **Use of Criteria** as the basis for the evaluation of the alternatives;
- **Weighting or Ranking of Criteria** to reflect the of importance of the criteria/indicators;
- **Sensitivity Analysis** to test the results of the evaluation and to verify the influential factors in the results of the evaluation.

Evaluation of Alternatives – West of Trim Road

Twenty-two evaluation indicators and 31 measurements were identified and used to evaluate the alternatives West of Trim Road.

Social

- Archaeological resources
- Noise
- Property Impacts
- Compatibility with Adjacent Communities
- Views and Vistas
- Smart Growth

Infrastructure

- Major Municipal Infrastructure
- Utilities
- Major Structure

Cost

- Capital costs
- Operating costs

Transportation

- Safety
- Transit Network Function
- Transit Ridership
- Transit Stations
- Pedestrian and Cyclists
- Road Network Functions

Biological

- Natural Heritage features
- Aquatic
- Wildlife
- Species at Risk

Physical

- Slopes and ravines

Evaluation of Alternatives – East of Trim Road

A total of 28 evaluation indicators and 68 measurements were identified and used to evaluate the alternatives East of Trim Road.

Social Environment

- Impact on existing residences / communities
- Impact on planned development
- Impact on recreational features
- Impact on archaeological resources
- Cultural resources
- First Nations
- Impact on agricultural resources
- Noise
- Air Quality
- Views and vistas
- Compatibility with municipal planning goals and objectives

Transportation

- Traffic Performance (autos)
- Road Network Function
- Safety
- Transit Network Function
- Pedestrian Network
- Cycling Network
- Ferry

Biological

- Aquatic
- Natural Heritage features
- Wildlife
- Species at Risk

Infrastructure

- Major infrastructure impacts

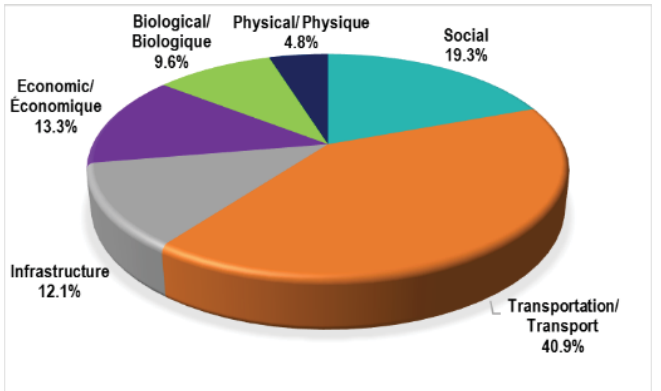
Economic Environment

- Businesses
- Capital costs

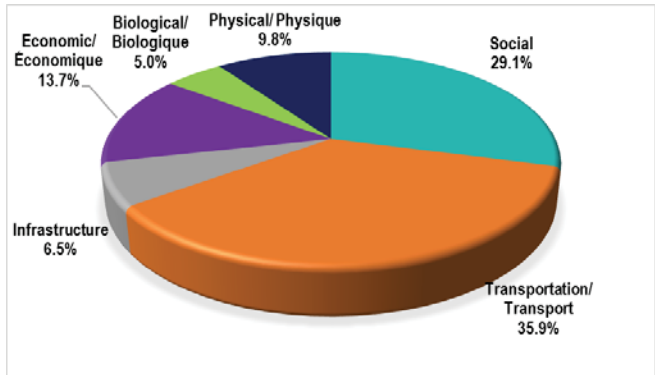
Physical

- Geology
- Groundwater
- Potentially Contaminated Sites

Weights were selected by the City of Ottawa, United Counties of Prescott-Russell, and the consultant study team, and were assigned to each of the indicators and measurements to reflect their relative value. An overall weight for each criteria group is presented below.



When each of the alternatives were compared with the others, the Median Alignment was found to be the preferred alignment. The Median Alternative remained the preferred alignment in the sensitivity tests performed.



Alternative 1 was found to be the preferred alignment. When subjected to sensitivity testing, Alternative 1 remained the preliminary preferred alternative.

Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening
Environmental Assessment Studies

Preliminary Preferred Alternative – West of Trim Road

An overall performance review was undertaken to determine if a hybrid alignment that takes sections from more than one alternative would yield an even better solution. A hybrid solution was developed that utilizes the north corridor to the Greens Creek area then transitions into the median. This hybrid takes advantage of:

- Proximity to developed areas that can encourage transit-oriented development and ridership from Blair Road to Montreal Road;
- Better connections to multi-use pathways;
- Reduces the number of structures required;
- Maintains the existing Transitway overpass both during and after construction;
- Reduces overall capital costs;
- Results in fewer construction staging impacts on both existing transit and vehicular traffic.

After leaving Blair Station, the LRT alignment continues along the north side of Highway 174 until it crosses into the median near Green's Creek. The LRT continues in the median until the alignment ends at Trim Road. The image below shows the crossover near Green's Creek, with the alignment of the LRT in pink, LRT stations in green and the existing Highway 174 in light blue. Locations where the highway will be widened are dark blue.



Preliminary Preferred Alternative – East of Trim Road

Alternative 1, the widening of Highway 174/CR17 was selected based on the application of the concordance method. Alternative 1 is preferred because it:

- Best addresses transportation needs;
- Requires less additional right-of-way and impacts fewer buildings;
- Supports sustainability policies;
- Supports transit and connects to LRT corridor;
- Will add pedestrian and cycling facilities where they are most needed;
- Supports re-design and improvement of road environment through communities;
- Allows for consideration of high-occupancy vehicle lane designation; and
- Does not add infrastructure through undeveloped natural areas.

The yellow line in the image below represents the location of Highway 174 and CR17 where the widening will take place.



Next Steps

The next steps in the study process are to:

- Incorporate comments and confirm preferred alternatives;
- Develop design details including: Station locations and design, interchange configurations, intersection layouts, Cumberland Village cross sections and pedestrian and cycling connections;
- Determine mitigation measures;
- Hold next public consultation late spring 2015;
- Presentation to Councils/Committees; and
- Environmental Study Reports.

Questions or Comments	
If you wish to provide us with more detailed comments, send written submissions by February 13, 2015 to:	
Angela Taylor, P.Eng. Senior Project Engineer City of Ottawa 110 Laurier Ave W, Ottawa, ON K2P 2L7 ☎: 613-580-2424 ext. 15210 E-mail: angela.taylor@ottawa.ca	Marc R. Clermont, P.Eng. Director of Public Works United Counties of Prescott-Russell 59 Court St, PO Box 304 L'Orignal, ON K0B 1K0 ☎: 613-675-4661 ext. 3100 E-mail: mcclermont@prescott-russell.on.ca



Études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la
Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17

Aperçu

Le présent bulletin d'information a été préparé pour faire connaître les études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17 de Prescott-Russell. Il comprend de l'information sur le contexte de ces projets, la manière dont les études sur ces projets sont intégrées, le processus d'études, un aperçu des conditions actuelles, les options de solutions pour chacun de ces projets, la méthode d'évaluation utilisée pour choisir une option privilégiée provisoire et les prochaines étapes du processus d'évaluation environnementale.

Aperçu du projet de prolongement vers l'est de la Ligne de la Confédération du train léger sur rail

La Ville d'Ottawa entreprend une étude d'évaluation environnementale (ÉE) portant sur le prolongement du train léger sur rail (TLR) de la station Blair jusqu'au chemin Trim, le long du couloir de la route 174, conformément au processus d'évaluation des projets de transport en commun. Le prolongement du TLR vers l'est (TLRE) est l'une de trois études d'ÉE en cours à la 2^e étape, qui porte sur le plan de la Ville d'apporter les avantages du transport par train léger aux résidents des secteurs se trouvant plus à l'est, à l'ouest et au sud, qui ajoutera 19 nouvelles stations et 30 kilomètres de voies au réseau de TLR. Le TLRE assurera un service rapide et fiable aux collectivités de Beacon Hill, Blackburn Hamlet et Orléans, ainsi qu'à la localité rurale de Cumberland. On propose un total de huit stations entre la station Blair et le parc-o-bus du chemin Trim. L'emplacement de ces stations a été conçu pour raccorder les services actuels et à venir de transport en commun et les parcs-o-bus du secteur. Le secteur visé par l'étude pour ce projet s'étend du chemin Blair à l'ouest et longe le couloir de la route 174 jusqu'au chemin Trim à l'est.

Aperçu du projet d'élargissement de la route 174 et du chemin de comté 17

Les Comtés unis de Prescott et Russell, en partenariat avec la ville d'Ottawa, entreprennent une étude d'ÉE de catégorie A portant sur le couloir de la route 174 et du chemin de comté 17 (CR17) de Prescott-Russell à partir de l'autoroute 417 jusqu'au chemin de comté 8 (chemin Landry). Cette étude est réalisée conformément aux exigences de l'annexe « C » du document sur les évaluations environnementales municipales de portée générale. Le secteur visé par l'étude pour le projet de la route 174/chemin de comté 17 s'étend de la jonction avec l'autoroute 417 à l'ouest et longe le couloir de la route 174 jusqu'au chemin Trim, où le secteur visé par l'étude s'élargit pour englober plusieurs trajets de remplacement. Le secteur visé par l'étude pour le TLRE chevauche la portion de la route 174 située à l'ouest du chemin Trim.



Secteur visé par l'étude pour le TLRE et la route 174/chemin de comté 17

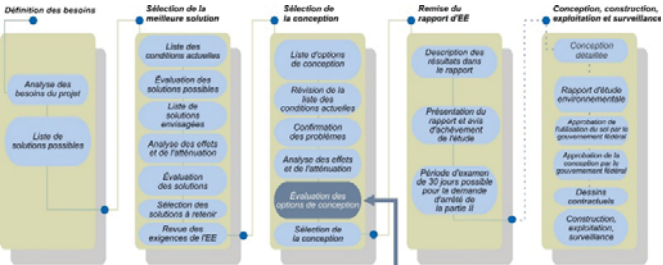
Coordination de la démarche des évaluations environnementales

Pour trouver une solution optimale pour ces deux projets dont les secteurs visés par l'étude se chevauchent, on coordonne la démarche de ces évaluations environnementales. Il est important de coordonner la démarche des deux ÉE afin :

- de maximiser l'utilisation du couloir routier actuel;
- de minimiser les conséquences sur l'environnement;
- de minimiser les coûts de construction;
- de tirer le meilleur parti possible de l'information obtenue dans l'étude;
- d'intégrer les activités de consultation avec les intervenants;
- de simplifier le processus d'ÉE;
- d'échanger de l'information, les travaux de conception technique et les consultations avec les intervenants.

Processus d'étude d'ÉE

Conformément au processus d'évaluations environnementales municipales de portée générale, l'élargissement d'une route ou la construction d'une nouvelle route ou installation asphaltée de plus de 2,3 M\$ constitue un projet de l'annexe C. Les projets de l'annexe C demandent la préparation d'un rapport d'évaluation environnementale, qui sera soumis à un examen public.



Processus d'évaluation environnementale municipale de portée générale pour un projet de l'annexe C

Conformément au processus d'évaluation des projets de transport en commun, qui est le processus d'ÉE suivi pour le TLRE, nous en sommes actuellement à l'étape de pré-planification et de consultation. Cette étape comprend l'élaboration et l'évaluation d'options. La présente séance portes ouvertes présente la sélection de l'option de tracé privilégiée pour le TLRE au moment de l'étape de pré-planification et de consultation.



Processus d'évaluation des projets de transport en commun

Conditions actuelles

Des données environnementales de référence ont été recueillies; elles serviront à évaluer les effets de ces projets sur l'environnement et permettront ainsi de proposer des solutions raisonnables, de déterminer leurs effets et de les évaluer.





Études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17

Option privilégiée provisoire – À l'ouest du chemin Trim

Une analyse globale du rendement a été effectuée pour déterminer si un tracé hybride, qui prend des sections de plus d'une option, donnerait une meilleure solution. On a élaboré une solution hybride qui utilise le couloir nord du secteur du ruisseau Greens, puis emprunte la voie médiane. Ce tracé hybride tire avantage :

- d'une proximité des secteurs aménagés, qui peut favoriser le développement axé sur le transport en commun et l'achalandage du chemin Blair au chemin Montréal;
- de meilleures liaisons avec les sentiers polyvalents;
- d'une réduction du nombre de structures requises;
- de la conservation des passages supérieurs du Transitway (autobus) de la route 174 pendant et après les travaux;
- de la réduction du coût global d'immobilisation;
- de la réduction des conséquences de l'échelonnement des travaux sur le transport en commun et la circulation automobile actuels.

Après avoir quitté la station Blair, le tracé du TLR longe le côté nord de la route 174 jusqu'à ce qu'il traverse dans le terre-plein près du ruisseau Greens. Le TLR continue dans le terre-plein jusqu'à ce que le tracé se termine au chemin Trim. L'image ci-dessous montre le croisement près du ruisseau Greens. Le tracé du TLR est en rose, les stations du TLR sont en vert et la route 174 actuelle est en bleu pâle. Les endroits où la route sera élargie sont en bleu foncé.



Option privilégiée provisoire – À l'est du chemin Trim

L'option 1, l'élargissement de la route 174/chemin de comté 17, a été choisie à la suite de l'application de la méthode de concordance. L'option 1 est privilégiée parce qu'elle :

- répond le mieux aux besoins en transport;
- demande moins d'emprises additionnelles et touche moins de bâtiments;
- appuie les politiques relatives à la durabilité;
- appuie le transport en commun et se raccorde au couloir du TLR;
- ouvre des possibilités d'amélioration des installations pour piétons et cyclistes selon les besoins;
- appuie la nouvelle conception et les améliorations prévues à l'environnement routier dans les collectivités;
- permet d'envisager la désignation de voies réservées aux VTOE;
- évite l'ajout d'infrastructures à des aires naturelles non aménagées.

La ligne jaune dans l'image ci-dessous représente l'emplacement de la route 174 et du chemin de comté 17 où se fera l'élargissement.



Prochaines étapes

Voici les prochaines étapes du processus d'étude :

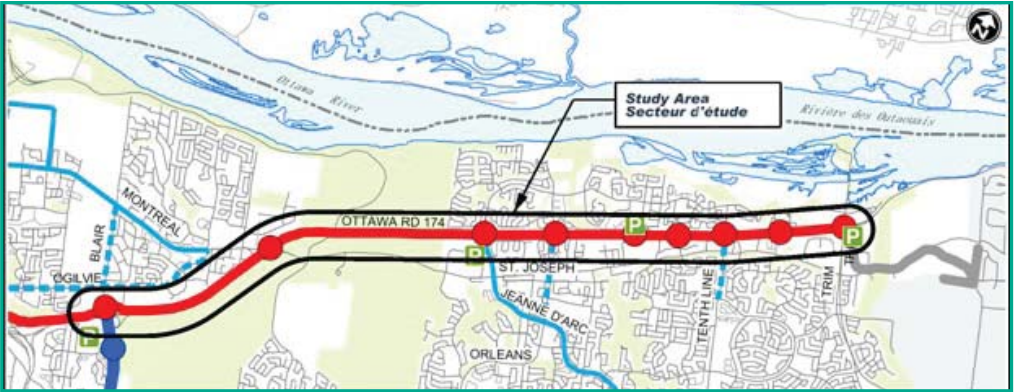
- Intégrer les commentaires et confirmer les solutions privilégiées;
- Élaborer les détails de la conception notamment la conception et l'emplacement des stations, la configuration des échangeurs, la conception des intersections, les coupes transversales du village de Cumberland et les liaisons pour piétons et cyclistes;
- Déterminer les mesures d'atténuation;
- Tenir la prochaine consultation publique à la fin du printemps 2015;
- Présentation au Conseil et Comités;
- Rapports sur les évaluations environnementales.

Questions or Comments	
Si vous souhaitez nous faire part de vos commentaires supplémentaires, transmettez-les par courrier, d'ici au 13 février 2015, à l'adresse suivante	
Angela Taylor, ing. Ingénieure principale de projet Ville d'Ottawa 110, avenue Laurier Ouest Ottawa (Ontario) K1P 1J1 ☎ : 613-580-2424, poste 15210 Courriel : Angela.Taylor@ottawa.ca	Marc R. Clermont, ing.. Directeur des Travaux publics Comtés unis de Prescott et Russell 59 rue Court, Case Postale 304 L'Orignal, ON K0B 1K0 ☎ : 613-675-4661, poste 3100 Courriel : MClermont@prescott-russell.on.ca



CONFEDERATION LINE EAST LRT EXTENSION/ HIGHWAY 174 WIDENING PROJECT

HIGHLIGHTS



The Stage 2 Confederation Line LRT East Extension will:

- Add 10 kilometres of rail and four new stations to the City's overall transit network. A potential extension to Trim Road that would add more stations and 3.5 kilometres is also being considered as part of the Environmental Assessment study.
- Bring 95 per cent of all Orléans, Blackburn Hamlet and Beacon Hill residents within five kilometres of rail.
- Provide new and direct access to more comfortable, reliable and efficient transit for residents near Orléans Boulevard and Montreal Road.
- Create opportunities to improve bus service connections through transit priority projects on Orléans and Jeanne d'Arc Boulevards to better link residents to the main rail line.
- Provide a more efficient rapid transit transfer point in Orléans for Clarence-Rockland Transit.
- Connect to existing Park and Ride facilities at Place d'Orléans and eventually to Trim Road.
- Offer a cost-effective alignment that minimizes community impacts and land requirements while maintaining excellent connectivity to surrounding communities.
- Connect to adjacent communities through existing and proposed multi-use pathways.

- Further improve transit service to major mixed-use centres at Blair and at Orléans Town Centre, major employment areas at Canotek Park and at Taylor Creek, numerous residential communities, as well as to other destinations including the Bob MacQuarrie Recreation Complex, La Cité Collégiale, and Place D'Orléans Shopping Centre.
- Integrate with the Highway 174 widening environmental assessment to achieve better design outcomes, ensure efficiencies of scale, and maximize benefits both to the community and to the environment.

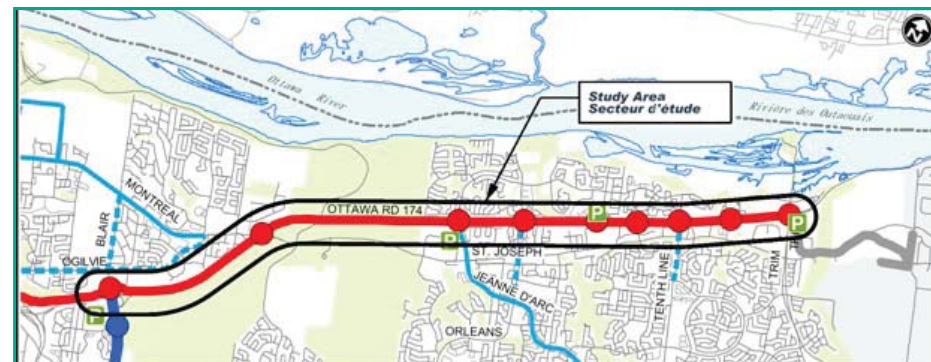
The Highway174 widening will:

- Provide additional capacity to serve the predominant flow of traffic and avoid adding infrastructure to undeveloped natural areas.
- Integrate designs that are appropriate for the surrounding communities, including median barriers to improve safety, and east of Trim Road, paved shoulders for pedestrians and cyclists, and intersections designed to manage speed.
- Provide significant quality of life enhancements in Cumberland Village, including reduced speeds, improved pedestrian and cycling facilities, enhanced river access, additional lighting and gateway features to the Nation's Capital.
- Allow for the consideration of HOV lane designations.



PROLONGEMENT VERS L'EST DE LA LIGNE DE LA CONFÉDÉRATION DU SYSTÈME (TLR) ET L'ÉLARGISSEMENT DE LA ROUTE 174

POINTS SAILLANTS



Le prolongement vers l'est de la Ligne de la Confédération du système (TLR) de l'Étape 2 va :

- Ajouter 10 km de rails et quatre nouvelles stations au réseau global de transport en commun de la Ville. Dans le cadre de l'étude d'évaluation environnementale, on envisage aussi un prolongement possible jusqu'au chemin Trim, ce qui ajouterait quatre stations et 3,5 km.
- Faire en sorte que 95 pour cent de tous les résidents d'Orléans, de Blackburn Hamlet et de Beacon Hill se trouvent à 5 km d'une ligne ferroviaire.
- Fournir un nouvel accès direct à un transport en commun plus confortable, plus fiable et plus efficace pour les résidents situés à proximité du boulevard Orléans et du chemin Montréal.
- Créer des possibilités d'améliorer les liaisons par autobus au moyen de projets de transport en commun prioritaires sur les boulevards Orléans et Jeanne d'Arc afin que les résidents puissent plus facilement avoir accès à la voie ferrée principale.
- Fournir une correspondance du transport en commun rapide plus efficace à Orléans pour le transport en commun Clarence-Rockland.
- Se relier aux installations existantes de parc-o-bus à la Place d'Orléans et plus tard au chemin Trim.
- Offrir un tracé rentable qui réduit au minimum les répercussions sur la communauté et les besoins en matière de terrains tout en maintenant une excellente connectivité aux communautés environnantes.
- Se relier aux communautés adjacentes par l'entremise de sentiers polyvalents existants et proposés.

- Améliorer encore le service de transport en commun vers les grands centres polyvalents à Blair et au centre-ville d'Orléans, les principales zones d'emploi du parc Canotek et du parc d'affaires Taylor Creek, les nombreuses communautés résidentielles, ainsi que vers d'autres destinations, y compris le complexe récréatif Bob MacQuarrie, La Cité Collégiale et le centre commercial Place d'Orléans.
- S'intégrer à l'évaluation environnementale de l'élargissement de la route 174 pour atteindre de meilleurs résultats de conception, assurer des économies d'échelle et maximiser les bénéfices pour la communauté et l'environnement.

L'élargissement de la route 174 va :

- Fournir une capacité supplémentaire qui tient compte du flux prédominant de circulation et évite l'ajout d'infrastructures à des aires naturelles non aménagées.
- Intégrer des conceptions adaptées aux communautés avoisinantes, y compris des terres-pleins pour améliorer la sécurité, des accotements asphaltés pour piétons et cyclistes et des intersections conçues pour gérer la vitesse.
- Fournir des améliorations significatives de la qualité de vie dans le Village de Cumberland, y compris une vitesse réduite, des infrastructures pour piétons et cyclistes améliorées, un meilleur accès à la rivière, un éclairage additionnel et des points d'accès à la capitale nationale.
- Permettre d'envisager la désignation de voies réservées aux VTOE.

APPENDIX C: OPEN HOUSE COMMENT SUMMARY LOG: COMMENT SHEETS AND CORRESPONDENCE

C1) Summary of Questions/ Comments

Topic	Comment/Question Summary	Comment by POH location
Transit – Station Placement	Suggestion given to focus on three stations in Orleans: Place D'Orléans, Tenth Line and Trim Road only.	Cumberland
	Transfer times should be addressed at Place D'Orléans station. Consider an under loop station similar to St. Laurent Station.	Cumberland
	The extension of the LRT to Trim Road should be the priority. There is not enough distance between stations in Orléans, and the starting and stopping would increase travel times.	Cumberland
	Concern that there are too many stations (Orleans and up to Taylor Creek). Trim Road as the terminus would be a sensible solution and there will be less bus traffic on the existing overburdened Highway 174/17 East.	All
	Reduce the number of stations to stay on budget, and rely on buses between stations. Extend the LRT right to Trim the first time rather than a phased approach.	Orleans
	How has the integration of LRT and OR 174 from Blair Station to Trim Road been considered?	Orleans
	With median stations, does this mean that buses will stop in the middle of the bridges to pick up passengers from ELRT? Passenger comfort must be considered given cold winters in Ottawa. How many bridges will need to be replaced?	Orleans
	Consider removing the Orleans Town Centre Station. Take the train from the median using a Trim Road overpass.	Orleans
	Consider an extra station at Jasmine Park to provide a fly-over from the north to the median of Highway 174.	Orleans
	Use a central platform station in the median over Montreal Road.	Orleans
Financial Costs	What are the financial costs of the proposed highway widening, and comparison to options 2 and 3?	All
	How will property taxes be affected by the ELRT project?	Orleans/Rockland
	Prescott and Russell does not have the funds to support the Highway 174 project. What are the projected 2031 costs considered for the first model?	Rockland
	OC Transpo uses expensive technology that serves mainly peak hours with only 20% ridership. Concern expressed that the LRT in Orleans will be less expensive than other lines, but Orleans residents will have to pay for improvements elsewhere first.	Orleans
	The project team should have detailed financial information on the 3 options before making a recommendation.	All
Transit – Local Routes	Improve local transit connections as part of the LRT. These are important to improve speed and efficiency.	Cumberland
	More direct routing from local buses from Valin/Trim to Ottawa Road 174/Trim. The current journey takes too long.	Orleans
	LRT/ local bus interconnection speed/timing is an important consideration.	Orleans
	The feeder local bus system is required to make the LRT system work, and to provide the right ridership levels.	Orleans
	Will the current express buses in Orleans continue once the LRT line is active? If not, transfer times may increase transit time to the downtown core.	Orleans
	Impacts on fares between the LRT and local buses should also be covered.	
	Existing bus lanes in Orleans should be converted into HOV or back into traffic lanes. These lanes should be upgraded as they are not safe in winter conditions.	Orleans
	Temporary bus stops should be placed along Highway 174 at Blair Station.	Orleans
	Existing City buses meet Ottawa's needs in public transportation. This is since many buses are half full now.	

Topic	Comment/Question Summary	Comment by POH location
Route Selection	Support was expressed for the median design option for the light rail transit.	Cumberland
	Defer constructing Orleans Blvd. to take LRT towards Trim Road as soon as possible.	Orleans
	What will the route alignment be at Trim station – north or south side?	Orleans
	Rail safety in the highway median will be a concern due to bad weather, traffic accidents or road work. How will this be addressed?	Orleans
	Has detailed engineering been completed prior to determining route #1 as most favoured?	Cumberland
	Support given for the hybrid/median option recommended strategy.	Orleans
	Support given for the project’s recommended route.	Orleans
	Support given for expanding Highway 174 since it is the preference for truck drivers and shoppers already. Existing alternative routes are already underutilized.	Orleans
	Ease strain on Highway 174 by upgrading/ expanding an alternative route.	Orleans
	Centre corridor approach for the LRT make the most financial and structural sense.	Orleans
	Consider a small ring road around the south of Rockland at the base of the East-West escarpment	Orleans
	Preference given for Alternative 3 west of Trim Road.	Orleans
	Expand the 174 to 3 lanes in each direction only as far as Jeanne D’Arc where the congestion builds.	Orleans
	Where is the information located regarding the integrated alignment alternatives including the preferred plan west of Trim Road?	Cumberland
	How were the alternative road corridors evaluated from Trim Road to Landry Road, including the preferred road corridor?	Orleans
	Is the existing right of way in Wilhaven Drive part of Option 2? If so, does the right of way allow the city to use it?	Cumberland
	Suggestion to consider a hybrid option of 1 and 3 to bring the 174 from east of Cameron to Rockland up to the same standards, and to focus on rural improvements.	Cumberland/ Rockland
	Support for the Innes to Baseline expansion project as it provides a new exit for Rockland and it is the cheapest.	Rockland
	Why has there been no study regarding expanding Bus Rapid Transit (BRT) or the Eastern LRT (ELRT) to Rockland?	Orleans
	Some concerns that Stage 2 only extends to Place D’Orléans and not Trim Road.	Orleans
	Constructing rail lines in the median of highways has been successful in Montreal and should be supported.	All
	Concerns were expressed that the highway expansion evaluation criteria were influenced by considering the LRT system.	All
	The highway should bypass Rockland going south or west due to the number of accidents already in Rockland.	Rockland
	Preference for the LRT west of Trim Road running on the south side on a transit right of way to Millennium Park, not along Highway 174. This will help increase capacity from north and south.	Orleans
	Preference for a ring road near Wilhaven Drive from Trim Road to Baseline Road in Cumberland), or extending Innes to Baseline Road.	Cumberland
	East of Trim Road, growth is occurring south of Highway 174. Preference for 3A or 3B since it allows the Highway to serve growth on both sides. A new road east of Beaton should curve further south to keep away from residents at Beckett’s Creek and Wilhaven Drive.	Orleans/Cumberland
	The existing Transitway underpass at Blair station should be used for the (LRT) train.	Orleans
	Consider retaining walls near Green’s Creek to provide more space.	Cumberland
	Going through southern corridors where vacant land exists is believed to be more expedient and less confrontational.	Cumberland
	Preference for ring roads to the south, using Innes, Frank Kenny, or other roads south of Wilhaven in Cumberland). These would offer the least disruption and be more amenable to future growth	Cumberland

Topic	Comment/Question Summary	Comment by POH location
Highway – Volume Increases	Option 3 may add to existing road constraints East of Blair. Concern that Innes Road and Renaud cannot handle current traffic volumes.	Orleans
	Concern about Trim Road as a potential bottleneck once volume increases there. Does the study assume future growth from Clarence/Rockland?	All
	Suggests Options 3B (Brian Coburn) with 3A (Innes to Baseline), as well as improving access from Beckett’s Creek and County Road 8 as affordable ways of decongesting traffic from the east towards the LRT.	Orleans/ Cumberland
	Once the highway project is built, how close to its capacity will it be (assuming it is built in the next 10-15 years)? Will the new highway have enough capacity?	All
	Concern that Highway 174 capacity constraints will result in more unsafe practices within Cumberland.	Cumberland
	Widening a highway only increases congestion since people purchase property further from jobs. Suggests building a BRT or LRT to Rockland first.	Orleans
	The issue with Highway 174 is Orleans itself, not the rural areas.	Orleans
Highway – Property Impacts	Where will expropriation lands be required for the highway route?	Cumberland
	To what extent will highway widening result in encroachments on private lands along the route, including the Village of Cumberland?	Cumberland
	What are the impacts from the highway expansion on residents in adjacent areas, including emergency access? Plans for “turnarounds”?	All
	Landowners should be formally contacted early in the process.	All
	How many farms/ hobby farms would be affected by the Innes/Boundary Road option?	Orleans
	How many properties would require some expropriation due to the Highway 174 widening?	All
	A central road through Clarence-Rockland will create opportunities and accessibility for all citizens. It will encourage real estate development towards the South.	Rockland
Highway – Speed	Land values will drop for locations near the highway expansion, particularly waterfront properties.	All
	Will the project be designated as a “parkway”?	Cumberland
	How will speed reductions be enforced without Regional Road 174 designated as a parkway?	Cumberland
	Travel time should be given an appropriate weighting in the evaluation.	All
Environment	A major highway should have traffic lights removed, such as options 2-3A and 3B.	All
	How will farmland be protected along the route?	Cumberland
	How will the Ottawa River be protected from increased salt, contaminants, accidents, oil spills and road kill?	Cumberland
	Why were environmental considerations given a low rating: 5%?	Cumberland
	More discussion is warranted on a new interprovincial bridge and its impacts on the study.	All
	Explain why crossing Beckett’s Creek is a challenge for environmental impacts, while the highway also goes along the Ottawa River.	All
	Request for information about four lane roadways built along major rivers.	Cumberland
	Concern about the low (5%) weighting for environmental concerns. Has the RVCA and Ottawa River been consulted?	All
	The risk of contamination and health issues along the Ottawa River should be considered.	Cumberland
	Concern that a low evaluation weighting was attached to the environment, given increased salt, runoff, and the potential for increased spills and accidents into the Ottawa River. More data is requested on evaluating environmental impacts.	Cumberland
	Environmental protection is important for areas east of Blair to Montreal Road, and Green's Creek.	Orleans
	Noise and water pollution are potential issues with the Ottawa River that need to be addressed.	Cumberland

Topic	Comment/Question Summary	Comment by POH location
Accessibility	The proposed area for expanding Highway 174 is within an ecologically sensitive area, and the “turtle crossing” signs are indicative of this.	Cumberland
	Priority accessible seating is desired and needed for the LRT	Orleans
	There is a need to invest in public restrooms within stations as a public health measure, but also that there is access for the general public and individuals with disabilities.	All
Transit – Maintenance and Delays	What plans are in place for addressing an immobilized LRT on route (Montreal Road and Jeanne D’Arc in particular)?	Orleans
	How are people going to get off (the LRT) if it stops between stations? Highway 174 would have to be shut down to load people onto buses.	Orleans
	Consider a train storage facility at Trim Road.	Orleans
Parking/ Park and Rides	Increase parking capacity at Place D’Orléans due to non-residents using the station instead of Trim.	Orleans
	The Park and Ride at Place D’Orléans will be completely overwhelmed if it is the end point of the LRT line.	
	Park and Ride at Trim Road is currently at capacity, prior to expected project volume increases.	Orleans
	Need to plan for additional parking lots along the proposed route, with a suggestion for Champlain.	Orleans
Pedestrian/Cyclists	Increase pedestrian/cycling paths	Orleans
	Wider sidewalks on Jeanne D’Arc/ Regional Road 174 overpass	Orleans
	Path lighting improvements – Blackburn Hamlet to Montreal Road station (too dark and dangerous)	Orleans
	Securing parking (at stations and on LRT trains) and bicycle ramps on the stairs are recommended.	Orleans
	What pedestrian/cyclist crossings are planned? Are pedestrian tunnels planned?	Cumberland
	Highway expansion could limit pedestrian traffic from north of the village (Cumberland) to the south of the village.	Cumberland
Traffic	Suggestions of non-LRT related road improvements to alleviate traffic volume concerns in Orleans	Orleans
	Construction-related traffic concerns during rush hours	Orleans
	Need to reconfigure the road network around the Place D’Orléans park and ride	Orleans
Performance Metrics	How will ELRT progress be measured in terms of ridership and travel time. How will ridership fees/ operational costs evolve?	All
Noise	Highway – Sound barrier requested (Multiple locations identified along Highway 174).	Orleans
	Sound barriers are needed. Traffic noise is constant day and night.	Orleans
Vibration	Highway vibrations from existing traffic at Highway 174, including bus lanes.	Orleans
	Highway vibrations at Highway 174 and Jeanne D’Arc – windows shake when heavy vehicles drive by.	Orleans
Open House – General	Use an audio system/microphone to keep the audience attention.	All
	Sign-in sheets could be improved to consider the privacy of names.	All
	More emphasis at the Open Houses should be on the benefits of the ELRT and what the new service will provide.	All
	Concern about bias towards highway expansion, given that the widening of Highway 174 has been considered for many years within the Transportation Master Plan.	All
	Evaluation scoring – within 10% should be considered equal. Belief that the selection was made without considering price or schedule.	All
	More emphasis is needed on reaching younger people that stand to benefit from the project (post -2031).	All
	Meeting venue had poor lighting for the poster session and the room was too small.	All



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

We are in agreement with the preferred route - the expansion of the existing Hwy 174. We own a 5 acre property at 1991 Beckitt's Creek Rd and @ 100 acres of farmland fronting Wilhaven Dr. We want to protect the farmland.

The owners of the homes along Hwy 174 should have known, as we have, that the expansion of the hwy has always been a possibility.

We are life long residents of Cumberland, we have watched the village change over the last 5 decades. The expansion has always been in the plan proposed for at least the last 3 decades.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL :

ADDRESS:

/PHONE

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

ottawa.ca/planning
ottawa.ca/urbanisme

Appendix C:

C2) Open House Comment Sheets

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

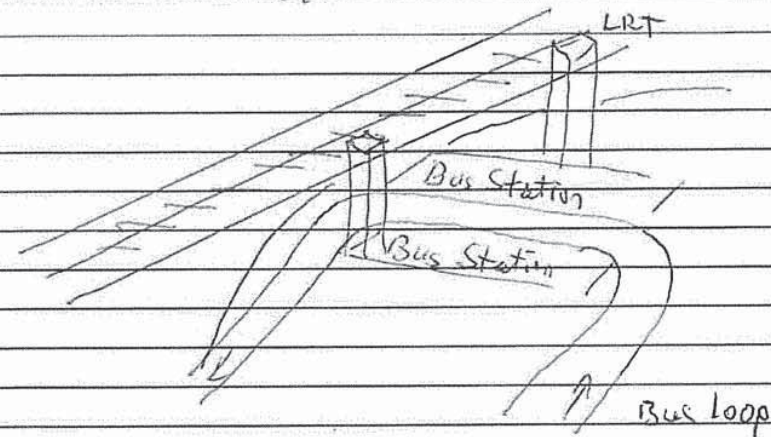
Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

With regards to the Place d'Orleans LRT station, has any consideration been given to creating an underpass station to bring the buses below the LRT station and in so doing minimize the distance to transfer to local routes and reduce the volume requirements to both the mall and the park & ride



Concept would be similar to the St-Laurent Station with the LRT running above.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

with the growth going south I do not foresee that the route proposed as an option would be going through Class A agricultural land (farm). The farmers will have sold their land to developers well before that time.

By the way - your sign in registration is not compliant in the privacy laws - others signing the sheet should not have access to my & other attendees name & contact details.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Mardi 3 février 2015
Centre comm. Comm. R. J. Kennedy
1115, chemin Dunning, Cumberland

Mercredi 4 février 2015
Complexe récréatif Bob MacQuarrie
1490, promenade Youville, Orléans

Jeudi 5 février 2015
Salle Guy Faubert
954, rue Giroux, Rockland

Dites-nous ce que vous pensez...

ENSURE THERE
IS AN AUDIO
SYSTEM IN ORDER
TO PROVIDE UNDEVIDED
ATTENTION!!!!

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 13 février 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADRESS :

TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement de la Ligne Trillium. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

If you think you will expropriate
even a portion of properties along the
174 for a bike lane, you have
a fight on your hands.

Absolutely
Rediculous

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL :

ADDRESS :

PHONE :

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1



Prescott
Russell

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

After listening to the presentation it is obvious
that the decision has already been made
as to where the 174 will be widened. This meeting
is only to inform us.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL :

ADDRESS :

PHONE :

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

I do like the median design for the light rail on the highway.
Do we really need so many stations in Orleans. They should
have ~ station at Place D'Orleans, take line then from.
I hope there is plans for an overpass at Frim road.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL :

ADDRESS :

PHONE :

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

1. IT WOULD APPEAR THE 174 OPTION HAS BEEN CHOSEN WHICH TOTALLY IGNORES FUTURE GROWTH. ALTHOUGH IT DOES REQUIRE AN UPGRADE THERE IS NO POTENTIAL FOR FUTURE GROWTH.
 2. OPTION 'B' ALTHOUGH IN THE COUNTRY NOW WILL ALLOW FOR GROWTH BOTH IN OTTAWA & TO THE EAST.
- BREAK THE MOLD! PLAN AHEAD FOR A CHANGE.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL: _____

ADDRESS: _____

PHONE: _____

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Keep up the good work.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL: _____

ADDRESS: _____

PHONE: _____

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Valin/Trim to 174/Trim needs a more direct route.
(there can be many things that slow down the 94 to Blair Rd.)
(also, any bus line along the South will take time to build)

I depend on the public transit service, sometimes trips
downtown are 1 1/2 hrs. (w/ traffic). (sometimes admit more)

A friend of mine who lives in Kanata (who also
uses public transit - we share the same medical disorder) finds
it takes a very long time to travel to my home (on me
to theirs). Please keep strong connections to the train built.

Trim is a major thoroughfare, keep traffic along it direct.

(Does) Will the train have priority seating use? I use it.

I support you are expanding east-west transit;
remember to make it helpful for all. LRT offers
more uninhibited travel than a bus, keep it direct & help all.
(Maintain good access to LRT)

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Problem: overloading parking at Place d'Orléans

In 1st Phase where LRT ends at Place d'
people from Rockland and Cumberland
will not park at TRIM and bus to Place d'

They will go direct to Place d', which will
overwhelm current parking facilities.

So, how to mitigate?

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Mardi 3 février 2015 Centre comm. Comm. R. J. Kennedy
1115, chemin Dunning, Cumberland

Mercredi 4 février 2015 Complexe récréatif Bob MacQuarrie
1490, promenade Youville, Orléans

Jeudi 5 février 2015 Salle Guy Faubert
954, rue Giroux, Rockland

Dites-nous ce que vous pensez...

Excellent projet.
Merci pour l'information!

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 13 février 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADRESS :

TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement de la Ligne Trillium. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015 R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015 Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015 Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Great job, keep up the hard work.
I sure vote for the ~~model~~ hybrid alignment

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MA"

ADDRESS :

PHONE .

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

- The Centre Corridor for LRT Makes the most financial and structural sense
- Ensure More Direct Bike/walking Paths from Blackburn Hamlet to Montreal Rd station with appropriate lighting* currently too dark and dangerous
- Defer constructing Orleans Blvd to take to Trim as soon as possible

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Excellent presentation of the transit plan (LRT)
answered all my questions and then some.
Congratulations!!

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

*I live at Darkrose pvt in between 10th Line & Trim
on the North side of Hwy 17 (Close to Petro-Canada)*
- Will there be a sound Barrier wall built due to noise?
*- When the LRT will reach Trim, ~~are~~ will the trains
be on the North side or the South side of Hwy 17?*

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

*The LRT South of Hwy 174 seems more logical to me as it leaves
expansion and/or repairs to road network more accessible and less
of a problematic - Rails in the median could be a hazard in bad
weather, during road work and if there is a traffic accident.*
*Option 3 For expansion of Road Network to East would add a second
route to an already limited road network EAST of Blair.
ORLEANS suffers greatly when it comes to access to downtown
Ottawa with Hwy 174 being the major route. Secondary roads such
as Innes Rd and Rignead can not handle the current traffic volumes
Wakeley Rd and Hunt Club Rd should be extended out to Naven
Rd as a minimum, if not then to Frank Kenny and Vars*

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

The Stage 2 LRT presenter (David Hopper) was exceptional and provided me with immense confidence about the project, the deep thinking involved and the clear expertise of City of Ottawa staff.

For many strong and convincing reasons, I hope the city moves forward with Hybrid / Median option that was recommended during the presentation.

If the incremental cost is not astronomical, it would be worthwhile to continue to go forward with the LRT in Stage 2.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

2023-8445 - ANGELA TAYLOR Project Manager, David Hopper, Val McGeer, Smith
180-35-35 project Median = 15m wide

LRT in the Median, HYBRID PLAN SOUNDS FINE.

My concerns ARE WITH actual construction, How it will affect TRAFFIC.

Will there still BE 4 lanes for cars AT ALL TIMES?

RUSH TRAFFIC IS HORRIBLE NOW. What WILL happen if it's further impeded?

SOLID TRAFFIC ALTERNATIVES need to BE in Place.

Before construction STARTS. Maybe using the "BUS" LANES, ETC.

② What happens when an LRT dies between MTL RD AND Jeanne Dore? Will we have LRT Ambulances?

IF these concerns ARE NOT dealt with, public opinion will BE harsh and very hard ON Politicians. I should thank.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

I am Retired now- which I will not
used the LRT as much. Previously used
O.C. Transpo for many years.

But the presentation was well done
with Dave. The rest of the Meeting

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Kudos on the smart thinking to combine
an EA to include the future of LRT into
Orléans and the expansion of the 174 -it's
this type of forward thinking that we need
more of.

My top concern is about the availability
of park and ride; currently, Trim is at capacity
or almost at capacity most days. The development
on the north side of place was going to
remove parking, with the expected increase
in ridership due to LRT this needs to
be addressed ASAP

A concern of mine is also for cyclists - will we have
secure parking? can we have the bike ramps on
the stairs? and will we have bike parking available
on the trains?

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/Ouest, Ottawa (Ontario) K1P 1J1

Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

Good large posters but impossible to tell the order in which they should be viewed to make the most sense - please consider numbering in future.

Presence of City of Ottawa employees to explain signs helped a lot.

Excellent presentation. David Hopper was extremely knowledgeable, funny, and sensitive to the crowd. I feel very informed.
↳ Larger room preferred in future, please - and cooler.

I approve of the preferred solution with the hybrid model.
It seems well thought out.

- ↳ good attention to pedestrian, cyclist, and car access.
- ↳ Looking forward to Jeanne d'Arc access.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL :

ADDRESS :

PHONE :

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

- May 14th LRT in the median is my preferred option.

~~The 10 terms are not have enough potential~~
~~relationships. It's a new, there is a new way to think~~
~~about things and the new terms are not clear at all.~~

- I'd like to see wider sidewalks on Penn Ave / 17th Ave
overpass once LRT is in service to make the street more
pedestrian friendly.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL :

ADDRESS:

PHONE :

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Mardi 3 février 2015 Centre comm. Comm. R. J. Kennedy
1115, chemin Dunning, Cumberland

Mercredi 4 février 2015 Complexe récréatif Bob MacQuarrie
1490, promenade Youville, Orléans

Jeudi 5 février 2015 Salle Guy Faubert
954, rue Giroux, Rockland

Dites-nous ce que vous pensez...	
Merci pour votre excellente présentation à Rockland le 5 février 2015.	
Je favorise l'extension du chemin Ines jusqu'à Baseline.	
1° Cela nous offre une autre sortie de l'aire urbaine à Rockland.	
2° Cette option coûte moins cher que toutes les autres.	
3° La désignation route touristique de la route 174 est un choix sage qui saura promouvoir le tourisme dans l'est d'Ontario.	
4° Il faut éviter de faire un choix politique. M. Jean-Marc Lalonde défend depuis trente ans l'élargissement de la 174. Il en a fait sa mission personnelle et le centre de son programme électoral. Il a tort! Nous devons faire un choix intelligent avec une vision d'avenir.	
5° Une route au centre de Clarence-Rockland ne retirera la vedette à Rockland et offrira une solution accessible à tous les citoyens. Cette route au centre de la cité favorisera le développement immobilier vers le sud.	
Veuillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 13 février 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca	
Optional information:	
NOM:	COURRIEL :
ADRESSE :	TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement de la Ligne Trillium. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Thank you for your excellent presentation on February 5th in Rockland.

I support the Ines to Baseline expansion project.

1. It provides a new exit for Rockland's urban area.
2. This option is the cheapest.
3. OR 174's labelling as a "touristic route" is a wise decision, as it will promote tourism in eastern Ontario
4. Political bias should be avoided. Mr. Jean-Marc Lalonde has been advocating for the expansion of OR 174 for thirty years. It has become his personal goal and the core of his electoral platform. He is mistaken! We must make a conscious decision in the best interest of our community's future.
5. A central road through Clarence-Rockland will create opportunities beyond Rockland and consists of an accessible solution for all citizens. This central road will encourage real estate development towards the South.

attended the presentation(s) last Wednesday evening. Though we came away very much enlightened with the status and progress of the project, we were disappointed with the lack of sensitivity and understanding surrounding the noise and vibration issues. Both these questions were raised by the floor and quickly dismissed by the speaker(s), with an apparent measure of contempt.

We can only hope that the next meeting will address more appropriately these concerns. Like some neighbours have indicated, maybe it's just "time to move." Though of course, that solves nothing for others. But then, we certainly don't look forward to the additional hell we'll experience when the construction stage begins given the extra dust, noise, vibration and traffic jams the project will produce.

We will attend the meeting on February 4 at the Recreation Complex. In the interim, we have attached a few "before and after" pictures of our back yard.

We have lived here for 30 years. A couple of years ago, we buried our "very expensive" in-ground pool because of the unbearable noise generated by traffic off the 174 as well as the traffic from the off-ramp to Jeanne-D'Arc which runs essentially, right into our back yard. We even no longer sit outside because of the noise. We don't even open windows or the patio door.

Why anyone would not back noise barriers is beyond reason and we don't understand why barriers have been installed all over the City and not here.

C3) Post-Open House Correspondence

Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

• No comments on LRT Extension

• OR 174 / PR CNTY R 17 WIDENING : SIMPLY PUT, THE PROJECT TITLE PREDEFINED THE PREFERRED OPTION. IN OTHER WORDS, PRIOR TO PROJECT START UP, THE CONSULTANT'S STUDY WAS PRE ADVISED TO VALUATE THE 174/17 WIDENING RATHER THAN STUDYING THE BEST LONG TERM OPTION FOR A MAJOR OTTAWA-MONTREAL/LAVAL LINK

• TRAVEL TIME FROM TRIM TO ROCKLAND HAS NOT BEEN ASSIGNED AN APPROPRIATE WEIGHT FACTOR IN CONSIDERATION THAT ~~7~~ TRAFFIC LIGHTS ALREADY EXIST WHILE SPEED LIMIT IS ^{PARTIALLY} ~~ALREADY~~ SET AT 70K. A MAJOR HWY LINK SHOULD NOT BE RID OF TRAFFIC LIGHT SUCH AS OPTION 2-3A & 3B.

• REGARDLESS OF METHODOLOGY USED TO VALIDATE PREFERRED OPTION, THE MOST EXPENSIVE OPTION SHOULD NOT NOR SHOULD IT EVER BE THE PREFERRED OPTION ESPECIALLY SINCE THE PREFERRED OPTION 1 DOES NOT HAVE AN ALTERNATIVE ROUTE BUILT TO COLLECTOR STANDARD AND POSES A HIGH RISK FOR HEALTH ISSUES & POTENTIAL CONTAMINATION TO THE OTT. RIVER

• IT SEEMS ODD THAT A LEAST COSTLY PHASED IN ALTERNATIVE LIKE A 3RD LANE ON 174/17 OR 2 LANE FIA OPTIONS 3A/3B ARE NOT BEING CONSIDERED ESPECIALLY SINCE CONSTRUCTION DOLLARS ARE AVAILABLE FROM FED/ON (BOM)

• OFF THE RECORD: OTTAWA'S P.M. (A.T.) SHOULD REFRAIN FROM SUGGESTING TO CUMBERLAND AND ROCKLAND RESIDENTS TO MOVE TO OTTAWA IF THEY DON'T LIKE THE COMMUTE

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

#3

From:
Sent: January-23-15 6:04 PM
To: Monette, Bob
Subject: Re: LRT/174 Widening Environmental Assessment & 2015 Budget Consultation - Étude environnementale du train léger & l'élargissement de la 174

Re: meeting on Feb 4,2015

We want to add our voice to the meeting, but will be out of town. Therefore, hopefully, this will help.

We have been residents of Chatelaine Village since 1981 ; in the same house and not immediately backing on the highway 174 . The first 25 years were very peaceful and we gardened and enjoyed quiet in our backyard . , was a parent "at home" with our 3 children . The last 10 years have been quite distressful. We only enjoy the garden in the middle of the day when the traffic is light, but of course that is the heat of the day in the summer when we don't really want to be in the garden. We now wear hearing aids which we take out when we go outside and it helps a bit , but not enough. It also means we can't talk to each other without the neighbours hearing as we must raise our voices. So, we have been discussing moving! That is sad and not really necessary for at least ten more years! We are being pushed out of our home! Mind you, as we do not back onto the highway itself, and as our home is well insulated, the traffic does not bother us indoors. We have done what we can ! We need the city to help!

We did notice some barriers being constructed along the highway in Convent Glen and hoped it would continue east! Will the barriers come in time to help us?

#4

From:
Sent: February-04-15 6:46 PM
To: Monette, Bob
Subject: I agree with your proposal

Sorry I couldn't make it out tonight (recovering from surgery.)

I agree 100% with the proposal that entails the LRT using the centre median. Also, sound barriers should be erected along this route to lessen the impact of any additional noise pollution to those residents living in homes nearby.
Thank you for taking the time to read this email.

#5

From:
Sent: February 04, 2015 10:14 PM
To: Taylor, Angela
Subject: LRT East / CR 174 Widening

Ms. Taylor,

I was unable to make it to tonight's open house regarding the eastern extension of the LRT and the widening of CR 174. I can't seem to find the display boards online.

Could you please direct me to the right place?

Thanks,

#6

OR 174/County Road 17 Widening Open House

The 174 Working Group of the Cumberland Community Association was invited to a pre-screening last night of tonight’s presentation. Tonight, you will hear the “preliminary” conclusion of the environmental assessment that, of the three alternatives considered, the “preferred corridor” is to widen existing regional road 174/highway 17 to four lanes from Trim Road to Rockland. In the absence of significant opposition, the study group intends to cease considering other options and proceed with a more detailed study of the 174/17 widening. You will hear that the proposal would entail turning the existing road into a parkway, approximately 30 metres (100 feet) wide, with possibly reduced speed limits in some areas. Obviously this would encroach on private lands on either side of the existing road in some places, especially in the Village of Cumberland. *A few of many concerns that might be raised are:*

- Have detailed engineering studies been undertaken to determine the feasibility and cost of the proposed parkway? If not, how can the study have concluded that route #1 is the most favoured option?
 - To what extent would the proposed widening increase the throughput of traffic? What assumptions about future growth in traffic from Clarence/Rockland does this accommodate? If, when the proposed parkway is eventually built, it can’t accommodate the traffic from Clarence/Rockland, would the road need to be widened again?
 - With the increase in traffic entering the 174 from a widened north bound Trim Road plus an increase in throughput volume from the proposed widened of 174 east of Trim, how long will it be before that intersection is at capacity and becomes a bottleneck?
 - Can the study group provide a guarantee that any eventual widening of 174 would take the form of a parkway, and not a freeway? If speeding is already an issue on the two-lane road, how will a proposed reduction in speed on a new four lane be enforced without official ‘parkway’ status?
 - What is the total detailed financial cost of the proposed widening? How does that compare with the detailed financial cost of building corridors through options 2 or 3?
 - Has a hybrid model been considered where 174 is upgraded as a two lane (similar to work done this summer Trim to Cameron) to Rockland and a new two-lane is build as the Brian Coburn, Innes Rd/Boundary Rd option effectively creating four lanes desired but spreading out the traffic.
 - How many residents along the existing highway and in adjacent areas (including the Village of Cumberland) would be directly and significantly impacted? How does this compare to the other two corridors?
 - What will the impact be on the Ottawa River? Increased salt and contaminant runoff, increased noise, risk of accidents/oil spills/road kill? Why were biological considerations of this rural area only given a weighting of 5% in the environmental assessment?
 - How would pedestrians/cyclists cross the new four-lane road with a proposed concrete median wall? If there would be pedestrian tunnels, how far apart and at what cost?
 - How would vehicles coming from properties abutting the highway access the road? It seems that under the current proposal, due to a concrete median wall, vehicles on the north side would only be able to access westbound lanes, and residents on the south side would only be able to access eastbound lanes, and then would have to proceed to the nearest junction to turn around if they wanted to go the opposite way (like St. Joseph/Montreal Road between Jean D’Arc and the Rockcliffe Parkway). How far apart would the turnarounds be?
 - If the lanes in one direction were blocked due to an accident or other circumstance, would vehicles in those lanes be trapped? Would people living on that side of the road have no way to leave their houses or emergency vehicles access them?
- Your input into this process is very valuable. Please consider sending written feedback to the study team.

#7

From:
Sent: February 05, 2015 10:01 AM
To: Taylor, Angela
Subject: East LRT and OR174/CR17 Widening

Hello Angela:

I attended the PCG Meeting Feb. 2nd and the Open House event Feb. 4th on behalf of Ottawa Riverkeeper.
Please add me to the mailing list for this project, and especially, send me an invitation to all future PCG meetings, or similar events.
I would also like to receive a copy of the Evaluation Report, once it is made available to the public.

Thank you.

#8

From:
Sent: February 10, 2015 1:59 PM
To: Taylor, Angela
Subject: Hwy 174 widening

I trust that you will learn from history before choosing any alternate route for Hwy 174. The 417 was built instead of widening 174/17. Total disaster as few people use it compared to 174. Most heavy truck traffic use 174 because it is much shorter and a substantial saving on fuel.

An alternate route would be a waste as it would be longer and you already have two routes, namely regional road 26 and 28 that handle all the village traffic south of 174.

Drivers will still use 174 even if an alternate is provided as major services and shopping is provided along highway 174 which is extremely convenient and important when people return home from work.

I firmly believe that Hwy 174 will always be the preferred route as it is shorter and certainly more scenic on an otherwise boring drive to and from the city.

Yours Sincerely

#9

From:
Sent: February 10, 2015 1:59 PM
To: Taylor, Angela
Subject: Hwy 174 widening

I trust that you will learn from history before choosing any alternate route for Hwy 174. The 417 was built instead of widening 174/17. Total disaster as few people use it compared to 174. Most heavy truck traffic use 174 because it is much shorter and a substantial saving on fuel.

An alternate route would be a waste as it would be longer and you already have two routes, namely regional road 26 and 28 that handle all the village traffic south of 174.

Drivers will still use 174 even if an alternate is provided as major services and shopping is provided along highway 174 which is extremely convenient and important when people return home from work.

I firmly believe that Hwy 174 will always be the preferred route as it is shorter and certainly more scenic on an otherwise boring drive to and from the city.

Yours Sincerely

#10

From:
Sent: February 09, 2015 9:06 AM
To: Taylor, Angela
Subject: East LRT expansion

Hi Angela:
It was really nice meeting you at the public meeting last week at the Bob MacQuarrie Recreation Complex. The meeting and open house were very informative and well done.
Unfortunately, by the time I left, I couldn't find any more of the handouts. Is it possible to sending me six of them through the interdepartmental mail? I would like to share them with my Condo Board. If you are able, my mail code is .
Thanks.

#11

From: McKinney, Frank
Sent: February 09, 2015 4:25 PM
To: Taylor, Angela
Subject: ELRT Landowner

This fellow has property issues and wants to make sure he is notified for the next series of POH's.

Frank

#12

From:
Sent: Friday, February 06, 2015 1:31 PM
To: Blais, Stephen
Cc: 'bob.monnette@ottawa.ca'
Subject: OC Transpo

Councillor Blais

1. Can you find out why the Express buses from the east have been told to go to most westerly end of the Hurdman Station (Stop 2C) instead of stopping where there is room. If you are getting off at Hurdman, it is most likely you are going on southeast transit way and most of the buses stop at Stop 1D. Many times, I would have been able to make my connection right away if the bus at stopped at the easterly end of the platform; it is frustrating having to wait and specially now with the frigid temperatures.
2. Finally, I did attend the information meeting at Bob MacQuarrie and I find the preferred-route an excellent choice. My comments would be :
 - The extension to Trim is a priority and to reduce the cost of that extension, I suggest that 4 stations not be built but as suggested, the land be acquired or reserved: Orleans Blvd, Shenkman, La Cite, Tenth. Another reason for dropping these stations is the distance between stations is so small and the trains would stop and go, lengthening the travel time.

Merci

#13

From:
Sent: Friday, February 06, 2015 2:22 PM
To: Monette, Bob
Subject: RE: LRT/174 Widening Environnemental Assessment & 2015 Budget Consultation - Étude environnementale du train léger & l'élargissement de la 174

Bonjour M. Bob Monette,
Je regrette d'avoir raté la réunion du 4 février, 2015.
Est-ce possible d'avoir un compte rendu de cette réunion ?
Pour nous ce qui nous intéressais c'était les murs antibruit. Car ou nous déménageons notre condo fait face au sud qui est près de l'autoroute-condos de Brigil.
Merci.
Au plaisir de vous lire bientôt.

From:
Sent: February-06-15 7:56 PM
To: Monette, Bob
Subject: RE: LRT/174 Widening Environnemental Assessment & 2015 Budget Consultation - Étude environnementale du train léger & l'élargissement de la 174

M. Bob Monette

Merci pour votre courriel. Alors nous désirons vous demander de faire part à la coordonatrice qu'il est essentiel d'avoir des murs antibruit tout au long de la 174 côté nord et sud.
Merci de l'attention que vous porterez à cette requête.
Bien à vous.

#14

From:
Sent: February 09, 2015 11:19 AM
To: Taylor, Angela
Subject: Emailing: Extension Route No 174-2.pdf, Extension Route No 174.pdf

Mrs Taylor,
I will be forwarding my comments to the politicians of Prescott-Russel.
The lack of vision demonstrated by the City of Ottawa is very much disheartening.
I understand that to a certain extent your hands are tied by the process but someone has to take the lead. This issue is a very serious one. I do not plead for myself as I expect to work for only another ten years and I also work primarily in Rockland. I plead for the future residents of the east end of the City of Ottawa who are very much treated as second class citizens compared to their western counterparts. Like it or not Route No 174 is a gateway to the northern portion of eastern Ontario and as such the City of Ottawa has a responsibility to those tax payers of Prescott-Russell as well.
Politicians have a very short vision, 4 years typically. The monies to pay for this are coming from one and only pocket, therefore let's make proper use of our resources and not start over and over with studies and solutions that are too short term and are obsolete before even implemented. Often it is better to spend more money at the start and save a lot more over the long term.
The ball is in your court Mrs Taylor and let's do real PLANNING that will not crumble under the weight of its own inefficiencies.

Yours truly,

Ottawa

Prescott
Russell

#14

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**
Tuesday, February 3, 2015 **Wednesday, February 4, 2015** **Thursday, February 5, 2015**
R. J. Kennedy Mem. Comm. Centre Bob MacQuarrie Recreation Complex Guy Faubert Hall
1115 Dunning Road, Cumberland 1490 Youville Drive, Orléans 954 Giroux Street, Rockland

Tell us what you think...

- Your preferred option "Highway 174" looks very limited.
Expansion towards the east in 50-75 years from now.
- You people think only 20 years ahead of time - Not
enough weight given to option N2; but I can clearly
see that you put the weights that better suit.
You - Not unusual I have seen this time ahead time.
- Not room to expand rapid transit. East from there.
- Hwy 174 corridor was never protected by City of
Ottawa since it initiated Hwy 15. Look at all
new homes - New service roads will need to
be built. As well as lights, roundabouts all
slowing down the traffic. There will be no net
gain on a highway that is already over capacity.
- With option N2 buy a right of way 100-125
metres wide and you will be able to expand to four
lanes and rapid transit down the road and it will
act as a collector. Away from the river and where
expansion will occur. With this option you will
create an alternative route when either highway is
blocked. The city should be planning ahead and

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell

Mardi 3 février 2015

Centre comm. Comm. R. J. Kennedy
1115, chemin Dunning, Cumberland

Mercredi 4 février 2015

Complexe récréatif Bob MacQuarrie
1490, promenade Youville, Orléans

Jeudi 5 février 2015

Salle Guy Faubert
954, rue Giroux, Rockland

Dites-nous ce que vous pensez...

IN THIS SITUATION I BELIEVE THAT TAXPAYERS ARE
WAY AHEAD OF THE CITY
- AS A BUSINESS OWNER, I NEED GOOD ACCESS,
AND YOU PEOPLE ARE KILLING THE ECONOMY WITH
STORIES AND SOLUTIONS BEFORE THEY ARE EVEN IMPE-
MENTED. IS IT RIGHT THE FIRST TIME AND STOP WASTING
OUR TAX DOLLARS. INSTEAD OF CREATING MORE JOBS.
FOR BUREAUCRATS. YOUR REMARKS TOWARD THE
TAXPAYERS OF ROCKLAND WERE OUT OF PLACE AND
YOUR ANNOYANCE VERY MUCH PALPABLE MRS TAYLOR.
- OPTION NO. 1 WILL POORLY SHAPE THE FUTURE
DEVELOPMENT OF THE EASTERN PORTION OF THE
CITY OF OTTAWA AND OF PRESCOTT-RUSSELL.
- REGIONAL ROAD NO. 174 IS NO LONGER A HIGHWAY
AND IS NOW ONLY A GLORIFIED BOULEVARD. THAT
IS WHAT THE CITY HAS CREATED IN ITS SHORT-
PLANNING. 20 YEARS GO BY VERY VERY FAST MRS
TAYLOR. DON'T LOCK THE FUTURE GENERATION OF
EAST END RESIDENTS INTO HOURS OF UNPRODUCTIVE TIME
IN THEIR VEHICLES. CONTRARY TO WHAT PLANNERS
BELIEVE WE DO NOT WISH TO ALL LIVE IN CROWDS.

Veuillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 13 février 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:	
NOM: _____	COURRIEL : _____
ADRESS : _____	TÉL : _____

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement de la Ligne Trillium. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

From:

Sent: February 08, 2015 10:18 PM

To: Taylor, Angela

Subject: ELRT- Hwy 174 _5 Feb15 EA Csltn Feedback

Good Evening Ms Taylor,

Thank you for Subject presn in Rockland. Your presn colleagues did a good job explaining the criteria used and the details of the sensitivity analysis. Suggest it be done a bit earlier in the presentations or in the report to appease concerns raised by the public. You should have discussed ROM cost of each option, clarified they were ROM or NPV costs, and or at least the relative weight assigned to costs in evaluating options.

Should have at least discussed briefly the EA done last year or so for potential Bridge over Kettle, Lower Duck Island or from Gatineau Airport area and its impact on your study.

Agree with ELRT in median option and north at. Canotek Park as presented.

Plse seriously consider allocating/getting more Park n Ride spots from Mall(s) at place d'Orleans (now to deal with current and future ridership and to encourage critical mass of shoppers on St-Joseph Blvd versus Innes Rd); and push adding parking structure at Place d'Orleans with ELRT to achieve and maintain ridership.

Ref Hwy 174: Consider a Small Ring Rd around south of Rockland at base of E-W escarpment.

Suggest Options 3B (Brian Coburn) with 3A' (Innes to Baseline. But via Wilhaven East of Becketts Creek) and Cnty Rd 8 (Landry), 2 lanes, would be affordable sooner (shorter length to build new) and will do more to decongest traffic from East towards LRT.

Landowners on affected paths should be formally contacted, perhaps earlier in process.

If too late to return Hwy 174 and 17 to Province (as it seems), do work with other surrounding initiatives. Orleans to Wendover are still dormitory towns for Ottawa - Large offices or services employers are needed to reduce commuting distances to work.

Keep us informed please.

#16

From:
Sent: February 08, 2015 7:59 PM
To: Taylor, Angela
Cc: Monette, Bob
Subject: East LRT comments

Well done. We attended the 4 Feb 2015 EA info session: Informative, but crowded. Provided good exposure to ELRT alternatives, need a liitle more on benefits. ELRT may be intuitively beneficial, but as citizens it would still be helpful to understand what the ELRT will deliver over the existing bus service in terms of ridership costs, service reliability, safety, commute time, two-fleet challenges (buses and trains), etc...Is the LRT cost-beneficial? It would be reassuring to see some of the analysis.

Our preferences: West of Trim - Alternative 3, East of Trim (Unsure. We will leave it to residents of East Trim to voice)

Some questions that would assist in overcoming some concerns:

- 1) What is the latest start date for a 2023 completion of the ELRT (Place D'Orléans)?
- 2) What are the service performance metrics that the ELRT must meet to be a successful project? For instance, is the new ELRT expected to decrease commute/ travel time by 20%? Most residents know how long it takes them to "bus" from their homes to their places of work. Will the ELRT investment improve commute times? How will ridership fees evolve? Will training both bus and train drivers and maintaining two different fleets be causes of higher operating costs? What service performance benchmarks wil the City use to demonstrate value added to tax-paying citizens?
- 3) Ref. the estimated "municipal" price tag of \$3B (2015\$), what will be the resultant impact on "municipal" tax increases...and planning horizon? Will future municipal tax increases to fund the ELRT and other projects be gradual...or sharp? If it is unlikely that an annual rate of 1.75% (2015) can be sustained, citizens should be told in advance so that we can take these tax increases into account when planning our own family budgets. Indeed, some citizens believe in financial planning.

Thanks for listening, and hoping to see some of the above issues addressed in the City's communiqués,

#17

From:
Sent: February-08-15 4:45 PM
To: Monette, Bob
Subject: RE: CORRECTION: LRT/174 Widening Environmental Assessment & 2015 Budget Consultation - Étude environnementale du train léger & l'élargissement de la 174

Bob,

Thank-you for the email notification and for coordinating this open house. It was nice to see such a high turnout and its great to see the planned infrastructure modernization - even if its not all we want as fast as we'd like to have it!

I will be passing on my comments to the city but I thought you should be aware that my two primary pieces of feedback are:

- 1) Extend the Confederation Line out to Trim, and reduce the number of stations to help stay on budget. We don't need more stations than we currently have for the buses along that route. And if you don't extend it that far, then the Place d'Orleans park and ride will be completely overwhelmed.
- 2) Expand the 174 at the same time to 3 lanes in each direction, at least out to Jeanne d'Arc. The morning commute and evening return is unbearable. Yes, I know you're going to tell me to take the train but I work in Kanata so unless the city is going to accelerate the train line buildout all the way to March Road, then the train isn't for me for my work commute. It will, however, be an option if going downtown for pleasure.

As my councilor, I trust you will be representing and supporting positive recommendations for the buildout of badly needed infrastructure in the east end.

As a side note, we desperately need to attract more business out to the east end and move away from being just a bedroom community. I wish there was a high tech presence in the east end...would certainly help me out!

#18

From:
Sent: February 08, 2015 1:19 PM
To: Taylor, Angela
Cc: MClermont@prescott-russell.on.ca
Subject: Confederation Line East LTR Extension / Road 17 Widening Feb 8, 2015

Angela,

I am submitting my comments to the meeting held last Thursday.

We appreciate you and your team's dedication to the project.

One question for Mr. Clement.

We know that the City of Ottawa can raise the monies to complete the Highway 174 Widening Project.

But the Prescott & Russell do not have this advantage. Our population base cannot support the borrowing of large amounts of money.

Now, the City of Ottawa decides to complete the widening up to Cannan Road - Ottawa Limits in year 2031.

Can Prescott & Russell be able to spend up to 30-50 million to complete their section? These could be the 2031 revised costs? Or, are my estimates too high??

Are the projected 2031 \$ costs included in the # 1 Model?

Also, is the information on the study available on the Ottawa City/Prescott websites?

Feedback would be appreciated.

#18

February 8, 2015

Angela Taylor, Ing

City of Ottawa.

I was able to attend the meeting at the Guy Faubert Hall on Thurday, February 5, 2015.

First of all, I would like to congratulate your team on the presentation of your in-dept work on the Confederation Line East LTR ExTension/Highway 174 Widening Project.

It must have been a let down for your team in the choice of hall for the update on the project. It did not help with the presentation as everyone was focused on the noise from the adjacent room. Hopefully, the Spring meeting will be held in a better environment.

The three choices offered, have their positive and negative merits. It was actually had to believe that the Model (1) was chosen to enhanced the existing road from Trim to Canaan and down to Rockland.

And you did stipulate that this model would be more expensive to realize.

The original cost was estimated at \$ 110 million five years ago. The Provincial and Federal governments were providing \$ 40 million each towards the project. Now the project must be approaching the \$ 150 million plus. This means that The City of Ottawa and that City of Rockland would have to put in \$ 70 million or more at today's rate. If we have to consider the City of Ottawa will only be addressing the project in year 2031 which is 16 years down the road. By then, we would probably looking at a cost of over \$ 300-400 million. Has your model looked at cost of the project at the 2031 level?

Now, the Second and Third choice of Models came in at less expensive then Modele 1. But in the long run, they may be cheaper. If the population of Ottawa is rising at the annual rate of 1-2% , then we have a growth of between 10-20 thousands people. If we select the middle of 15,000 a year for let's say 50 years down the road, which is an increase of 750,000 new residents. By 2045, Ottawa will have a population of 1,500,000 residents. I believethe major growth of home construction will be in the Orleans area. The Orleans residents will take advantage of the transit network to work in the Ottawa Center. People will be buiding their homes in the South-East area of Orleans towards Canaan Road. This will demand more roads be built along the Basline Corridor. Taking this in consideration, would it make more sense to chose Model 2 or 3 now and be ready for this influx of growth?

We will be spending \$ 300-400 million on the widening of the 174 Highway after 2031. We will then have to spend another 500-700 million to acquire land and build roads on the Baseline Corridor.

Do you think that the City of Ottawa will be in measure to spend 0.8-1.1 billion down the road on these two projects? Would it not be a better choice to select Model 2 or 3 now, this would be more cost effective.

The City of Ottawa councillors in 2031 will be voting on the Cost of the Project including the Environmental Assessment Study submitted. Like today, they have to control costs to keep the propery taxes on the low side - not to alienate property owners. Which model presented will be the most cost effective and look after population growth? Model 1 selected in 2015 wlll now become number 3. (Let's not kid ourselves – controlling costs is always number 1).

I realize that major decisons will have to made in the future on how to deal with transportation of people in and out of the City Of Ottawa. We will have to be very innovative. Other provinces and probably countries around the world will be looking at the City of Ottawa for guidance on how to deal with their tranportation needs. We have to get it right.

#19

From:
Sent: February 08, 2015 10:27 AM
To: Taylor, Angela
Subject: Hwy 174 / CR 17 Widening

Hi Angela,

I'm a resident of Cumberland (McTeer Road) living within the boundaries of the EA and I missed the open houses for the widening of Hwy 174 and CR 17 last week. Is there any way you can provide me with the information presented on the following? Please note that I couldn't find anything online except the SoW and some updates to the timeline but I'll happily take a link to any online documentation that exists

- A general project update including integration of LRT in the OR174 corridor from Blair Station to Trim Road
- The development and evaluation of the integrated alignment alternatives for the LRT and OR174 widening including the preferred plan West of Trim Road
- The development and evaluation of alternative road corridors from Trim Road to Landry Road, including the preferred road corridor

Thanks in advance.



4 FEB

Prescott
Russell

#20

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
 Bob MacQuarrie Recreation Complex
 1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...

The timeframes of 2023 for LRT & 2031 for hwy are beyond the horizon of benefit for most residents now over 60 and 45/50 respectively.

① Somehow you have to stimulate interest and participation by those age cohorts.
I reckon the mean age in tonight's audience at about 60.

② The key to overall success will be the adequacy of the feeder bus system. A steady moderate flow of delivery to LRT stations requires comparatively frequent runs at less-than-articulated bus-full rate.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **February 13, 2015** to:
Angela Taylor, City of Ottawa, 110 Laurier Ave. W. Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL :

ADDRESS :

PHONE :

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1Y1

From:
Sent: February 05, 2015 3:43 PM
To: Taylor, Angela
Cc: Valerie McGirr (Valerie.McGirr@aecom.com); Anda Bruinsma ; Barry Turner; Clive Horne; Kevin Poll; Linda Brousseau; Mike Bray; Scott Eaton ; Euan Swan
Subject: OR 174 / CR 17 Widening - Questions - CCA 174 WG
Importance: High

Dear Angela,

As a follow-up to the February 4 Open House concerning the 174/17 widening east of Trim, the 174 Working Group of the Cumberland Community Association requests answers to the attached questions concerning the options under consideration.

I look forward to hearing back from and wish your team well in its presentation tonight in Rockland.

Regards,

**Cumberland Community Association 174 Working Group
Questions Concerning the Widening of Ottawa Road 174 East of Trim Road as Presented at the
OR 174/County Road 17 Widening Open House – February 4, 2015**

- 1. Feasibility and Cost Preferred Option:** Have detailed engineering studies been undertaken to determine the feasibility and cost of the proposed parkway? If not, how can the study have concluded that route #1 is the most favoured option?
- 2. Traffic Throughput:** To what extent would the proposed widening increase the throughput of traffic? What assumptions about future growth in traffic from Clarence/Rockland does this accommodate? If, when the proposed parkway is eventually built, it can’t accommodate the traffic from Clarence/Rockland, would the road need to be widened again?
- 3. Parkway versus Freeway:** Can the study group provide a guarantee that any eventual widening of 174 would take the form of a parkway, and not a freeway? If speeding is already an issue on the two-lane road, how will a proposed reduction in speed on a new four lane be enforced without official ‘parkway’ status?
- 4. Cost:** What is the total detailed financial cost of the proposed widening? How does that compare with the detailed financial cost of building corridors through options 2 or 3?
- 5. Hybrid Model:** Has a hybrid model been considered where 174 is upgraded as a two lane (similar to work done this summer Trim to Cameron) to Rockland and a new two-lane is built as the Brian Coburn, Innes Rd/Boundary Rd option effectively creating four lanes desired but spreading out the traffic.
- 6. Impact on Residents:** How many residents along the existing highway and in adjacent areas (including the Village of Cumberland) would be directly and significantly impacted? How does this compare to the other two corridors?
- 7. Impact on the Ottawa River:** What will the impact be on the Ottawa River? Increased salt and contaminant runoff, increased noise, risk of accidents/oil spills/road kill? Why were biological considerations of this rural area only given a weighting of 5% in the environmental assessment?
- 8. Road Crossing:** How would pedestrians/cyclists cross the new four-lane road with a proposed concrete median wall? If there would be pedestrian tunnels, how far apart and at what cost?
- 9. Turning Points:** How would vehicles coming from properties abutting the highway access the road? It seems that under the current proposal, due to a concrete median wall, vehicles on the north side would only be able to access westbound lanes, and residents on the south side would only be able to access eastbound lanes, and then would have to proceed to the nearest junction to turn around if they wanted to go the opposite way (like St. Joseph/Montreal Road between Jean D’Arc and the Cartier, formerly Rockcliffe, Parkway). How far apart would the turnarounds be?
- 10. Blocked Lanes:** If the lanes in one direction were blocked due to an accident or other circumstance, would vehicles in those lanes be trapped? Would people living on that side of the road have no way to leave their houses or emergency vehicles access them?

#22

From:
Sent: February 04, 2015 6:30 PM
To: Taylor, Angela
Subject: Questions Related to Option 1 - 174 Widening

Hi Angela

I know you have a busy few days ahead but would appreciate if you could add the following questions to the list of items that need clarification and response. As well would you please send me a mock up similar to what you did for the village core to show the property lines and where the planned road will go and planned intersections/roundabouts from Cameron to Trim. As you know there are many residents along this strip who live between the escarpment and the river, close to the road, as well the houses on the top of the escarpment will be affected. We should all have access to the planned road orientation and its potential impact on our homes.

1. As I asked at the meeting last night, and this might be in your data document which I hope we will be able to get a copy of; when the road is finally built (10-15 years from now) based on the current projections of increased traffic from outside the city limits travelling west into and out of Ottawa via 174 (since that currently is the only option and that is why people want to access it), how close will it be to capacity? If the projections show that a widened 174 will be close to or over capacity at that time, how can this even possibly be the right solution to lock into a 15 year planning window? It would mean that at the moment that the road was completed the projected over capacity would result in it being exactly as it is now. This is a plan for failure. Please explain.
2. Why was a hybrid option of 1 & 3 not considered where bringing the 174 from east of Cameron to Rockland up to the same standard as west of Cameron to Trim was done this summer and a rural 2 lane for Option 3/3A? This would allow for constuction to be done in stages with the least amount of disruption to traffic. It achieves the objective of 2 additional lanes, allows traffic to naturally select a preference resulting in less traffic on each road, it would reduce the volume on the 174 as people on the southern link would chose the Brian Coburn/Blackburn Hamlet Bypass or people would access public transporation at the Millenium Park Hub. As well the hybrid option allows for an alternative route in case of accidents or disruptions on either road. If my assumption in question #1 is correct then by the time you build the four lane 174 it will already be over capacity. In the hybrid model, you will still have the opportunity to double both options adding an additional 4 lanes in the future and with more visibility on where the development is and what would be required. Obtaining land now for future use is a defered savings which can easily be calculated based on expected land prices in the future.
3. What is the exact number of actual working farms and number of large property hobby farms that would be affected by the Innes/Boundary Road Option. How many would be seriously affected by a two lane road dividing their fields (for which they would be compensated). If it is the city's intent to protect farm land from future development could you explain to us how this will be accomplished and can you guarentee that this Grade A farmland will be protected and never be allowed to be sold to developers. (As far as we have heard, nearly all of the current 'working' farmland south of Cumberland has been committed to developers on rights of first refusal.)
4. How many properities would be directly affected by a 2 lane version of Option 3? By this I mean will the road run in their front or back yards as will be the case of Option 1?

5. How many properties are directly affected by the widened 174? How many properies would require some expropriation.
6. What it the total length of the proposed road from Sarsfield Road to Boundary Road.
7. What is the total length of the road in Option 1 from Landry to Trim?
8. What is the total length of the proposed roads from Trim to Boundary in each of Options 2 & 3.
9. If the EA can mitigate for the miriad of environmental, wildlife and biological issues that are of grave concern along the entire length of the proposed four lane road where it is in very close proximity to the Ottawa River, why is the challenge of crossing Becketts Creek so monumental in terms of potential environmental impact? The roads south and east of Cumberland crosss many creeks and their tributaries. Please explain.
10. I have friends who live on Wilhaven and have been told that there is an existing road right of way behind their house. Is this right of way part of Option 2? If so does a right of way not entitle the city to use it?
11. There is a general concern amoung many residents that this process was not without an expected outcome. From the beginning you and the study team have done your best to be unbiased. However, last night I heard that widening the 174 has been in the City's transportation masterplan for a long time. This statement completely undermined the legitimacy of your process. As well, we have repeatedly heard and this has been reinforced through our city councillors communications that the 174 was going to be widened. This would imply that he knew the outcome of all your work prior to its conclusion. All this to say that when we planned to buy our property in 1997 we went to the then Cumberland City Hall and inquired about future plans for the 174. We were absolutely assured that they had no plans to widen the 174. In 2002, when we wanted to begin the construction of our house we approached the newly amalgamated city transportation department with the same question and received an absolute and unequivocal response that this was not anywhere in the city's long term plans. In 2007, when the province offered up their 40M to Rockland so they could get their road widened to Canaan, the Major of Ottawa made it clear that expanding the 174 was not something the city was going to consider, not on their list of priorities and did not align with the commitment to public transporation. So I would like to know: How did the widening of 174 get into the transportation master plan when it was flatly denied by our highest elected official in 2007? How has this masterplan bias was removed from your entire process of finding and evaluating the options to determine the best solution.
12. Without a detailed financial work up on the 3 options, how can the study team make an unequivocal recommendation. Is the cost of the solution not an overweighted factor in this decision?
13. Does the study team truly believe that Option 1 is the best long term solution (meaning implementation in 15 years so effectively 20-25 years from now) to manage the anticipated increase in traffic from east of the city boundary. Will the expanded 6 lane 174 have enough capacity with the traffic accessing at Trim to accomodate this volume.

I am sure that there are many more questions that will come your way. We hope to continue to work with your team and trust there is still some opportunity to influence your decision and modify the proposed recommendation.

Kind regards

#23

From:
Sent: February 05, 2015 4:44 PM
To: Taylor, Angela
Subject: Re: Questions Related to Option 1 - 174 Widening

Hi Angela;

I would like to add one more comment to [this](#) list; however I feel it necessary to establish my bona fides to set the context of my questions.

I am an engineer who has spent more than 40 years in the Space Industry and have held executive positions for the last 20 years of my career. For the last 12 years of my career I was Chief Engineer for Asia Satellite in Hong Kong. I have procured multiple satellites, ground systems and built a 5000 sq. meter building in Hong Kong that have required me to manage procurement programs in excess of \$US 500 Million. I believe I can confidently say that I know a bit about comparing procurement alternatives.

Purchasing satellites is not unlike your process. The buyer sets down requirements and suppliers propose alternatives. Technical issues are evaluated separately from contractual issues and price. Technical issues are evaluated by teams of Engineers using pre-agreed weightings to score proposals. The scoring is a subjective judgement on the part of individual engineers. Although objectivity the goal, true objectivity is a challenge to achieve.

In this system we would consider technical scoring between two suppliers that was within 10% to be essentially equal. That is, within a 10% subjective evaluation, suppliers would be equally ranked and the determinant would be contractual terms, schedule and price. (The assumption here is that minor differences can be negotiated to an acceptable solution, a process I believe you refer to as mitigation)

Options one and three differ by about 10%. (My memory may be a bit flawed but I believe the scoring was 1.83 to 1.66) Your evaluation is highly subjective and I would consider these evaluations as essentially equal. What exactly would your scores have to differ by before being considered essentially equal?

You have made a selection without consideration of price or schedule. I can assure you that if I went into a Board of Directors meeting and told them I had made a selection based solely on technical issues and disregarding cost, that would have been my last day on the job. I believe we can both agree that building along the existing route of the 174 with the necessity of blasting into the escarpment, complex drainage issues, HV power lines etc will be more expensive than a route across open farmland. Can you advise me what your team considers the relative cost of options one and three to be?

It seems to me that a hybrid solution of two lane versions of proposals one and three would be optimum. The value of this solution is redundancy. There are two few options from the east end to the proposed transit system. I have observed the 174 closed at lest three times a year while accidents are cleaned up. Besides satellite engineers, like myself, love redundancy.

Lastly, I am shocked that such a low score was attached to the environment. A four lane highway will have increased salt, POL (petroleum, oils and lubricants) runoff, and the very real potential for accidents that spill contaminants into the river. I would like more data on how you evaluated environmental impact.

Regards;

#24

From:
Sent: February 05, 2015 3:03 PM
To: Taylor, Angela
Subject: Open House Slides

Good Afternoon,

My name is [David](#) and I help run a local news blog called Ottawa This Week. I was hoping to make it out to either of the three open houses on the east LRT extension/highway widening. Unfortunately this is all on my own time and I can't make it to any. If it is possible to send my the slides from these presentations I would really appreciate it. Thank you.

#25

From:
Sent: February 04, 2015 11:48 AM
To: Taylor, Angela
Subject: Ottawa Road 174/Prescott-Russell County Road 17 Widening

Feedback on Open House Feb 3, 2015

Our feedback on the selection of option 1 - widening the existing 174 from two lanes to four, with sidewalks and cement median between the vehicular lanes:

The impact on the Ottawa River has not been addressed to our satisfaction. The material presented indicated a given weighting of only 5% which seems minimal for the scope of this project. There was no conversation around this at the Open House. The river is our greatest resource. Do you have the endorsement of the Rideau Valley Conservation Authority and the Ottawa River keeper for this option? During the Open House a resident asked for successful examples of four lane roadways build along major river ways and gave examples of other municipalities' decisions to direct roadways away from river ways. The study team indicated they would provide this information and we would like to be a recipient.

The pedestrian options were not covered in depth other than to indicate there would be sidewalks alongside the 174. How would people living on the north side of the roadway have reasonable pedestrian access to the village? In our situation, we need access to the village to get our mail, as Canada Post will not deliver to our address and requires us to maintain a post office box at the Post Office located in the village.

At the first Open House where this project was first introduced, one of the main reasons given for a new solution was public safety. The proposed option opens all kinds of questions on how emergency vehicles respond to the addresses on the north of the 174, but the big question in our minds is that since funding doesn't permit any work to commence until after 2031 how can you say this solution possibly addresses the importance of safety. Many accidents can happen while we maintain the status quo for 16 years.

Also at the first Open House it was indicated that all possible options would be looked at before presenting a preferred solution. It left us with the impression the study team would be looking at this problem with fresh eyes and no preconceptions. During this past Open House two comments were made by one of your team members which leads us to believe this was not the case. They were "you always look to build upon existing roadway" and "people want to get to the 174 at Trim". In our many years of experience with project management we were taught to look at the root causes for behavior. The 174 was built some time ago using technology, engineering and environmental understanding in existence at the time. We now better understand our impact on the environment and have many more transportation options available to us. People want to "get to Trim" because that is what exists now, not because it is optimal. It is very disappointing you have taken such a narrow scope.

The proposed option will also not address the concerns of those that use the 174 as a thoroughfare, those being commuters that go into the city from east of Cumberland and truckers that use the route to

link to Quebec. Our fear is they will become frustrated and resort to behaviors that will be unsafe as they are driving through the village, which is what happens at present, repeatedly day after day, or choose alternate routes that are not equipped, just transferring the issues that Cumberland is dealing with now to other neighbourhoods.

We would also like to give feedback on the mechanics of the meeting itself.

This is an important issue to Cumberland residents. It involves their homes and their neighbourhood, which is the essence of their lives. You know this. It is entirely expected that people become emotional and upset and have difficulty accepting what is actually being presented to them. You need to have facilitators available that are not involved in presenting your work that can control the crowd and ensure that all people are heard. There were many residents that were speaking and I wanted to hear what they were saying but it was impossible. A process needs to be laid out at the start of the meeting where residents feel confident they can speak and be heard in an orderly fashion. Councilor Blais attempted to do this but it was not enough. People were leaving in frustration. There were also very many negative comments about Mayor Watson coming to shake hands but not sticking around for the "dirty stuff". Perhaps even a five minute preamble from him before the meeting could have helped you be more successful in presenting your information.

#26

From:
Sent: February 05, 2015 8:30 AM
To: Taylor, Angela
Subject: Bike Paths/ County Road 174 Widening

Ms. Taylor,

Good morning. I am currently on the Cumberland Community Association Board as the Trails Co-ordinator. I attended the public meeting on the expansion of transit and roads to Rockland last night and was most impressed with the plan for the widening of the 174. I talked to the Rep from Parsons about the ideas for expansion of bike trails along this route. She suggested that I stay involved throughout the next steps of planning the project and that I should contact you to stay informed. I would be happy to provide assistance or just stay informed of the developments as required.

Best Regards,

From:
Date: Wed, Feb 4, 2015 at 9:07 AM
Subject: Ottawa Road 174 Widening
To: angela.taylor@ottawa.ca

Dear Ms. Taylor,

Thank you for your presentation last night at the R.J. Kennedy Mem. Comm. Centre.

I was pleased that you offered to distribute a copy of your **full Evaluation Report**. If it is electronic, please send it to ; if a hard copy, please send it to the address below.

In particular, I am interested in reviewing the full report to understand more fully how you arrived at the 188 points which you calculated for Alternative 1, as compared to the 166 points for Alternative 3A, as a basis for declaring widening the 174 as the preferred option.

I would also be interested in receiving a copy of the **budgetary estimate** which you used to quantify the cost to taxpayers of the four options.

Many thanks in advance.

#27

From:
Sent: February 04, 2015 8:21 AM
To: Taylor, Angela
Subject: Feedback to EA study Town Hall

Dear Ms. Taylor,

I have several questions and concerns regarding the environmental assessment that was presented last night in Cumberland. In order to provide you with the most constructive feedback, please send me the full report as well as supporting data so that I may determine for myself that you have addressed these concerns. It would also be helpful if you would provide me with links to the current transportation master plan and the budget that was used to determine what could and could not be considered in the current timeframe.

Thank you in advance.

From:
Sent: January 29, 2015 6:43 PM
To: Taylor, Angela
Subject: East LRT to run along centre of 174

Hello,
Dates for Open Houses do not work for me?
My comment is that running the LRT in the Median may create a big distraction for those driving?
As it is now, it does not take much of a distraction to have a Major Impact!
Thank you

#28

From:
Sent: February 10, 2015 10:27 AM
To: Taylor, Angela; david.hopper@parsons.com; Monette, Bob; Blais, Stephen
Subject:

Good morning,

I am enclosing my comments regarding the Public Consultation last week.

Should you have any questions regarding the attached, please feel free to contact me.

Enjoy your day,

#29

#30

#31

From:
Sent: February 11, 2015 3:23 PM
To: Taylor, Angela
Subject: Hwy 174 widening

I attended the meeting on February 3, 2015 in Cumberland concerning the LRT and Hwy 174 widening.

Needless to say, like everyone else, I was very unhappy about the outcome.

The results according to your study were done mathematically with results being 1.88 points for alternative 1 and 1.66 for alternative 3A.
Some reasons given were that alternative 3A would bring developers to buy up prime farm land. Everyone knows that if developers want to buy land and the farmer wants to sell it, it will happen with or without alternative 3A. As for prime land, lots of farm land along Innes Road is rocky terrain and, according to your diagram, alternative 3A would only have two lanes. You wouldn't be needing that much land. The only farm land you would need is from Sarsfield Rd to Canaan Rd. where there is no road at all.

Still you are telling the people whose land runs along Hwy 174 that you will be taking part of our land. (Some of us don't have much to begin with.) To you it doesn't matter if we have apple trees, raspberry plants, blueberries etc. in the area you plan to take. Maybe our septic systems will have to be changed to accommodate this Hwy widening. The new septic systems are now installed above ground limiting the use of our yards. You are disturbing more people widening the 174 than you would be if you went with alternative 3A. On top of everything else I was told that you will not be putting sound barriers and that you are going to have sidewalks along the Hwy (this is ridiculous). With the little difference in point between the 2 alternatives, you could have overlooked the point and went with compassion.

It makes me mad that our input at your previous meetings were not put into consideration. You just went ahead and did what you had planned all along. You shouldn't have bothered us with those meetings. You were just giving us false hope. We really didn't have a say.

#32

From:
Sent: February-12-15 8:04 PM
To: Monette, Bob; MClermont@prescott-russell.on.ca; Taylor, Angela
Subject: Re: East LRT and OR174/CR17 Widening project

Hi Bob,
What has to be done to engage Rockland in this discussion? I worry as someone who lives less than a kilometre from the highway that Orleans will be the next Etobicoke and Rockland the next Mississauga. I don't see this as an option in the documents that I have read so far.

Thanks,

From:
Sent: February-11-15 8:22 PM
To: MClermont@prescott-russell.on.ca; Taylor, Angela
Cc: Monette, Bob
Subject: East LRT and OR174/CR17 Widening project

Hello Angela and Marc,
I am the former head of the Orleans Riverwalk Community Association (Tenth Line and Jeanne d’Arc). For the first 19 years of my life, I resided in Mississauga. I have concerns about the East LRT and OR174/CR17 Widening project.

- 1) Instead of widening the road, there is no study of expanding BRT or ELRT to Rockland. Why was this option excluded?
- 2) The first 19 years of my life were spent living in Mississauga and watching Mississauga expand. I was there to see the QEW and 401 widen. Due to induced demand, widening road only increases congestion. Road widening allows people to purchase property further away from jobs. I can assure you that no matter how wide the 401 will be in Toronto, the traffic there will always be horrible. This project seems to replicate that for Ottawa. If we want to avoid being like Toronto, shouldn’t we avoid Toronto’s mistakes? I would prefer to see us build LRT or BRT to Rockland first (we can do it, the 174 and 17 used to be the Canadian Northern Railway);
- 3) With median stations, does this mean that buses will stop in the middle of the bridges to pick up passengers from ELRT? Passenger comfort must be taken into consideration given the cold winters we have in Ottawa.
- 4) How many bridges will require replacement regardless of which option is chosen?

Thanks,

Induced Demand:
What's Up With That: Building Bigger Roads Actually Makes Traffic Worse | WIRED<<http://www.wired.com/2014/06/wuwt-traffic-induced-demand/>>

<<http://www.wired.com/2014/06/wuwt-traffic-induced-demand/>>

What's Up With That: Building Bigger Roads Actually Ma...<<http://www.wired.com/2014/06/wuwt-traffic-induced-demand/>>

The concept is called induced demand, which is economist-speak for when increasing the supply of something (like roads) makes people want that thing even more. T...

View on www.wired.com<<http://www.wired.com/2014/06/wuwt-traffic-induced-demand/>>

From:
Sent: February 12, 2015 3:29 PM
To: Taylor, Angela
Subject: Confed. East / 174 Widening Open House - Comments

Hi, Angela;

It was nice to get a chance to chat with you on February 3 and 4 at the open houses. As you might have guessed, I came away from the presentations with some thoughts - mostly on the transit side, I'm afraid. (I can always be counted on for having opinions about transit, it seems.) Well, I put those ideas into a document and attached it to this e-mail.

Thanks for getting the open house panels posted to the Web so quickly. Usually I get my comments in before that happens. I did write most of the document before they were posted, however, so if I am off about some of the details, please forgive my memory.

I will be reviewing the on-line information and if I come up with any further thoughts I will send them in to you.

QUICK LINKS

North, Median, South, or Hybrid2

 Southern Alignment.....2

 Northern Alignment.....2

 Median Alignment.....2

 Hybrid Alignment.....2

Blair Station Area.....3

 Lessening Bus Congestion.....3

 Blair Road Underpass.....4

 Running Corridor East of Blair4

Montreal Station Area4

 Topography4

 Alternative Options6

 Median6

 Underground.....7

 North Side9

 South Side9

Green’s Creek Crossing.....9

Sir George Etienne Cartier Parkway Crossing.....9

Greenbelt Crossing10

Jeanne d’Arc Station Area10

Orleans Station Area.....11

Place D'Orléans Station11

Orleans Town Centre Station.....13

Tenth Line Station.....13

Taylor Creek Station.....13

Trim Station13

Conclusions.....14

Summary.....16

Figure 1: Bus stops at Blair along the OR1743

Figure 2: Elevation reading along the 174 at Montreal Road.....5

Figure 3: 174 over Montreal Road Profile.....5

Figure 4: Narrow Median Station.....6

Figure 5: Montreal Road Underpass to Median8

Figure 6: Rerouting Traffic at Place D'Orléans.....11

Figure 7: Place D'Orléans Southern Alignment.....12

Figure 8: 174 / Trim Road Interchange with Median Alignment.....14

Figure 9: Elevated Jasmine Station15

#33

I have been thinking about the eastern extension of the Confederation Line. Based mainly on my memory of the open house panels (this was mostly written before they were posted on the Web), here are my comments/suggestions:

North, Median, South, or Hybrid

Southern Alignment

Generally, the south side alignment can be ruled out due to the over-head high-voltage power lines. Also, there is no development, or even possible future development, along the south side of the 174 until it enters Orleans; so all customers going to, or coming from, Montreal Station would need to cross under/over the 174. This means that for roughly six of the eight and a half kilometres of the affordable line the tracks would be on the wrong side of the 174.

However, a south-side alignment has already been ‘roughed-in’ for a Transitway with a partial cavern under the current Place D'Orléans bus station and underpasses at old and new Tenth Line Roads. Also, the well used Trim Road Park & Ride is on the south side of the 174. When the interchange at Trim Road and the 174 is implemented, it is likely that the east-bound 174 on-ramp from St. Joseph Boulevard will be removed, potentially leaving a clear corridor from Tenth to Trim, on the south side of the electrical wires. The south side is a bit of a mess at Place D'Orléans for roads, though and the tracks would need a lengthy tunnel in this area.

Northern Alignment

The North side alignment generally has appeal because it can be built without disturbing the existing roadway and is furthest from the electrical wires. The corridor does get a bit more constricted just east of the Sir George Etienne Cartier Parkway, but there should still be room for a compact running configuration. Each interchange crossed would require multiple overpasses/underpasses and would likely cause severe road disruption during construction. New underpasses/overpasses would also be required at the Parkway and Orleans Boulevard. Approximately two and a half kilometres of the line, through the Greenbelt, could not have any development.

For the area between Blair Station and the future Montreal Station, a northern alignment is easily accessed through the existing Transitway underpass at Blair Road. Keeping the train along the north through this section would allow for a future station near Jasmine Park to be added to serve the residential area to the north.

Once into Orleans, a northern alignment would serve residential communities and the Place D'Orléans Park & Ride. It is likely that the track placement at the P&R would require the redesign and replacement of the pedestrian overpass. The Place D'Orléans bus station and shopping mall are on the opposite side of the 174.

Median Alignment

Running the trains in the median has the potential to require new roadwork along the entire corridor as the lanes of the 174 might need to be moved outward. The extra road width would not be much, but multiplied by the length of the corridor might amount to more work and expense than staying on one side. At some interchanges, however, there might be a saving because the ramps are no longer ‘in the way’ and, thus, do not need to be crossed. Where the 174 passes under cross-roads, it will need to be determined if the span can be widened sufficiently (usually through modification of the abutments) to allow current and future traffic lanes and the tracks to pass. If there is insufficient room, then the overpass will need to be replaced with a longer one; a very disruptive process for traffic.

Hybrid Alignment

Combining various sections of the north and median alignments has been chosen as the preferred alignment. Specifically, the tracks would stay north of the 174 between Blair Station and Montreal Station and then move into the median for the rest of the corridor to Trim Road.

Depending on the requirement to replace overpasses in the area, it might be worth a second look at moving from the median to the south side at Place D'Orléans; thus creating a north-middle-south hybrid alignment.

Blair Station Area

Lessening Bus Congestion

Since Blair Station is going to be very busy with all of the buses coming and going, it might be worth while to split the bus load between Blair and St. Laurent stations. This can be done by introducing temporary bus stops along the 174 at the Blair Pedestrian overpass and having the buses continue to St. Laurent Station. This solution would reduce the delay to buses as they would no longer need to enter the Blair Station’s bus loop.

West-bound passengers could transfer to the west-bound Confederation Line by crossing the existing overpass over the tracks. East-bound passengers might transfer from the train at St. Laurent Station to an empty bus, or they can stay on the train as long as possible and cross the 174 via the pedestrian overpass.

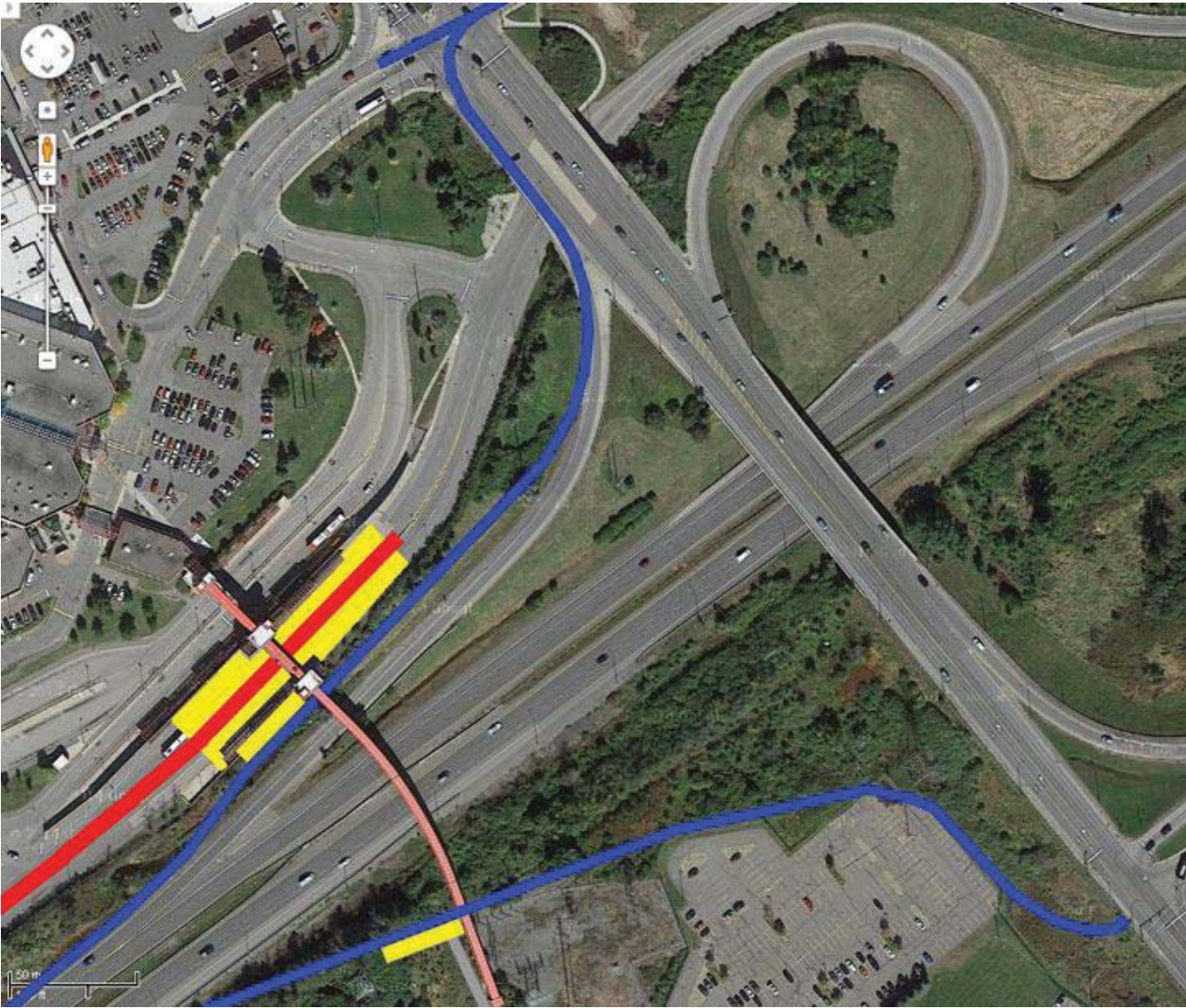


Figure 1: Bus stops at Blair along the OR174

Moving buses onto new 174 ramps will free up the Transitway underpass of Blair for conversion to rail. It might be necessary to lengthen the bus lanes on the 174 to better connect with the 174 ramps east of Blair.

Blair Road Underpass

From the Blair Station, the LRT should head east along the current Transitway alignment. The underpass of Blair Road is tentatively ear-marked for use by the Cumberland Transitway, but since that BRT is not scheduled to be constructed until after 2031 (i.e., it is not in the Affordable Plan), it should not have the priority. The rail line will be costly and any options that can reduce its price should be chosen – within practical limits, of course. If and when the Cumberland Transitway is to finally be constructed, then the extra grade separation can be added for the LRT; and constructed while the LRT continues to use the existing underpass. After the LRT has moved to the new underpasses, then the buses can have the current alignment back.

Running Corridor East of Blair

Departing Blair Station on the north side of the 174 allows the LRT tracks to be built without the need to modify the roadway. Buses would be moved onto the 174 and use the existing Blair Road on-/off-ramps and the new tracks would use the existing Transitway ramp space. There might be some minor disruption to drivers on the 174 as the existing Transitway’s fly-over is removed. It doesn’t look as if that fly-over to the south side of the 174 is wide enough for double tracks. I don’t know if the heavy-lift ‘creepers’ could be used to quickly move the structure to the south of the 174 for future recycling, but that could be looked into.

There is a corridor of approximately 23 metres wide that can be used for the tracks along the north side of the 174. This is plenty of room for tracks and a station, if another is added in the future. Access to any future station would only be from the north.

Montreal Station Area

Topography

As shown in the preferred (Hybrid) plan, the next planned station east of Blair would be at the 174 interchange with Montreal Road. Since the tracks are on the north side of the 174, then the station will also be on the north side. The plan is to begin raising the tracks on an earthen ramp or structure to take the train over the east-bound to west-bound on-ramp, then through an elevated, central platform, station situated between the on-ramp and Montreal Road, continue over Montreal Road and the west-bound to west-bound on-ramp, over the west-bound to west-/east-bound off-ramp, and then have the tracks swing over the west-bound lanes of the 174 and descend to match the road grade in the median of the 174. The merger would happen west of the Green’s Creek crossing. The description tells of how the merger would make use of the rising grade to have the road come up to meet the tracks.

I have looked carefully at the topographical map provided by the City at geoOttawa (<http://maps.ottawa.ca/geoOttawa/>) and I can not find the rising of the grade until after Green’s Creek. (Figure 2, below) If the tracks are to match the road’s level before the creek, then the tracks will need to do so on a down-hill slope. The road’s slope is pretty consistent, dropping from 64 metres above sea level (ASL) over Montreal Road to 50 metres ASL, 600 metres to the east; a grade of about -2.33%.

The road grade does not begin to rise until after it has crossed Green’s Creek. Even then, the 174 rises very slowly with the Sir George Etienne Cartier Parkway easily passing over it. The idea of using the rise of the road to meet the rail is not practical since the road hasn’t even gotten up to 56 metres ASL until it has crossed most of the Greenbelt. The tracks will need to drop to meet the road grade.

In order for the tracks to pass over the 174’s west-bound to west-/east-bound off-ramp, they will need to be at about 64 metres ASL; and to pass over the 174’s west-bound lanes 400 metres later, the tracks will need to still be at 56 metres ASL. This is a mild drop of 8 metres in 400 metres distance, for a grade of -2.0%. Unfortunately, there is only about 150 metres left before the creek and the tracks would need to drop a further 6 metres in 100 metres to match the elevation of the road before the crossing; a grade of -6.0% before quickly leveling off to begin ascending. This sounds more like an amusement ride than a transit system. It is not something that I would recommend doing.



Figure 2: Elevation reading along the 174 at Montreal Road

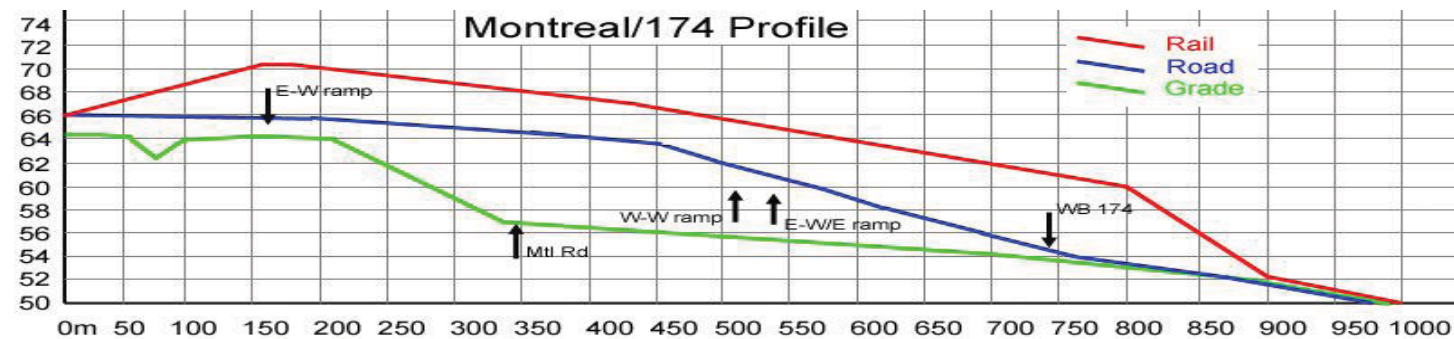


Figure 3: 174 over Montreal Road Profile

Alternative Options

The remaining three options are:

- Move the train into the median of the highway before the Montreal Road interchange and follow the existing grade of the roadway;
- Take the tracks under the ramps, Montreal Road, and the 174 lanes and have the road drop to meet the median tracks;
- Keep the tracks on the north side of the 174 until after the LRT crosses the Greenbelt; or
- Widen the Transitway fly-over east of Blair and run the trains on the south side of the highway.

Median

The tracks would need to rise over the west-bound lane of the 174 on a new structure and come down in the median somewhere between Blair Road and Montreal Road. This could be coupled with a new station near Jasmine Park.

Moving the tracks to the highway median west of Montreal Road will likely put the new Montreal Station in the middle of the road overpass. This could require the overpass to be widened by as much as 21 metres. This assumes a central platform of 10 metres width. There are ways of reducing that width requirement, but the point is that the interchange could take up more space. Pushing the highway's lanes apart also requires the circular ramps to be pushed out. Unfortunately, property has been sold from the north-east quadrant so the west-bound to west-/east-bound off-ramp can't be moved too far; any major movement will need to be into the NCC's Greenbelt land to the south.

It may be possible to shoe-horn everything in, however, with other platform configurations. For example, the central platform of the station need not be a full 10 metres wide. Ten metres is, I believe, the standard that was being used for preliminary design of the downtown stations (since changed to side-platform stations); but those stations were being designed to handle thousands of people per hour. For these stations, that might not be necessary so one solution might be to use a narrower platform. This will limit the stations pedestrian through-put and could result in crowding if trains from both directions arrive at the same time.

Splitting the platform into east-bound and west-bound sections could be used to reduce platform crowding while still narrowing the platform width; essentially trading length for width. There are practical limits to this, though, and the platform mustn't be made too narrow.

In Figure 4, the station platforms are separate for each direction, but they are still joined (by the light blue connection) across the road so that only two stairs and elevators provide the required redundancy. The stairs and elevators would go down to alcoves in the abutment walls. Having the connector across the road allows for 'over-flow' space for customers, as well as a safe way for them to get to, or come from, either side of the street below. Railings should, of course, be provided along the connector.

With this arrangement, the platform width could probably be reduced by half, down to about five metres. This allows for a three metre wide elevator or stair case with a two metre path remaining.

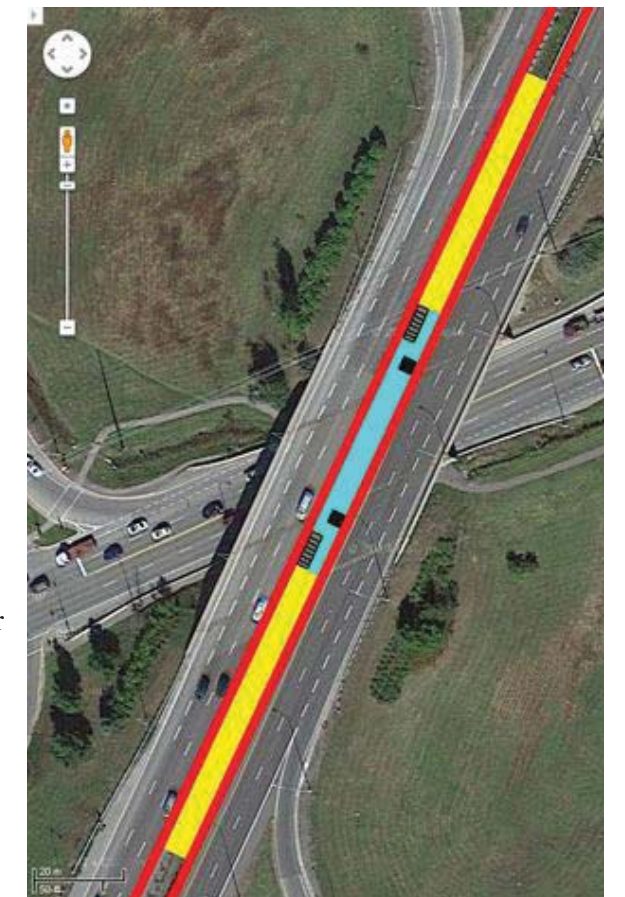


Figure 4: Narrow Median Station

There is roughly five metres between the existing east-bound and west-bound structures over Montreal Road. That means that the tracks would need to be placed on the existing structures and the car lanes shifted out onto the outside of widened structures.

The structures would not need to be widened more than about four metres each, which might allow the circular ramps to be ‘tightened’ and not moved. Since the existing overpass structures are approaching 60 years old (constructed in 1958 – see geoOttawa’s 1958 imagery), they should probably be replaced entirely; with longer and wider ones. Normally, this would involve building the new structures beside the existing one and shifting traffic onto the new structure while the old one is demolished. However, since the required additional width is quite small, there is no need for an entirely new three-lane structure to be built beside the existing ones.

Instead, it might be possible to construct the new platform areas and connector in such a way as to use it temporarily for traffic. In this scheme, after the platform base and connector are ready, the west-bound to west-bound circular ramp would be closed. Two lanes of west-bound traffic would run on the two northern bridge lanes (including the former on-ramp lane) and two lanes of east-bound traffic would use the third (formerly the west-bound ‘fast’ lane) lane and the connector. With all traffic removed from the old east-bound structure, it can be removed and replaced in the same location. When that is done, traffic would be shifted to the new bridge (if it was built wide enough) or the new bridge and the connector, so that the old west-bound structure can be replaced.

Once the tracks are past Montreal Road, they could continue in the median, as planned. This idea effectively changes from the Northern or Hybrid alignment to the Median alignment sooner, so there would be a structure between Blair and Montreal stations to get the tracks into the median. That structure could be a Jasmine Station.

Underground

Instead of taking the tracks up and over the roads, the tracks would be sunk in a trench under the ramps, Montreal Road, and the west-bound lanes of the 174. Because the 174 is dropping to the east, the road would eventually drop to meet the level of the tracks, putting the tracks into the median of the highway. The lowest point of the trench would be under Montreal Road which is at about 58 metres ASL at the ramp intersection west of the crossing. If there are utilities under the road, then that would push the trench deeper. Assuming a three metre buffer for utilities, then the tracks could be at about 50 metres ASL; approximately the same elevation that the 174 will drop to. Starting the descent from around the end of Shefford Road, a -4.5% grade will be down to 50 metres ASL before Montreal Road. Unfortunately, the trench would be as long as the elevated structure required to get to the median in the Preferred Plan. However, that one structure would cross the interchange, provide grade-separated station infrastructure, and cross into the median of the highway; it does it all.

It would also be possible to use the same split platform arrangement shown in Figure 4. In this case, the platform connector could span *under* Montreal Road so that pedestrians could safely cross to the side the wanted. I would recommend that the stairways be constructed such that there is no riser on the back of the tread. This will allow a clear view from platform to platform through the stairs. The space between the two elevators could be enclosed for storage; so that it does not provide a ‘niche’ for ne’er-do-wells to hide in.

I suppose a benefit of going below grade is that people from the neighbouring houses won’t complain as much about losing their privacy to people looking over the noise barrier from the trains. With an elevated train, there might be a need to increase the height of the existing noise barrier.

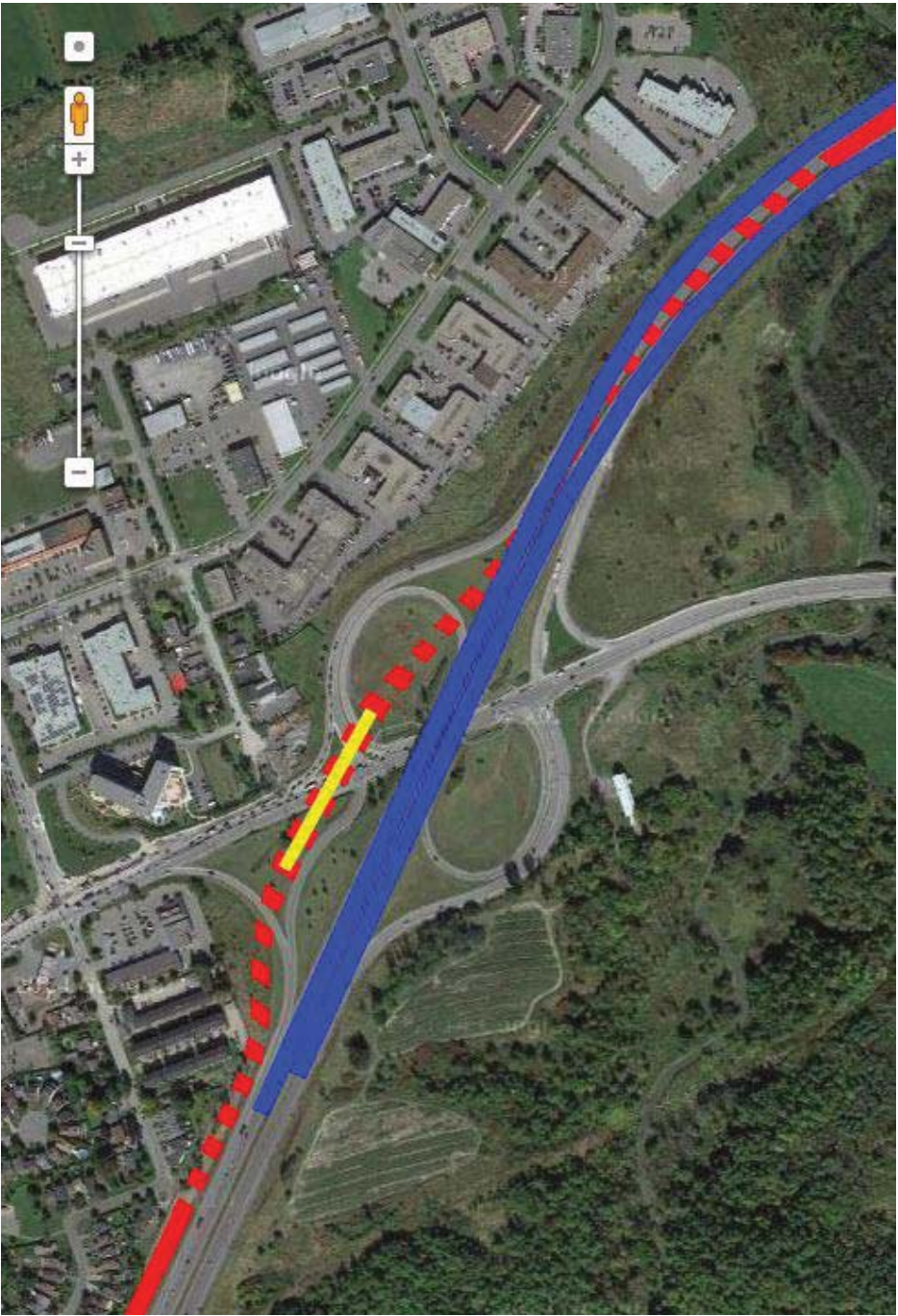


Figure 5: Montreal Road Underpass to Median

If the underpass of the west-bound 174 lanes can be done first, then the empty trench (before tracks and station are installed) could be used to detour the west-bound traffic from the 174, allowing the west-bound overpass of Montreal Road to be replaced. Then east-bound traffic would be detoured onto the new west-bound bridge while the east-bound structure is replaced. Once the 174 is restored, the tracks and station can be added to the trench.

In order to take advantage of having an empty parallel trench to divert traffic to, the trench’s underpass of the west-bound 174 lanes must be built first. I suspect innovative methods would be needed so as to not ‘break’ the 174’s lanes during construction. Perhaps top-down construction or the Sequential Excavation Method (SEM) with a supporting ‘umbrella’ could be used. Alternatively, it might be possible to temporarily widened the east-bound lanes to allow two west-bound lanes to detour around the construction point but return to their lanes before the bridge.

North Side

Keeping the train on the north side through this interchange could require a shorter overpass structure, crossing the ramps and Montreal Road only. It will require a second structure to fly-over the west-bound 174 lanes east of here. This interchange is simplified, but it causes added expense further along the line. The Sir George Etienne Cartier Parkway is the first of the remaining structures which likely will not support a northern alignment without major construction.

Similar to the underground option, the new two-lane structure for the train could be temporarily used to carry the west-bound 174 lanes while the old Montreal Road overpasses are replaced; then the rail infrastructure can be added.

South Side

In this section, there are no overhead high-voltage wires along the south of the highway so running the train there would be possible. However, the station would also be south of the highway, where there is no chance of development; so everyone accessing the station would need to walk 60-150 metres further than the other options.

Construction-wise, crossing the intersection on the south side will be similar to crossing on the north side. The differences are the earlier need to get to the south side of the highway and the crossing of Green’s Creek to the east. It might be possible to widen the existing Transitway north-side to south-side fly-over east of Blair so that it can hold the two sets of tracks. At Green’s Creek, things are potentially a bit easier since there is more room on the fill south of the 174. This means that no modification to the roadway may be required for tracks to be laid south of the 174.

Green’s Creek Crossing

The least expensive way to cross Green’s Creek is to use the existing fill. Unfortunately, there is not enough top width to handle an extra 10+ metres of running track; there will have to be modifications made. Putting the train in the median or to one side will require the same amount of width, assuming that the traffic lanes are moved closer together.

Probably the easiest method for widening out the top of the fill is to add retaining walls to both sides. I suggest that tie rods can be drilled through the fill to tie the two retaining walls together. The walls could be corrugated metal or concrete. Once retaining walls are added, there could easily be 50 metres of width to play with.

An interesting thing to note is that the 174 is not centered over the culverts; there is more space on the south than the north. In fact, it is likely possible to add the tracks to the south of the highway without needing to modify or move the roadway at all; assuming the fill’s top has been widened. Placing the tracks on the north side could require the lanes to be moved closer together to make room; that is, the west-bound lanes would need to be modified. Having the tracks in the median would require one or both directions of traffic to be moved aside to make room.

Something to remember is that the four culverts under the fill were installed in 1958 and are approaching 60 years old. Their condition should be assessed before tons more fill is added on top of them. Indeed, when I look at the satellite imagery, I see an ‘oil spot’ on the outside, west-bound lane. This is usually the sign of a sharp up-ward ‘bump’ out of a depression. Such a subsidence is usually caused by a localized packing or removal of the supporting material. There was such a mark at the spot where the sink-hole opened on the 174 at the Jeanne d’Arc ramp; also over a culvert. In both cases, the oil spot has been visible in satellite images since at least 2002. If the asphalt is being removed, you might be able to fill that depression and verify that the underlying material has not been eroding.

Sir George Etienne Cartier Parkway Crossing

The existing overpass could not likely offer the added space required to run the train along the north side or south side of the highway. Either the overpass would need to be replaced with a longer one or an additional, new, span would need to be added to the side for the train.

If the train is in the median, the directions would be split by the central bridge pier and the roadway lanes would be pushed out to the sides. There is the possibility of separating the traffic lanes (widening the median space) by cutting the slope of the abutments and shoring them with retaining walls. This would be the least cost option, but it requires the train to be in the median.

If the northern or southern option is chosen to simplify the Montreal Road interchange, then the tracks could cross the west-/east-bound 174 lanes in the 250 metres between Green’s Creek and this overpass to avoid the extra span; although the saving from not adding a span on the Parkway bridge would likely be more than countered by the cost of adding a fly-over.

If the highway is widened in the future, there might not be room under the existing structure if the tracks are in the median. If the tracks are in their own span to one side, then there would be room to add highway lanes under the existing structure by trimming back the abutment slopes.

Greenbelt Crossing

The high-tension power lines enter the highway corridor and follow the south side of the 174 from about 500 metres east of the Parkway through Orleans. The way things are, this hydro Right of Way (RoW) essentially precludes the tracks from being on the south side of the highway. However, the RoW is 30 metres wide so the wires could, potentially, be moved to the south, allowing lots of room for the tracks to be laid between the wires and the roadway. If this were done, then there would need to be track crossings added for hydro vehicles to move from the highway to the power lines.

Alternatively, the wires could be moved to the north of the RoW and the tracks added to the south of that, but still within the RoW. There are likely limitations as to how close to the highway the power poles can be, but with a 30 metres RoW, it might be possible. This would maintain HydroOttawa’s access to their wires from the highway while providing an easy corridor for the tracks that requires no roadway modifications.

Across the Greenbelt, there is a generous 15 metre median between the highway’s lanes so there would be room for the tracks in the median without modifying the roadway. A median alignment of the tracks might also be the easiest to deal with if the highway is widened in the future. Potentially, the existing bus-only lanes could be repurposed for general traffic – assuming that they were properly constructed in the first place and were not simply repurposed shoulders.

On the north side of the highway, space would be tight, but the tracks should just fit without modifying the roadway until it gets to the on-ramps from Jeanne d’Arc. At that point, there is a stretch of almost 200 metres that would need to take neighbouring farm property. If the highway is later widened, then the west-bound lanes would need to be widened into the median.

Jeanne d’Arc Station Area

There is an electrical distribution facility on the south side of the highway and the wires string through the interchange on the south side. It may not be practical to try to run the tracks south of the highway through this stretch. In addition to the cost of any underpasses or overpasses, there would be considerable costs for utility relocation.

On the north side of the interchange, there is minimal space. If the tracks were to be on an overpass, then it would need to entirely be a structure; i.e., there is no width to build an earthen ramp to the west or east. That structure would need to have clearance over top of Jeanne d’Arc Boulevard, which itself passes over the highway.

The least expensive and least disruptive solution for this area would be to run the tracks along the median, under the existing overpass. The median currently provides enough space for the tracks without affecting the roadway. If the highway is widened in the future, the slopes of the abutments can be removed to create the space.

In order to have space for station platforms, it might be necessary to use a narrow central platform or to split the platform, as shown in Figure 4. Splitting the platform puts the bridge piers in the connector so it needs to be wide enough to allow wheelchair passage around the piers. Making room for a wide central platform by cutting back the abutments and moving the lanes apart will likely rule out future road widening under the existing structure.

Orleans Station Area

Because there are no road ramps at Orleans Boulevard, there is plenty of room on the north side of the highway for running the tracks; although there would need to be a new underpass of Orleans Boulevard built. Building that new structure will be very disruptive to traffic on Orleans Boulevard.

On the south side, we have the familiar problem of the high-voltage wires.

Again, a track in the median can be made to fit by removing some of the bridge’s abutment slopes and moving the highway lanes out a bit. There, is, however, still limited room so efforts should be taken to use a narrow platform so as not to preclude highway widening in the future.

Place D'Orléans Station

Due to the central piers of the bridges and abutments on the edge of the rock cut in this area, both of the overpasses (pedestrian and Champlain Street) will need to be replaced with longer structures. Since so much effort and money will be used, perhaps it is time to also ‘fix’ the mess of roads in the area. I suggest that two new traffic structures be built to take cars around a one-way loop to the north of the 174, such as shown in Figure 6. This will remove several of the intersections and provide a traffic system much closer to free-flow.

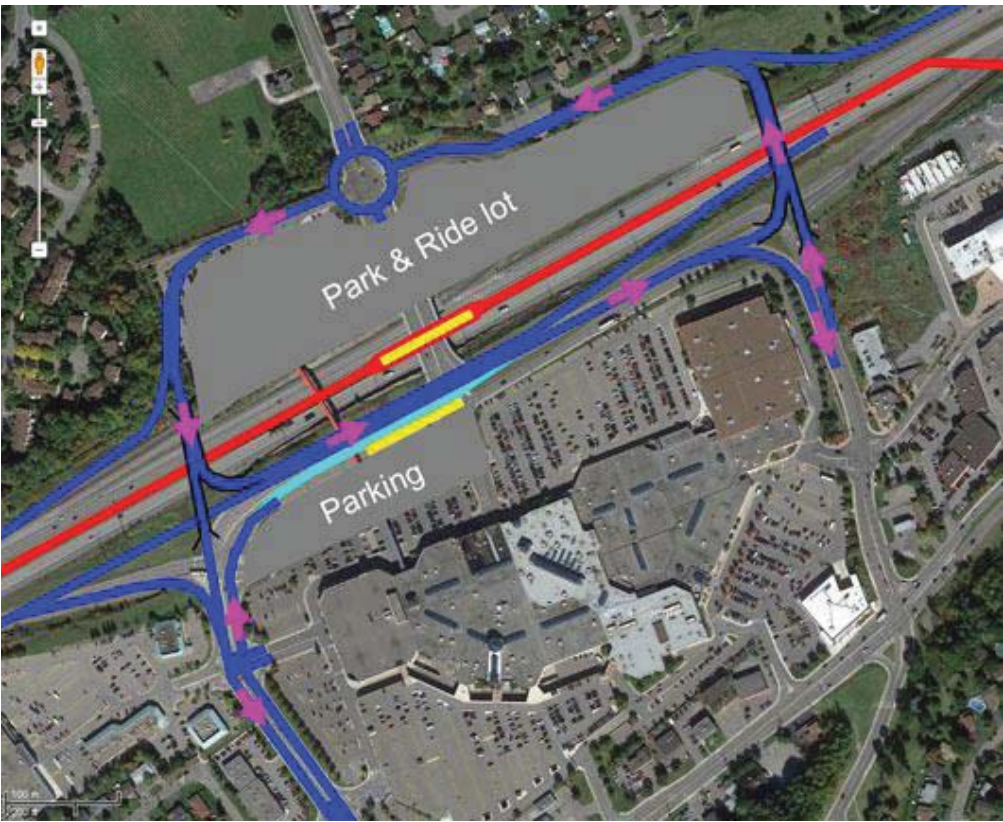


Figure 6: Rerouting Traffic at Place D'Orléans

In Figure 6, I have included a short stretch of light blue representing a Bus-Only lane to service the bus platform. With fewer roads along the south of the highway, and fewer buses needed after the train begins, simplified bus may be all that is needed. Buses can access this stop from all directions. This arrangement frees up the previous bus area for mall parking. [If the east-bound 174 on-ramp is pushed further east, then the bus platform can also be moved east, if this is helpful.] I would suggest that any bus staging and lay-up be done in a section of the Park & Ride Lot, across the 174.

One major benefit to building the new bridges is that they can be built with minimum disruption to the current traffic network. It also clears out the area north of the highway to provide ample space for a huge Park & Ride lot.

Although I have shown a median alignment for the train, since all new overpasses are being built, the tracks could alternatively be run on the north (serving the Park & Ride directly) or the south of the highway; but with those alignments, the there would also need to be underpasses of the on-/off-ramps. There would need to be some analysis done to find out where most of the ridership comes from throughout the day to determine if a south-side station would simplify transfers for the most customers. If it is determined that the Park & Ride generates the most ridership, then the train could be moved to the north side of 174 and the peak period buses looped around the Park & Ride.

A south side alignment has the option of veering in to the existing bus station. Figure 7 shows a possible southern alignment. The train could swing under the east-bound lanes of the 174 and remain below-grade in a trench until it surfaces east of the area. The train would stay on the south side and use the existing south-side infrastructure.

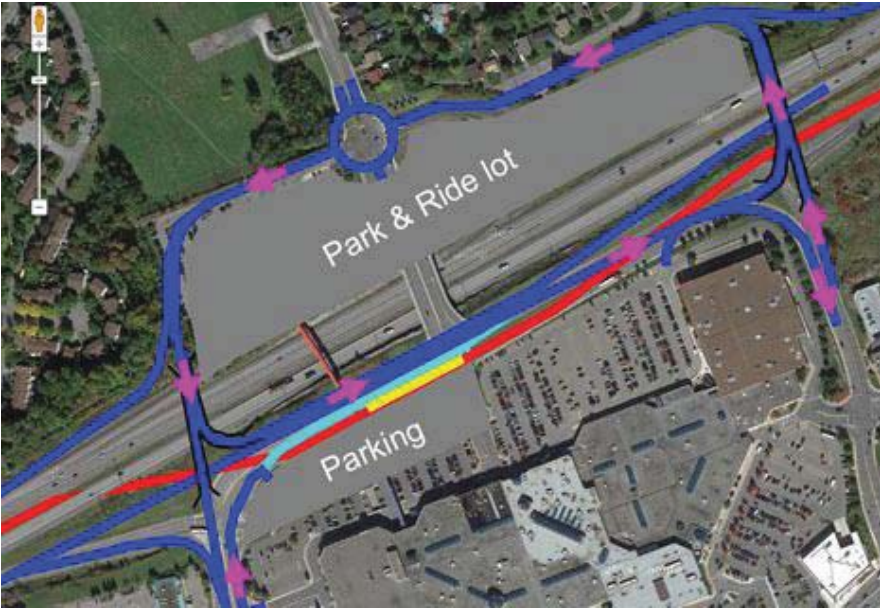


Figure 7: Place D'Orléans Southern Alignment

I have **not** shown, in Figures 6 or 7, any pedestrian overpass between the bus platform, the train platform, and the Park & Ride. It would likely be situated approximately where the existing bridge is over the highway. It might be the case that only peak traffic volumes from the Park & Ride require a wide pedestrian overpass, so moving the station to the north might result in a narrower pedestrian structure; or, at least for half of its span with a median alignment.

This station is to be the (temporary) terminus of the eastern extension of the Confederation Line. As such, it will likely require storage space for extra trains; this can be provided by adding a tail-track to the east of the station. In the future, the tail track will be the start of the next extension. In Figure 6, I have shown that the tracks could veer south as they go east from here, if the existing Transitway infrastructure was to be used. Figure 7 shows the tracks staying on the south.

With the one-way loop, some folks might have a bit further to drive, but it will be mostly without intersections; which could make it faster. As well, making the drive seem a bit longer might help coax people onto transit.

Orleans Town Centre Station

There is no need, in my opinion, for stations to be less than one kilometre apart; therefore, this station is not needed.

Tenth Line Station

Unfortunately, it appears that the Transitway infrastructure under tenth Line is being used to develop the area. The west extension of Eric Czapnik Way, under Tenth Line, makes it very difficult to run the train on the south side of the 174 without constructing more underpasses.

A median alignment should be able to fit under the Tenth Line structure, especially if the 174’s east-bound off-ramp is pushed south to the next span and the central platform of the station off-set slightly to the south. (The bridge pier does not have to be in the center of the platform.)

Running the train on the north side will require a number of new structures.

Taylor Creek Station

The position of this station is far enough away from Tenth Line, but it could be too close to the Trim Station if that station is not positioned east of the interchange. The location of this station will serve La Cité Collégiale if a pedestrian overpass is provided. This would likely remove pedestrians from Trim Road and give people a safer route. Construction of this station could be delayed if cost is an issue; however, it will only get more expensive by waiting.

Trim Station

The 174 / Trim Road interchange needs to be designed so that it allows the train to get to the Trim Park & Ride. This means either providing room under ramps and Trim Road, room over the ramps and Trim Road, or having the train follow the 174 median and then cross the east-bound lanes, either over or under. Also, the interchange should minimize the amount of land that it takes away from the existing Park & Ride.

Probably the simplest configuration for this area is to have the train in the median as the 174 passes over Trim Road. Then the tracks would remain elevated as the highway dropped, allowing the train to pass over the east-bound lanes. This idea is shown in Figure 8. It takes advantage of the steeper grades possible for road vehicles by having the 174 descend faster than the train east of crossing Trim. Once there is sufficient clearance, the tracks swing south, over the east-bound 174 lanes, and then they drop to the station. The station will likely only have a platform on one side of a single track.

Because there will be rail connectivity from the Trim Park & Ride directly to downtown, I would strongly recommend that the size of the Trim Park & Ride area be enlarged while the tracks are being laid. If people arrive to take the train but can not find a parking space, they may not try again.

Another idea is that there could be space to store trains out at this end of the line. Perhaps even a maintenance facility could be built here, where land is currently available.

Finally, another thing to keep in mind while designing this interchange is that the 174 is likely to be widened at some time in the future; space must be included for extra lanes. Also, sufficient space needs to be allotted for Trim Road to include cycling and pedestrian facilities, as well as space for future widening.

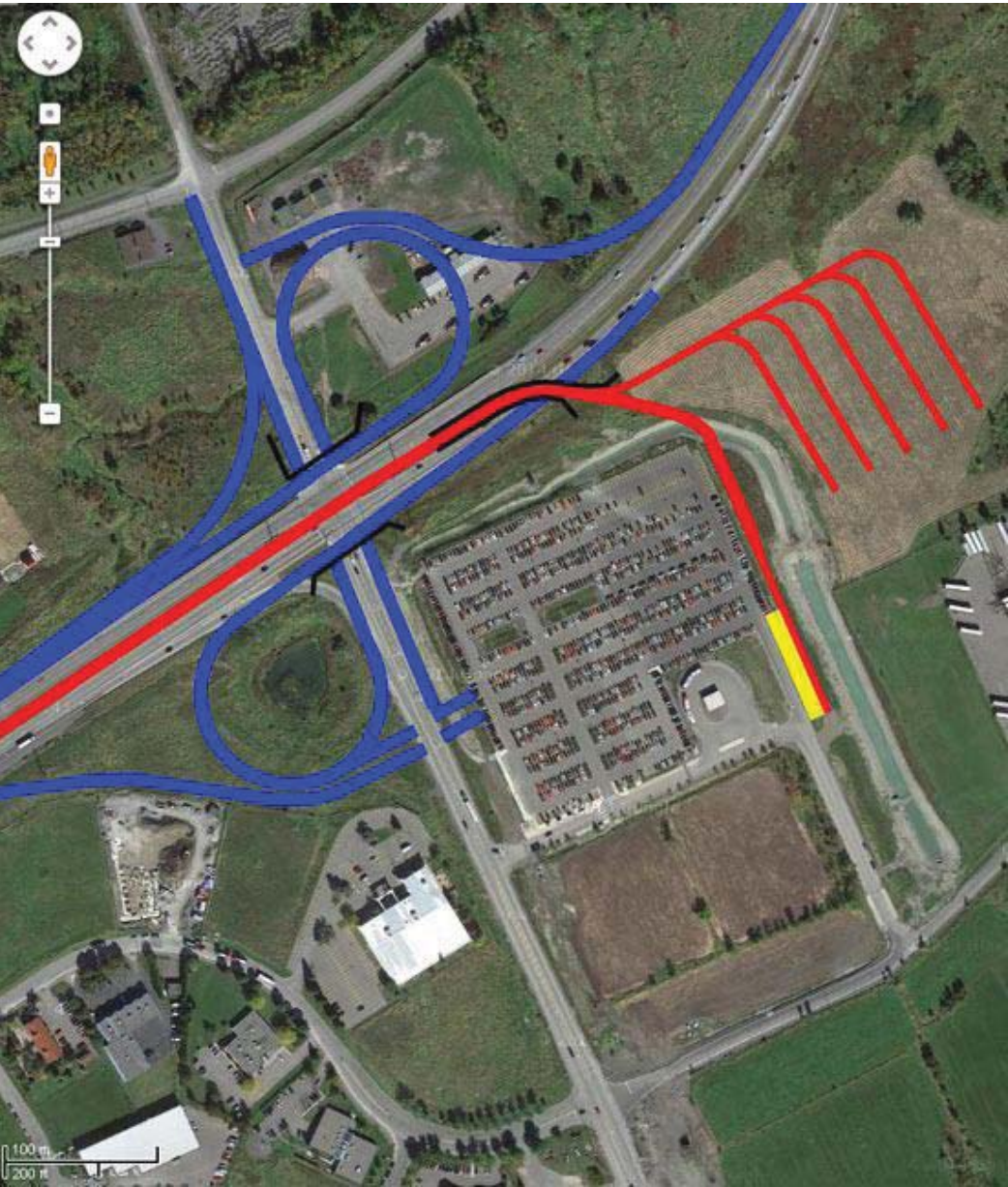


Figure 8: 174 / Trim Road Interchange with Median Alignment

Conclusions

I like the idea of providing temporary bus stops off the 174 at Blair. This will allow better bus service while the Transitway is converted to rail. It requires slight lengthening of the 174’s bus-lanes, but it frees up the underpass of Blair Road, which will bring the train to the north side of the 174.

Along the north side, there is a possibility of adding a station at Jasmine Park. This is an existing, fairly densely populated area. It is sufficient distance from either Blair or Montreal stations that people would require a bus to get to either. Adding this station would improve service to this area at a reasonable cost; and it might reduce bus costs.

There are three real options for the Montreal Station and getting the train across the interchange; the method described in the preferred plan does not appear to be one of those options due to the topology of the area. Of the three, two are on the north side of the 174 and the other is in the median. The median option would appear to be the least expensive, requiring a fly-over from the north side to the median and pushing the 174’s lanes apart.

One way to provide the fly-over is to rise up for an elevated station at Jasmine and then drop into the median from there. This would lengthen the distance that the lanes need separating, but it would eliminate an additional pedestrian overpass to a central platform at a Jasmine Station; if that station were to be added.

If the train stays on the north, then the options are to take the train over or under the interchange. Going over will likely mean that the tracks will stay north of the highway while they cross Green’s Creek; then a structure would be required to move the tracks into the median. If the train goes under, then it can also be moved into the median at the same time.

Of the options, I think that either an additional elevated Jasmine Station, transitioning into the median before the central Montreal Station, or a northern, underground station, moving to the median east of the station would work well. If the Jasmine Station is being built, then that option is probably the least expensive.

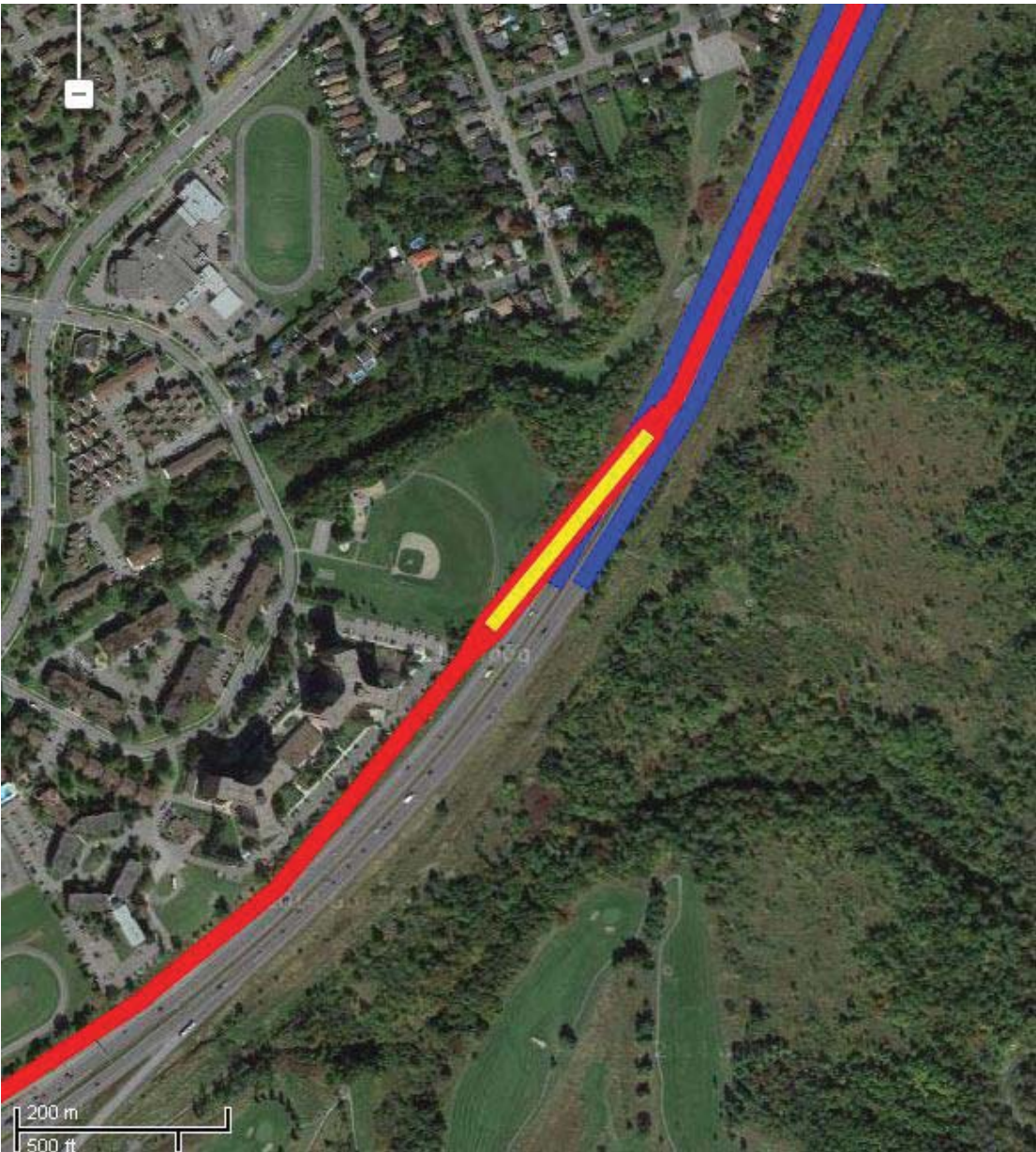


Figure 9: Elevated Jasmine Station

The new track and station infrastructure at the Montreal Road interchange should be used to mitigate traffic problems while the existing, aged, 174 overpass structures are replaced.

Assuming that the tracks are in the median before Green’s Creek, there is relatively little to cause problems until the Place D’Orléans area, where I believe the existing structures do not have sufficient clear spans. This area’s roads are a bit of a mess and could also be simplified. As well, the Park & Ride could be made much larger. I have suggested one arrangement that adds two traffic overpasses and one pedestrian overpass. It provides more space for a larger Park & Ride, and it restores parking at the mall. In this area, I would suggest that the trains still be in the median for simplicity.

From Place D’Orléans, it is, again, easy to keep the train in the median until after the highway crosses Trim Road. This is all the more practical, seeing as how the future Transitway infrastructure seems to be being used for roads. Once at Trim Road, using the drop in the 174 after the new overpass, the tracks can swing over to the Trim Park & Ride Station.

There might be some savings from not building some stations at the start, but if those stations are built in the future, the ultimate cost will be higher. That said, I do not think that the Orleans Town Centre Station adds significant access to the system but it does add another station to slow things down. I do not recommend including that station, now or in the future.

Summary

I agree with a (slightly modified) Hybrid alignment and think that it can produce the lowest cost solution; however, the devil is in the details. The following points are the essence of what is written in this document, but I encourage the reader to make use of the entire document because there are many details in the full text that might prove useful.

- Add temporary bus stops along the 174 at Blair Station.
- Running the train in the median of the 174 allows for the replacement of fewer structures.
- The existing Transitway underpass of Blair should be used for the train.
- Consider an extra station at Jasmine Park to provide a fly-over from the north to the median of the 174.
- Use a central platform station in the median over Montreal Road.
- Use retaining walls to increase the top width of the fill across Green’s Creek. Tie the walls together with rods through the fill.
- Cut back the slope of existing bridge abutments using retaining walls to provide more space.
- Reconfigure the road network around Place D’Orléans and the Park & Ride there.
- Forget about the Orleans Town Centre Station.
- Take the train from the median of a new Trim Road overpass directly to a much enlarged Trim Park & Ride.
- Consider a train storage (and possibly maintenance) facility at Trim Road.
- Purchase houses, if necessary, to ensure convenient pathway connections from neighbourhoods to the stations.
- Ensure this work does not preclude future widening of the 174. Add extra infrastructure now if necessary – it only gets more expensive as time goes by.

So, extra bus stops at Blair Station; use the existing Blair underpass; move from the north to the median at a new Jasmine Station; temporarily use a central platform between the two Montreal Road overpasses to help replace those structures; stay in the median until it has gone over Trim Road; then turn south to the enlarged Trim Park & Ride. While this is going on, reconfigure the road network at Place D’Orléans, the Park & Ride, and Champlain Street.

#34

From:
Sent: February 13, 2015 11:52 AM
To: Taylor, Angela
Subject: Fwd: LRT/174 Widening Environmental Assessment & 2015 Budget Consultation - Comments

Hello Angela,

Thank you for the opportunity to to comment on the LRT/174 Widening EA and budget. We will focus on the LRT portion.

> We live within walking distance of the current and planned Jeanne d'Arc station, and attended the 4 Feb briefing on the LRT Ph 2 extension to the East end. I was impressed by the methodology for the EA and selection of the preferred option. Also, the presentation was very well done. (The room available with "free sauna" was unfortunate. That so many stayed until the LRT portion was complete, shows the importance to citizens East of Blair.) Due to the late hour and our discomfort we could not stay for the second topic, widening of 174 East of Trim. We have shared the information gained with many friends and neighbours.

The hybrid solution for LRT Phase 2, i.e., North side of 174 from Blair to Montreal Rd, then down the median to Trim Rd sounds very good. We have seen the construction of a rail transit system in the median of Hwy 640 North of Montreal. With friends and family I have walked at least half the area East of Blair to Montreal Rd, and have kayaked Green's Creek. Both areas will need preservation as identified in the EA studies.

The 174 median solution means that the current bus lanes should be converted to traffic lanes or HOV status, and should be better constructed and maintained than they have been until recently. In Winter, taking the bus through the greenbelt section caused many of us commuters real concern as buses in bad weather did have wheels slide off the pavement, especially on the North side of Hwy 174. The LRT will be safer, cleaner and faster.

Relatives and friends in other parts of Canada are embarrassed when their Capital city is not up to standard. When they visit and see the infrastructure limitations they are rightly disappointed. Decision makers must also remember that Orleans and other East end Ottawa residents have historically made the best use of transit. Attempts to get more employment East of the Greenbelt have been disappointing, except for retail. Improvement of this reality is likely to be slow. Travel to workplaces elsewhere in the city of Ottawa is thus unavoidable for most residents, so we really deserve reliable fast transit. This LRT Phase 2 extension is an excellent solution to the need and the many problems that are getting worse with steady population increase. The car is not the solution for the majority in a modern city, and never has been. Ottawa's electric street cars were a sign of innovation and the need in their day.

We strongly endorse the recommendation to build the LRT line all the way to Trim Rd in Phase 2 (affordable). This change to the approach was stated by residents at the Orleans meeting on 4 February and applauded by the audience. It is critical to the large number of transit users East of Trim Rd who

use the Park and Ride, those who must drive to work during the main commute (slow, frustrating and accident filled), or the thousands of citizens building homes or planning to move to condos/ apartments near Trim Rd. The small stations such as Orleans Blvd can wait until demand and funding permit. Four of the identified eight stations are needed (St. Joseph, Jeanne d'Arc, Place d'Orleans and Trim).

The attendance of four Councillors and other politicians was a good sign. The briefing was most appreciated. For many critical reasons and for the future of Ottawa, its citizens and visitors, the Confederation Line East LRT Extension must proceed and be funded.

#35

From:
Sent: February 13, 2015 6:40 PM
To: Taylor, Angela
Cc: Frank Carrier
Subject: Hwy 174 / County Road 17 Widening

Further to a meeting I attended on Feb 5, 2015, I would like to submit the following comments:

I do not support the preferred option of widening the existing Hwy 174/County Road 17 for the following reasons:

- 1) The preferred option is not what is best for the community or the residents of Rockland. It was selected by Ottawa because it ends up at the Trim Road LRT station, with the hopes of increasing ridership for the Ottawa LRT. I would be interested in knowing what the results of the evaluation methodology would have been without the LRT as a factor.
- 2) The selected option is the most expensive, without any good justification, except to feed the LRT.
- 3) The biggest problem with Hwy 174 isn't the commute from Rockland to Orleans, it's actually IN Orleans - the bottleneck at Jeanne D'Arc St. This is not being addressed or resolved at all with the preferred option. In fact, this option will only worsen the matter by bringing the traffic at a faster rate during rush hour to the problem area. An alternate route would have split the volume of traffic in two and reduced the amount of traffic going through 174/Jeanne D'arc.
- 4) The representatives there didn't seem to all be on the same page or understand the proposed design the same way. While discussing the options with one woman before the meeting, she confirmed the divided highway meant we would no longer be able to turn left onto the highway to go to Rockland. She advised we'd have to turn right (which is the opposite direction of Rockland) and go turn around somewhere to head back to Rockland. This is totally unacceptable to me. However, while discussing the same topic with one of the men, he advised there would be no divided highway in front of the homes fronting the highway. Service roads would be the preferred option - although there is absolutely no space for one in front of our home.... Not sure anybody really knew the answer to our questions, or they were just avoiding it..
- 5) During another discussion, they assured us there would be no expropriation of properties along the highway, but said they had no details of individual properties. I do not believe they were honest with us. Our property has Hwy 174 on one side and the Ottawa River on the other side, with no space for an extra lane, or a service road. I would appreciate factual information and details affecting my specific property.
- 6) Waterfront properties are few and far between, and this 4 lane highway will negatively impact all waterfront properties along the Hwy 174/Ottawa River. The increased traffic noise, the faster traffic, etc. will significantly reduce property values and impact the enjoyment of our properties. Waterfront properties have a higher value than other properties, and the evaluation methodology likely does not give an appropriate rating/weighing factor for waterfront properties, as it should. It seems the evaluation gave more consideration to farmland than waterfront properties.
- 7) I live on approx. 1.5 acres of waterfront. There is still a rich abundance of wildlife on our property with many bird species, owls, falcons, eagles, spawning fish in the bay, many types of ducks and waterfowl with their young, as well as turtles who lay eggs each year. More roads, more traffic, more noise, will have a significant impact on the wildlife and push them out farther away. Although

one lady said the 'turtles' were taking into consideration, I do not believe anybody there understands or appreciates the real impact such a highway would have on the wildlife..

8) Although these meetings were called 'consultations', they truly are not. The decision is already made, and the presenters weren't really there to consult, or hear what we had to say. In fact, some of them got really defensive, to the point of being rude and unprofessional. It was simply a marketing meeting, and for this reason, many residents won't bother submitting comments, because they know it won't matter.

9) Bringing a 4 lane highway right through the middle of our urban area isn't what I would consider a good plan. In most other cities I've seen in Ontario, the 4 lane highways are built as a by-pass in the outskirts of the town - not right through it. Our town can only grow south, which is also where other communities are building up.....it only makes sense to build the highway south of the city to accommodate the growing suburbs and the surrounding communities as well. Not everybody travelling from Rockland needs to end up at Montreal Road.....An alternate route for those commuters going south or west would make sense..

10) The Hwy 174 in Rockland has a speed limit of 70 kms, and already has numerous accidents each month. Making it a 4 lane highway with a higher speed limit will only increase the number of accidents..

I would like someone from the city of Ottawa to come and show me the details of the Hwy design in front of my property.

#36

From:
Sent: February 14, 2015 7:34 PM
To: Taylor, Angela
Subject: Confederation Line East LRT Extension

Hi Angela:

This is my feedback of the presentation in Orleans on Feb 4.

Good presentation. Location: cannot be in a worse place. How come other sectors, namely the West... have better locations than us...again? We were over the limit established by the Fire Marshall - the City broke its own rule. The presenter spent too much time spent on the methodology - could have been done in a minute, leaving more time for the stuff and the questions that we came for. When we lift our hand for a question, we expected to be able to raise our point and not be ignored. After all, we paid big bucks for this contractor who just did mediocre quality work.

Our concern is that 100% of the taxpayers are forced again to pay for LRT as it is the case for the OTranspo cash cow who serve mainly after peak hours tax-payers that pay low taxes in near empty expensive buses; nevertheless, only 20% use it (buses would never be able to accommodate 30-40% of the people - people are already compressed as sardines at peak times). I am retired and am so happy to never take OTranspo anymore but unhappy to be one of the 80% that will not be using it but paying dearly for it. I have used OTranspo 4 months per year (at a cost of \$1,200-1,300/year including the OTranspo share of my high taxes, or \$300/month), cycling the other 8 months.

Like I mentioned to Stephen Blais, we will be paying for LRT way before we get the services and also have the least expensive part of the LRT line (will be done on the already built transit way up to the 174 and then in the centre of the 174) compared to the down town tunnel and NCC land and West extremely high cost proposals. That's once again totally unfair to have us pay for the others; this is considered highway robbery.

I told Mr Blais, which he concur, that the Orleans blvd, Centrum and Taylor Creek stations could be removed (not many would be using it) and better use the money to do a good job and extend the LRT right away up to Trim Road.

At the end, are we getting the bang for our buck? Of course not! Please have a peak at the attached Powerpoint presentation of China's 1,318 km train line for a 300-386 km/hour train much more comfortable than the LRT. It crosses 244 bridges of including the two longest ones in the world (164 km & 114 km) and 22 tunnels for a total tunnel-distance of 161 km. Some of the train stations are bigger than our airport terminal. And all that for \$40 Billion and completed ahead of time. We'll pay \$3-5B for just a few dozen km and one short tunnel, and it will take years and years to complete it. We are not impressed at all at the high cost of the LRT.

Cheers

#37

February 15, 2015

Angela Taylor, P. Eng, Senior Project Engineer
City of Ottawa

Dear Ms. Taylor:

At the Confederation Line, East Light Rail Transit (LRT) Extension meeting of February 4, 2015 we expressed concerns and was instructed to put them into writing. Here are our concerns:

- We have lived on East Acres Road for over 20 years and back onto the 174. Vibration issues have been an ongoing concern and we've contacted the City several times and our concerns continue. The City Engineers have previously installed seismographs on two separate occasions spanning 36 months and determined vibrations were excessive. We were assured that it would be fixed the next year when paving work would be done on the 174. At the time we were advised to live with it, get cracks repaired and bill the City ... which seemed unreasonable since the problem wasn't solved. Once paving was done the vibrations ceased for a short period of time ... we had work done and did not make a claim with the City thinking it was fixed for good. Unfortunately it wasn't .. our house still vibrates and damages continue.
- When we recently learned that two rails of LRT would be in our backyard we assumed it would be on the south side of the 174 which made most sense since no homes would be affected and there was plenty of easement between the hydro poles and the 174. We were shocked to learn that wasn't what the City was proposing.
- We are concerned about our quality of life and the continuous damage being done to our home. The vibrations are endless as buses or transports cause our bed to literally shake. It's scary to think the damage that is being done to the home we have lived in for over 20 years.
- We have serious concerns about two new LRT lanes being added to the north side of the 174 ... since nothing has been done to address our vibrations in the past we have lost faith in our concerns being addressed in the future.

As taxpayers we hope our concerns will be taken into serious consideration and addressed appropriated.

We weren't naive when we purchased our home backing onto the Queensway ... we enjoyed the privacy of no neighbours in our back yard, love the location and accepted that there would be noise disruptions with so many vehicles using the 174. We were not expecting the constant vibrations and damage to our home due to poor construction of the 174 and additional bus lane added at a later date. This will only intensify with wear and tear of the road and 2 LRT lanes.

How will the City address our serious concerns?
Sincerely,

cc: Tim Tierney, Ottawa City Councillor of Beacon Hill-Cyrville Ward

#38

From:
Sent: February 16, 2015 6:09 PM
To: Taylor, Angela
Cc: Tierney, Timothy
Subject: Confederation Line - Expansion of the 174 and the LRT

Expansion of the 174 and the LRT

I attended the Open House on the LRT at the Bob McQuarrie Complex on February 4th, 2015 and as a property tax payer in this city, I have serious concerns.

My deepest concern because it impacts my home life, is the fact that our house backs onto the 174 that runs between Blair Rd and Montreal Rd referred to as Beacon Hill South. We have lived in our house for 17 years now. The main reason we bought this house in 1998 was the fact that it backed onto the 174 and offered the privacy we wanted in a backyard. It stood to reason there was no room for growth or surprises. A highway is a highway especially when it backs onto a developed community with no space between the highway and the houses. We were shocked to hear the expansion would be on our side of the 174, just feet away from our backyard.

For the first several years, though we could always hear the expected constant flow of traffic, the house never shook.

In the past few years and with the addition of a bus lane on the south side of the 174, our house vibrates regularly from the flow of heavy trucks and buses. When we called the City we were told “road engineering problems”. We thought the City had fixed the problem after our calls but the fix was temporary and the vibrations returned within a few weeks.

The ongoing vibrations are causing crack damages to our house as I’m sure to all the other houses on our street that back onto the 174.

What will our home life will be like with the extension being added and the trains so close in proximity to our house?

Was our house, where the space is the narrowest, ever taken into consideration for the expansion?

Aside from my house which is my primary concern I also want to raise my other concerns. Does Ottawa, a one-horse government town, really need both - an LRT system and a public bus system? Can we the tax payers really afford the cost to keep both running? Is the demand there? Currently, with so many government and private layoffs in Ottawa how many riders will actually use these modes of transportation regularly to support the cost?

Ottawa does not compare to big cities such as Toronto and Montreal in transportation needs. The higher population and diverse employment centres in those big cities dictate public transportation needs. The same principle should apply to Ottawa.

Ottawa is not a big city with a much smaller population. The major employers are at the three

government levels: Federal, Provincial and Municipal. We do not have a multitude of big industries like Toronto or Montreal. As well, Ottawa is still small enough that many travel to and from work by bicycle, year round, every day. Ottawa encourages bicycle travel and provides bicycle lanes and paths for easier and safer travels.

From experience, I firmly believe that the existing City buses meet Ottawa’s needs in public transportation.

I travel the bus daily to get to and from work, Bus #24 (Runs Peak hours only) Montreal Rd and Shefford to Centertown and back again. Some days the bus is full, riders are standing and some days it is half full with many empty seats.

I work compressed hours and every 2nd Friday at noon I take a different bus. I take Bus #12 from Centertown to Montreal Rd and Ogilvie. Again, ridership is very sporadic. Some days the bus is full while other days there are many empty seats and again the bus is taken same day, same time.

Ridership on any given weekday (peak hours or not) is very sporadic on at least those 2 bus lines.

Was ridership really taken into consideration?

Another concern – The planned LRT station at Montreal Rd and the 174 is not easily accessible to the neighbourhoods it intends to serve.

There are existing bus stops at that location - Bus #95 on both sides of the 174. These stops do not easily serve the people of Beacon Hill North nor South given the fact that these stops are isolated and not within reasonable walking distances from the stops to the neighbourhoods.

Walking up from Shefford north you must climb a big steep hill and then go down another big steep hill on Montreal Rd to get to the stop/station at the 174. Reverse getting off the bus, climb up Montreal Rd first. Both those big steep hills are a work out for those that are physically fit. Not all commuters have the physical capacity to tolerate the climb. The other option is to take a local bus to the station, just to avoid the big steep hill. Local buses do not run often enough to accommodate that option.

The presenters highlighted the station convenience for Canotek Industrial Park riders. Reality is, Canotek Industrial Park is made up of small privately owned businesses that people drive to and from in their personal vehicles. Free parking is provided to workers and customers and it really is relatively a small area.

The fact that a new turning lane was installed at Montreal Rd and Shefford intersection to manage the heavy flow of vehicle traffic to and from that Industrial Park onto the 174 is proof of personal vehicle and truck volume.

As a tax payer I sincerely hope the City deals with our house vibration concerns appropriately and that our hard earned tax dollars are going to be spent wisely.

#39

From:
Sent: February-14-15 12:56 PM
To: Monette, Bob
Subject: Re: Highway Expansion

I was unable to attend the public meeting regarding plans for the LRT and expansion of Hwy #174 but my good friend, _____, gave me an update and what I read in the local newspaper alarms me.

Every time I return from my January sunshine getaway in Fort Lauderdale, I come home embarrassed about the highway infrastructure, or rather lack thereof, in the capital city of CANADA! The proposal for LRT is good news and going down the middle of the 174 has merit as long as protective barriers are considered. God forbid that we have an accident that causes a truck or car to rollover into the tracks of the train.

The reason for my concern is that we must go the route of encroaching on the expensive and beautiful homes along the river in Cumberland and that of the Camelot Golf Course (I have no personal relationship with either) and make us wait 15 YEARS to get relief from the congestion of traffic into Orleans from the east. I find it incredibly insulting that Stephen Blais would say "So you want to go to heaven but you don't want to die" as quoted in the paper at the meeting in Cumberland. I can hardly believe that an elected official would speak so arrogantly to the public. We are taxpayers and deserve to see our tax dollars spent in an efficient and judicious manner.

Going through southern corridors where vacant land lies will be expedient and less confrontational. I trust that you will represent the interests of those concerned in a professional and wise manner.

I look forward to your reply,

#40

The editor Ottawa Citizen
Marie-France Lalonde MPP
Mayor Jim Watson
Steven Blais, Robert Monette, Jodi Mitic counsellors
13 February 2015

Re:LRT East and Expansion of Rte 174 East

I recently attended the city’s demonstration of the proposed LRT and expansion of the highway east from Orleans. I came away not only disappointed but astonished at the lack of foresight at some of the decisions. We were encouraged to respond and submit our ideas to Council. I have deliberated, talked to others about the plans and finally have decided that I need to put my views into writing.

Primarily I have *two major concerns*:

1. Lack of current/ planned future parking for the LRT
2. Proposed expansion of 174

1. Lack of Parking

With respect to the LRT parking, there is already a dearth of parking spaces. The intent is to eliminate all of the crosstown buses and expand ridership with faster, larger payload trains. If a report on CTV news this evening is accurate, ridership is currently down but try to find a spot in one of the three areas for Place d’ Orléans bus commuters. There are empty land spaces north of the current *Park-and-Ride* lot off Champlain. If the planners intend on expanding ridership I suggest that they immediately start planning for a parking garage/lots in that area.

2. Proposed expansion

While the LRT plans at least *appear* thoughtful with a compromise ‘middle lane’ approach, the same cannot be said about the thought that went into expansion of ‘highway 174.’ Three proposals were apparently put forward for the panel to investigate. Two of the proposals show routes going south from Trim beyond Cumberland and Rockland. These leave the current 174 as well as the centre of Cumberland and Rockland relatively untouched as the roads go through vast stretches of current farm or fallow county property around both Cumberland and Rockland.

Firstly, it is absurd to try to put an expanded route in an ecologically sensitive area so close to the river (think salt and chemicals from the highway). Anyone notice the ‘turtle crossing’ signs?

Secondly, a *true* visionary might say “If you build it they will come.” An *average* visionary would examine the plans that were proposed 20 years ago under the previous provincial Conservative government calling for ‘ring roads’ more to the south using Innes, Frank Kenny or other roads (south of Wilhaven in Cumberland) as the alternatives suggest. To the current taxpayers these plans would offer the minimum of disruption; to future residents it would be most amenable to future growths. It would make sense to build the roads getting through these areas **PRIOR** to the building of the residences, schools etc.

Looking at the recommended proposal, it would seem that the panel of experts who came up with this idea must believe the opposite. It seems they based their choice on “Let’s disrupt as

many of the current residents as possible." They must have used the model of the building of the Metropolitan (Route 40) through Montreal that successfully built a mobile parking lot.

The panel decided that the million dollar houses that were allowed to build along the shore of the river in the past 20 years would eagerly accept the idea of a 4 lane highway barreling by their residences. The rebuttal to this point came from the chair of the expansion committee who said "We are planning on a service road." Where do they expect to put the service road? North of the residences in the middle of the river? Over the current electric lines south of the current road (try getting approval for that!)? Further south through the existing Camelot golf course? Any and all of these would see court challenges lasting years from the current residents, the members of the golf course etc.

Of course, the panel said that **no shovel would be put towards the expansion before 2031!!!** Have any of the members of the panel spent 5 minutes sitting along the current highway during either of the 'rush hours'? I doubt it. Because, the line starts at Trim Road and is bumper to bumper to Rockland from 7-9:00 am and reversed during the afternoon drive home. This week the traffic signal at 174 and county road 35 did not work. The lineups were incredible!

And 2031(?)! I looked around the room and with few exceptions, it could be safely assumed that the vast majority of the attendees that evening would have little concern with driving by then except perhaps to pass the required driving tests for individuals over 80 years of age.

Conclusion

Before city council accept these plans (and provincial and federal governments are asked for more funds to back these ideas) clearly something must be done NOW that will minimize the costs to the present residents and maximize the growth that is anticipated for future residents. I propose that the current Rte 174 be 'uploaded' to the provincial government and immediate action be taken to expand parking facilities along the current bus routes. In addition, the plans to move the highway further east take a turn off Trim and avoid the congestion, confusion and court challenges that are to be expected with the panel's choice.

If the council wants to increase ridership now and in the future it must take action now. I have much confidence in the work of Counsellors Blais, Monette and Mitic and trust in their abilities to make these changes happen. I challenge the mayor and the MPP for support!

Ottawa

Prescott
Russell
Faircl Feb 12/15-

#41

Confederation Line East LRT Extension/ Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orleans

Thursday, February 5, 2015
Guy Faubert Hall
954 Groulx Street, Rockland

I was at this meeting.

Tell us what you think...

- West Trim
- No Microphone - Large Crowd - Could not hear Sheme! You would think the city of Ottawa would own a microphone and someone would be smart enough to bring it.
 - I think the street car west of Trim should be running South on transit right of way to Milenium Park @ Trim Not Along 174. That way you feed it from both sides North & South
 - How are people going to get off of it if it stops between Stations, 174 would have to be shut down to load people onto buses.
 - Emergency turn arounds would be gone - longer response for Amb, Police & Fire because they would have to go to next exit. Fire Crews - Could not use medium. Major Problem
 - Growth is to the South. Not North of 174
 - East of Trim in my opinion should be 3 Ave 3B. A New connection to the South of Rockland makes much more sense than running along river in #1. Once again it can be used from both sides the new road east of Beaton should curve further south to keep away from Residents at Beaton's creek & willow area.
 - I totally disagree with the preferred routes the city is
 - Parking - and Most of people at meeting seemed to disagree too

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by February 13, 2015 to:

Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier Ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Mardi 3 février 2015	Mercredi 4 février 2015	Jeudi 5 février 2015
Centre comm. Comm. R. J. Kennedy 1115, chemin Dunning, Cumberland	Complexe récréatif Bob MacQuarrie 1490, promenade Youville, Orléans	Salle Guy Faubert 954, rue Giroux, Rockland

Dites-nous ce que vous pensez...

Avoir des routes qui répondent aux besoins est très important. Cependant avoir une route à capacité plus forte va déplacer le problème. Plus de voitures vont l'emprunter et c'est le centre-ville qui aura l'engorgement avec toutes ces voitures.

Privilégier le transport en commun. Le train est excellent. Pour le moment, les autobus sont là. Un élément clé pour augmenter l'utilisation du transport en commun — sans oublier l'importance de la sécurité et de la fiabilité du service — cet élément clé est le coût. Au lieu d'augmenter, ce coût devrait diminuer pour l'usager.

On subventionne largement la construction de route, le transport en commun devrait l'être aussi.

Toutes vos études sont intéressantes, nécessaires aussi, car il faut connaître l'état des choses, la réalité. Mais ça ne veut pas dire qu'il faut les faire avorter.

Veuillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 13 février 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADRESS :

TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement de la Ligne Trillium. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Mardi 3 février 2015	Mercredi 4 février 2015	Jeudi 5 février 2015
Centre comm. Comm. R. J. Kennedy 1115, chemin Dunning, Cumberland	Complexe récréatif Bob MacQuarrie 1490, promenade Youville, Orléans	Salle Guy Faubert 954, rue Giroux, Rockland

Dites-nous ce que vous pensez...

Merci pour votre excellente présentation à Rockland le 5 février 2015.

Je favorise l'extension du chemin Innes jusqu'à Baseline.
1° Cela nous offre une autre sortie de l'aire urbaine à Rockland.

2° Cette option coûte moins cher que toutes les autres.

3° La désignation route touristique de la route 174 est un choix sage, qui saura promouvoir le tourisme dans l'est d'Ontario.

4° Il faut éviter de faire un choix politique. M. Jean-Marc Zolonde défend depuis trente ans l'élargissement de la 174. Il en a fait sa mission personnelle et le centre de son programme électoral. Il a tort! Nous devons faire un choix intelligent avec une vision d'avenir.

5° Une route au centre de Clarence-Rockland va retirer la vedette à Rockland et offrir une solution accessible à tous les citoyens. Cette route au centre de la cité favorisera le développement immobilier vers le sud.

Veuillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 13 février 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADRESS :

TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement de la Ligne Trillium. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca



#44

Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Tuesday, February 3, 2015
R. J. Kennedy Mem. Comm. Centre
1115 Dunning Road, Cumberland

Wednesday, February 4, 2015
Bob MacQuarrie Recreation Complex
1490 Youville Drive, Orléans

Thursday, February 5, 2015
Guy Faubert Hall
954 Giroux Street, Rockland

Tell us what you think...	
<p>I think you have done a great job in planning the O-train going down the centre between Hwy 174 to Place Orleans with barriers. However the residents backing onto Hwy 174 already have problems with noise, pollution, and the house shaking when buses, and large transport trucks go by. A barrier down the center near the O-train is not enough. We have black soot on the tables in the back yard. What is this doing to our lungs? Noise level that is constant bringing stress levels up, and vibrations that shake the house is not healthy for any of us.</p> <p>I looked into barriers walls a few years back and was told barriers would be only constructed on the parts the provincial gov't owned. Our section of Hwy 174 belonged to Ottawa, hence, we did not get any.</p> <p>If you are going to build a 3 lane on each side of the Hwy 174 plus the O-train barriers need to be constructed on each side of the hwy as well as the barriers with the O-Train.</p> <p>If the health concerns (pollution, noise, vibrations) are addressed with proper barriers I would be in agreement with your plans.</p>	
<p>Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by February 13, 2015 to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1 Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca</p>	
Optional information:	
NAME:	E-MAIL:
ADDRESS:	PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

Appendix D: Frequently Asked Questions

Question		Answer
General Questions		
Why are we studying the widening of Highway 174, CR 17 and the Eastern LRT?	<p>The 2013 Transportation Master Plan (TMP) has identified a shortfall in transportation capacity for residents of the City of Ottawa and the United Counties of Prescott-Russell, primarily to access jobs and activities in the central area of Ottawa. In addition to providing additional transit capacity to meet the objectives of the TMP, additional road capacity is also required.</p> <p>Through the corridor the transportation model indicates that one additional lane of road capacity is needed in each direction. This could be either a new two-lane roadway, or a widening of the existing roadway. The model also indicates that the majority of these trips want to access Highway 174 at Trim Road and Highway 417 west of the split.</p> <p>Road safety is another concern on the existing road. Adding the additional lanes will allow for many of the safety issues to be addressed. Dividing the highway to prevent vehicles from crossing over the centre line and finding ways to consolidate driveway accesses will improve safety.</p> <p>There is less travel demand across the City by a southern route than travel demand to the downtown area and areas close to Highway 417 via Highway 174 and Highway 417. Routes that connect south of Highway 174 along Trim Road will require additional capacity to manage the downstream traffic volumes. Development continues in the east end of the City of Ottawa and in counties outside the City, placing pressure on the transportation network to accommodate more trips.</p> <p>West of Trim Road, the implementation of LRT from Blair Station east to Place d’Orléans (and eventually to Trim Road) will provide additional travel capacity and connection to a widened 174/17 will help to improve ridership.</p>	
Why was the 174/17 corridor selected over the others considered?		
What is the timeline for the LRT extension and the widening of Highway 174 and CR 17	<p>The Confederation Line East LRT Extension is a Stage 2 project planned for 2023, subject to funding availability. The widening of 174/17 is not currently identified in the City of Ottawa’s Affordable Plan to 2031 and will be dependant on funding availability.</p>	

Will noise be an issue?	The LRT will have a continuously welded track, is electrically driven, and will be generally quiet. The change in road proximity and traffic volumes will affect noise levels. Impacts are being investigated in greater detail, along the full length of the corridor and mitigation measures will be provided where warranted.	
Eastern Light Rail Transit		
How were the station sizes, design and locations determined?	Stations are located based on a number of criteria including connection to north-south bus routes (at arterial road crossings of the corridor), serving existing and projected ridership, and a spacing that maximizes the number of people who can access the station within a 600 metre walk zone. There will be an emphasis on connectivity, accessibility and personal safety by designing stations that provide good connections to local bus routes, sidewalks, pedestrian pathways, and bicycle facilities. Stations will be fully accessible to passengers with disabilities through the use of elevators, logical arrangement of station elements and clear, concise signage. Creating logical stations with clear sight lines assist all members of the community by creating safer places.	
How will this system be paid for?	The capital cost to build the system will be part of the City’s Stage 2 LRT implementation and the City will be seeking funding partners with upper levels of government. On-going operating costs, including increases or decreases in service levels to meet ridership demands, will be part of OC Transpo’s on-going budgetary process.	
Why not extend the LRT to Trim Road and beyond?	Implementation of the LRT is based on the City of Ottawa’s Affordable Plan to 2031 as well as projected ridership demand. The City’s TMP currently identifies LRT implementation terminating at Place D’Orleans before 2031 with the extension to Trim Road post 2031. Beyond Trim Road, there is insufficient ridership to extend the LRT any further. We are protecting for a bus rapid transit corridor east of Trim to Frank Kenny to connect Eastern communities with the LRT.	
How will the transit system operate when the LRT opens?	<p>LRT trains will be completely segregated from other traffic, allowing them to move on a regular and predictable timetable, making trips fast and reliable.</p> <p>The bus network currently has a set of local, semi-express and express routes designed to take passengers downtown in the morning (and home in the evening) and serve the major node at Place d’Orléans during the midday and evening. The LRT will require a reorganization of the route network, including:</p> <ul style="list-style-type: none">• Bus routes will take peak period passengers to the nearest LRT station• Few buses will travel across the greenbelt and further west	

	<ul style="list-style-type: none">Enhanced local service will meet increasing demandTrains will stop at all stations allowing for passenger transferWhile bus routes will not be timed to the arrival or departure of trains these connections will be as direct as possible with climate-controlled areas <p>CRT buses from Clarence-Rockland will have the ability to connect to the easternmost LRT station, allowing passengers to transfer to the LRT to travel into downtown.</p>
Where will there be Park and Ride or Kiss and Ride facilities?	Currently there are two Park and Ride locations at Trim Road and at Place d’Orleans. These will be maintained and expanded if possible. Kiss and Ride in some form will be considered at every station.
Widening of Highway 174 / County Road 17	
Will the new highway lanes be for all drivers or only vehicles with more than one person in them?	The environmental assessment will identify the impacts of the largest footprint. This will include the additional lane width and buffer width that is needed to protect for High Occupancy Vehicle (HOV) lanes for vehicles with more than one person. If the City decides to make the lanes available to all drivers, the protected right-of-way will be wide enough and will not need additional approvals.
Won’t the arterial roads that connect to the highway become overloaded if the highway is widened?	Our modelling work indicates that the arterial roads can adequately handle the additional traffic moving to and from the widened highway, although the level of congestion is likely to increase as travel demand increases.
How is the widened highway being designed to make travel as safe as possible?	Current design standards are being applied. With the LRT being placed in the median (from Montreal Road to the eastern terminus), the road traffic will be separated from the trains by concrete barriers and safety fences. Visibility, design speed, lighting and safety measures are being designed into the plans.
Was a southern ring road considered?	A ring road has been investigated by both the MTO and the City over the years and modelling work indicates that it would be poorly used. A ring road would be very expensive and currently neither the City nor MTO are carrying it forward for development.
Was a three-lane highway with reversible lanes considered?	The City conducted a review and assessment of whether reversible lanes would be suitable in this context. Safety and cost considerations indicate that a reversible lane is not appropriate along Highway 174 and County Road 17. The costs of putting overhead signage that indicate the direction of traffic in the centre lane would be expensive. Furthermore, for safety reasons, we need to put a barrier between lanes of different directions. Using reversible lanes over long distances, with driveway accesses and at highway speeds is not a feasible option.

Is more development being allowed in the corridor?	<p>Growth continues across the Region, however the City has placed a moratorium on lot severances and new access points to Highway 174. Existing lots that are not yet developed may be allowed a new access, but wherever feasible the City’s preference is to consolidate these potential new entrances with existing entrances.</p> <p>A study similar to the Greater Toronto Area’s “Places to Grow” is being considered for Eastern Ontario, which would look at where development should occur. There is a lot of good agricultural land in this area, and if we build a road, there would be pressure to develop the surrounding areas and we could lose this agricultural land.</p>
Will property by expropriated to widen the 174/17?	There are many places along the corridor where we have sufficient right-of-way. For those portions where property is required to expand the right-of-way, then we will have a discussion with those residents who will be impacted.

APPENDIX E: PRESENTATIONS AND DISPLAY MATERIAL

Confederation Line East LRT Extension & OR 174/CR 17

- Environmental Assessment Studies

Consultation February 2015

ottawa.ca
3-1-1
TTY/ATS 613-580-2401

Study Area

ottawa.ca
3-1-1
TTY/ATS 613-580-2401

Presentation Overview

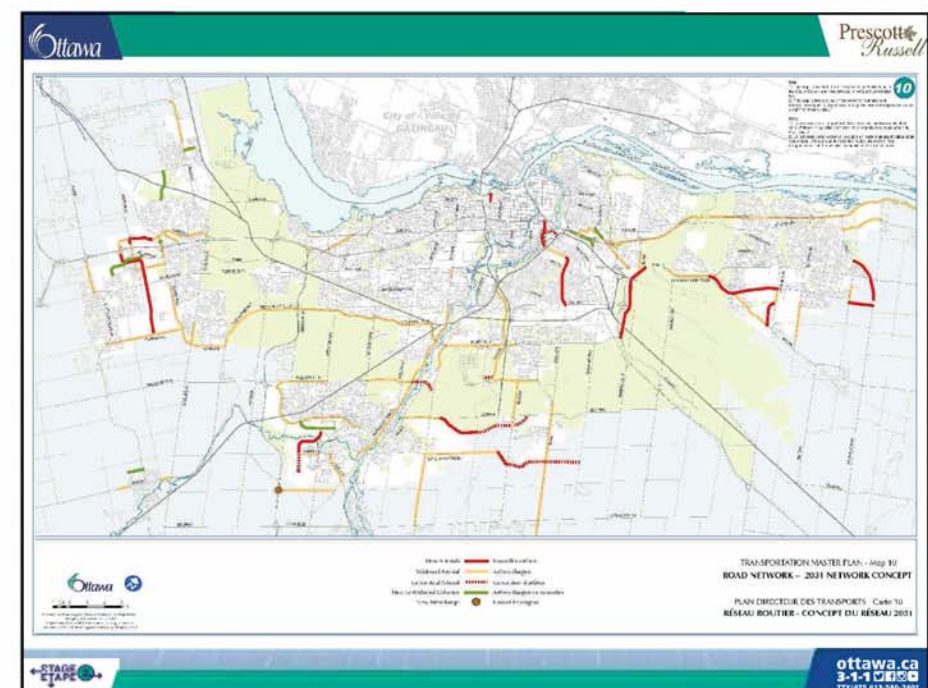
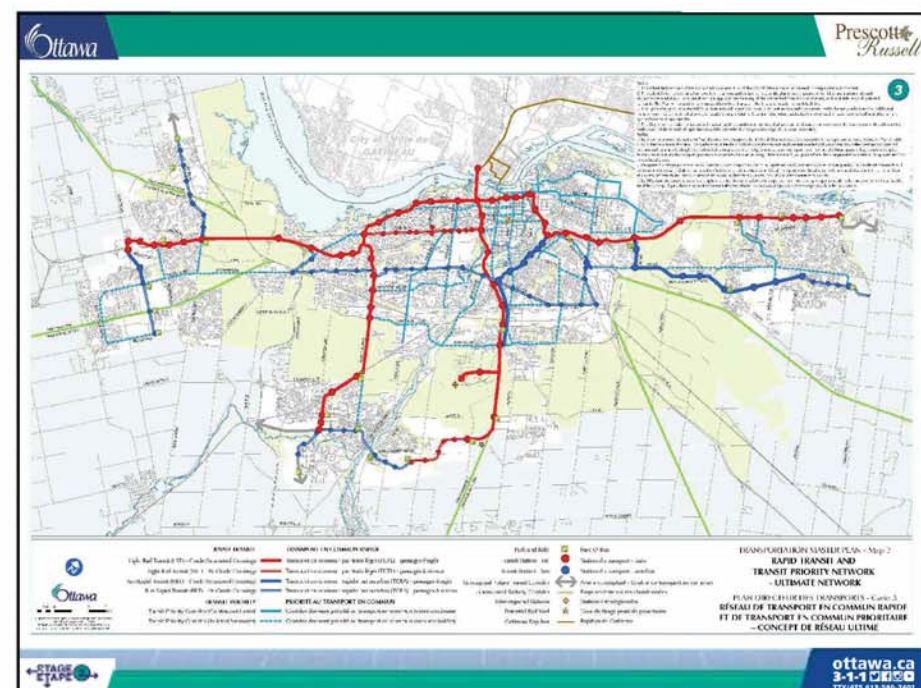
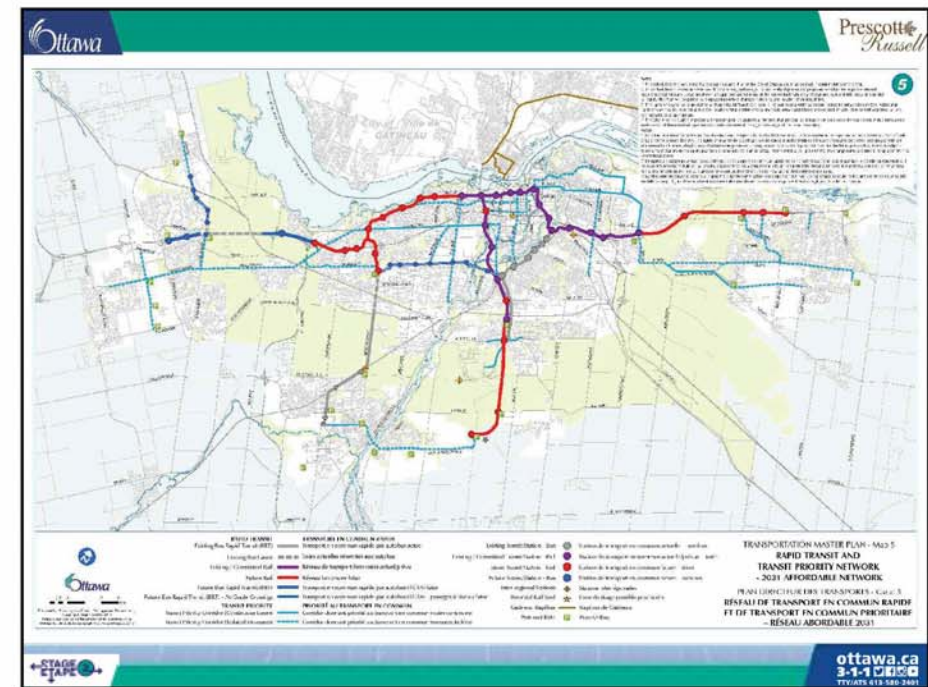
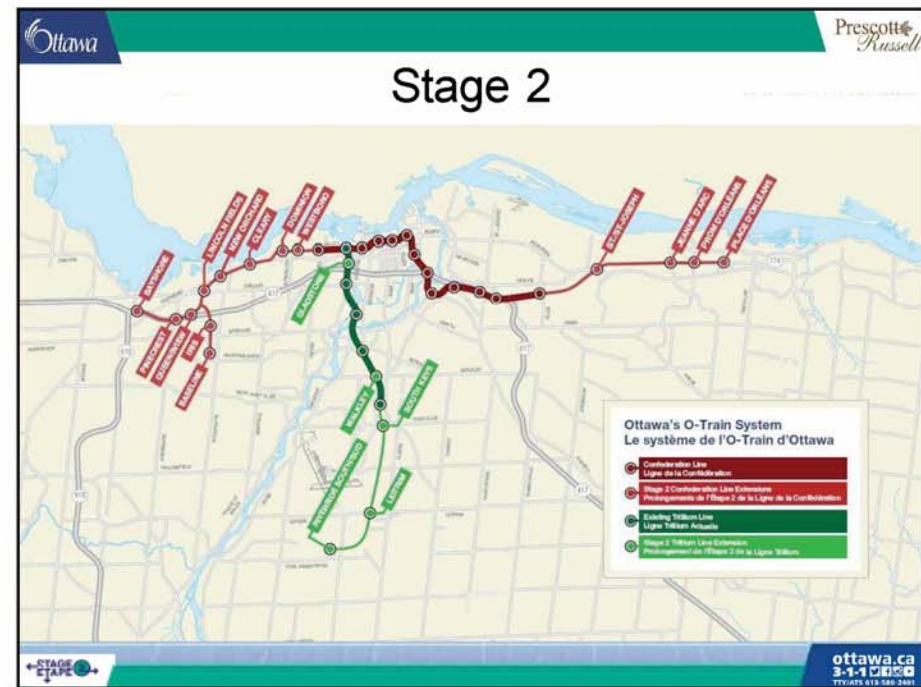
- Study objectives
- Study process and timelines
- Evaluation methodology
- West of Trim – alternatives and preferred plan for LRT and OR174 widening
- East of Trim – alternatives and preferred route
- Next Steps

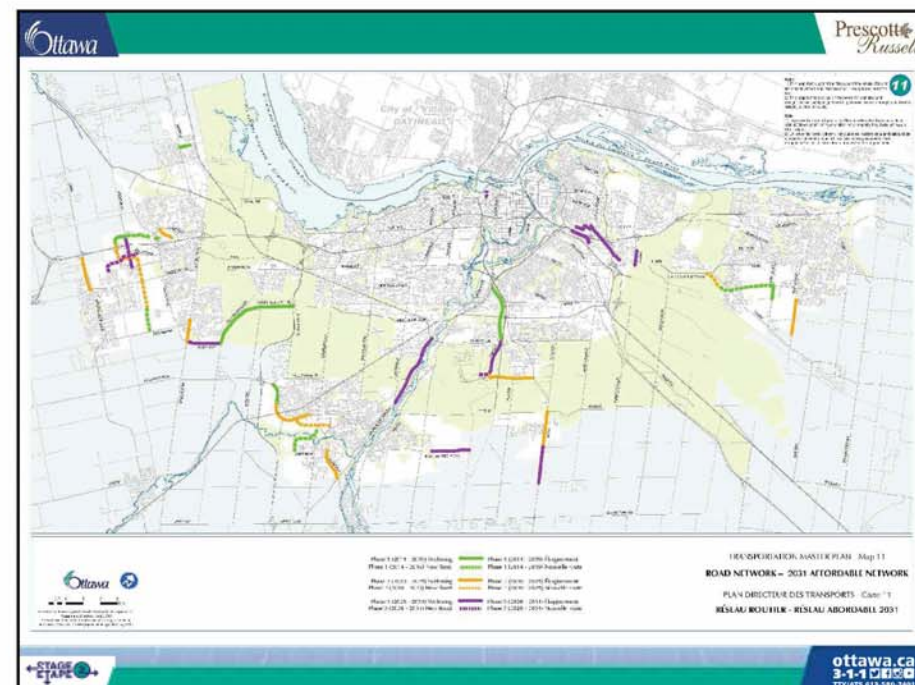
ottawa.ca
3-1-1
TTY/ATS 613-580-2401

Study Objectives

- Obtain EA approval for :
 - Extending LRT east from Blair station to Trim station with 8 stations
 - Widening the OR 174 from the Split/Hwy 417 to Trim Road
 - The preferred route and design from Trim Road to Landry Road

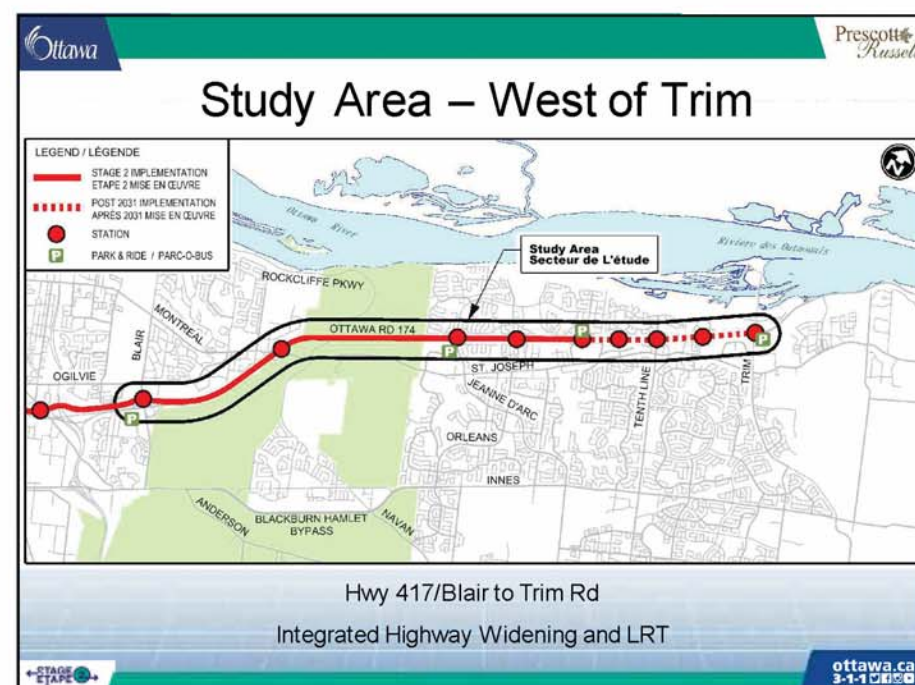
ottawa.ca
3-1-1
TTY/ATS 613-580-2401



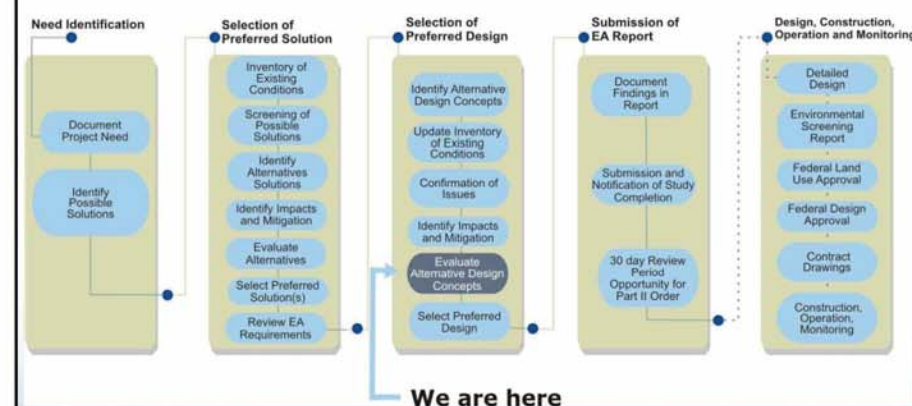


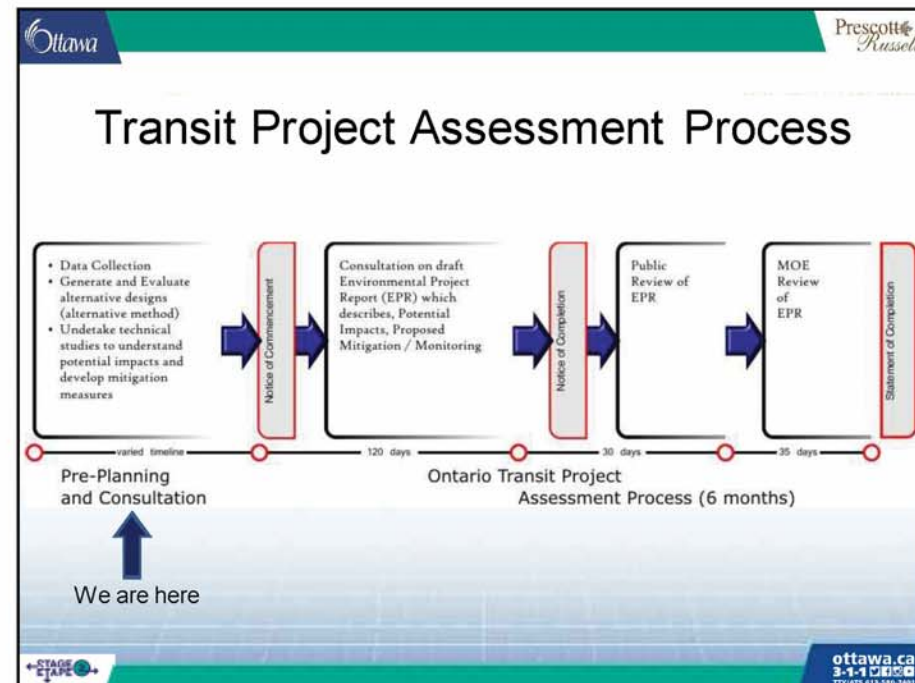
OR 174-CR 17 and ELRT Integration

- LRT to run along OR 174 from Blair station to Trim Road
- Coordinated approach to both EA studies needed to:
 - Maximize the use of the existing road corridor
 - Minimize impacts on the environment
 - Minimize construction cost
 - Build on existing study information
 - Integrate stakeholder consultation activities
 - Streamline EA process



Municipal Class EA Process





Ottawa Prescott & Russell

Evaluation Methodology

- Planning tool used to evaluate the alternative solutions
- Methodology will address the following objectives:
 - **Use of Criteria** as the basis for the evaluation of the alternative solutions
 - **Weighting or Ranking of Criteria** to reflect the of importance of the criteria/indicators
 - **Sensitivity Analysis** to test the results of the evaluation and to verify the influential factors in the results of the evaluation
- Concordance Method chosen for this evaluation
- Involves:
 - Ranking the alternatives according to defined indicators and measurements
 - Weighting the indicators and measurements
 - Calculating scores and adding the scores in a matrix

ottawa.ca 3-1-1 3-1-1 3-1-1

Ottawa Prescott & Russell

Evaluation Methodology

ottawa.ca 3-1-1 3-1-1 3-1-1

Ottawa Prescott & Russell

West of Trim

- Alternative Alignments
- Evaluation Criteria
- Criteria Group Weights
- Evaluation Results
- Sensitivity Analysis
- Overall Performance Review
- Preliminary Preferred Alternative

ottawa.ca 3-1-1 3-1-1 3-1-1

Ottawa

Prescott & Russell

Alternatives – West of Trim

- North or south side station and runningway

STAGE 2

ottawa.ca
3-1-1
TTY: 613 580-2461

Ottawa

Prescott & Russell

Three LRT Alignment Alternatives

STAGE 2

ottawa.ca
3-1-1
TTY: 613 580-2461

Ottawa

Prescott & Russell

Alternatives – West of Trim

- Median station and runningway

STAGE 2

ottawa.ca
3-1-1
TTY: 613 580-2461

Ottawa

Prescott & Russell

Three LRT Alignment Alternatives

STAGE 2

ottawa.ca
3-1-1
TTY: 613 580-2461

Ottawa

Prescott Russell

Three LRT Alignment Alternatives

STAGE 1

ottawa.ca
3-1-1

Ottawa

Prescott Russell

Three LRT Alignment Alternatives

STAGE 1

ottawa.ca
3-1-1

Ottawa

Prescott Russell

Three LRT Alignment Alternatives

STAGE 1

ottawa.ca
3-1-1

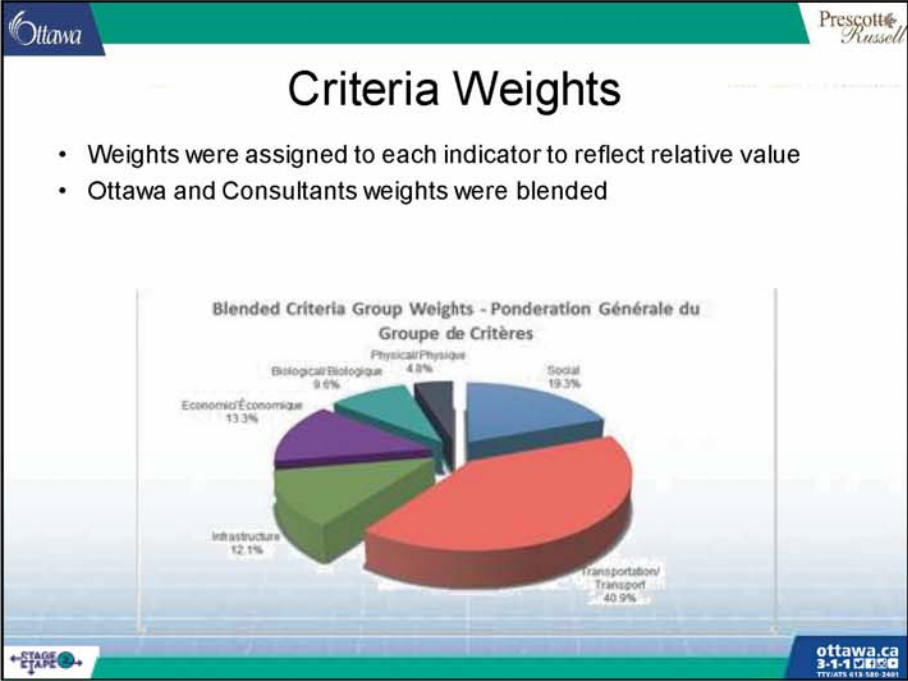
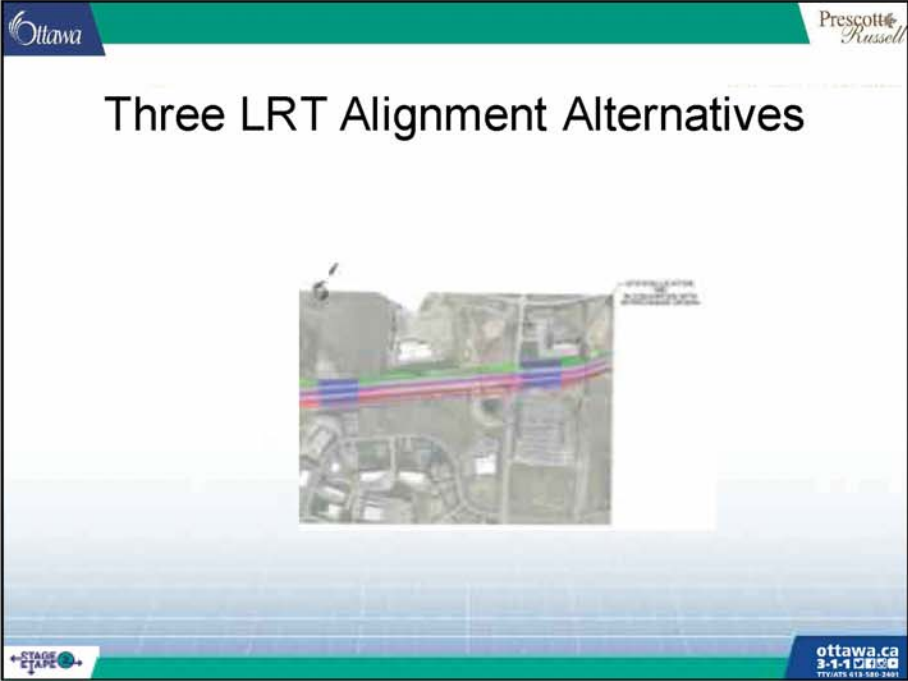
Ottawa

Prescott Russell

Three LRT Alignment Alternatives

STAGE 1

ottawa.ca
3-1-1



- Ottawa Prescott & Russell
- # Evaluation Criteria
- Social**
 - Archaeological resources
 - Noise
 - Property Impacts
 - Compatibility with Adjacent Communities
 - Views and Vistas
 - Smart Growth
 - Infrastructure**
 - Major Municipal Infrastructure
 - Utilities
 - Major Structures
 - Cost**
 - Capital costs
 - Operating costs
 - Transportation**
 - Safety
 - Transit Network Function
 - Transit Ridership
 - Transit Stations
 - Pedestrian and Cyclists
 - Road Network Function
 - Biological**
 - Natural Heritage features
 - Aquatic
 - Wildlife
 - Species at Risk
 - Physical**
 - Slopes and ravines
- ottawa.ca
3-1-1
TTY: 613 580-2461

Ottawa Prescott & Russell

Evaluation Results

Integrated Alternatives	Alternatives			Sum	Rank
	North LRT	South LRT	Median LRT		
North LRT		0.59	0.10	0.70	2
South LRT	0.41		0.21	0.62	3
Median LRT	0.90	0.79		1.68	1

Details of the Concordance Tables for the evaluation are at the Resource Table

ottawa.ca
3-1-1
TTY: 613 580-2461

Ottawa

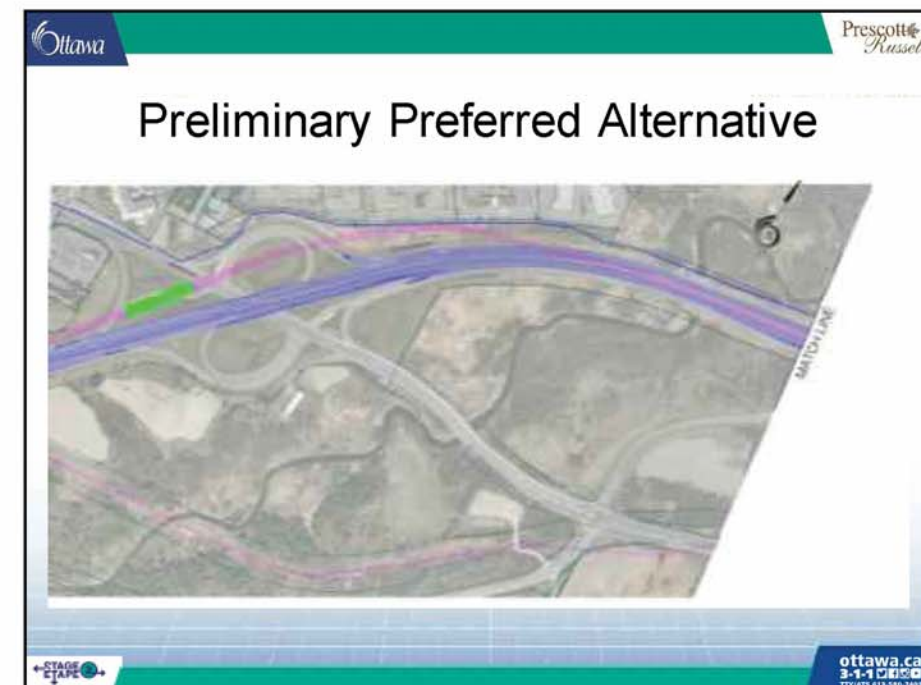
Prescott & Russell

Sensitivity Testing

Sensitivity Test Description:		Alternative Concordance Results			Preferred
		North	South	Median	
City of Ottawa Weights	Sum	0.64	0.71	1.65	Median
	Rank	3	2	1	
Consultant Team Weights	Sum	0.75	0.54	1.71	Median
	Rank	2	3	1	
Blended Weights (City of Ottawa and Consultant Team)	Sum	0.70	0.62	1.68	Median
	Rank	2	3	1	
All Weights Equal	Sum	0.74	0.60	1.66	Median
	Rank	2	3	1	
No Social considered	Sum	0.68	0.57	1.74	Median
	Rank	2	3	1	
No Transportation considered	Sum	0.86	0.42	1.72	Median
	Rank	2	3	1	
No Infrastructure considered	Sum	0.62	0.68	1.70	Median
	Rank	3	2	1	
No Cost considered	Sum	0.68	0.68	1.63	Median
	Rank	2	2	1	
No Biological considered	Sum	0.71	0.65	1.65	Median
	Rank	2	3	1	
No Physical considered	Sum	0.68	0.65	1.67	Median
	Rank	2	3	1	

STAGE STAGE

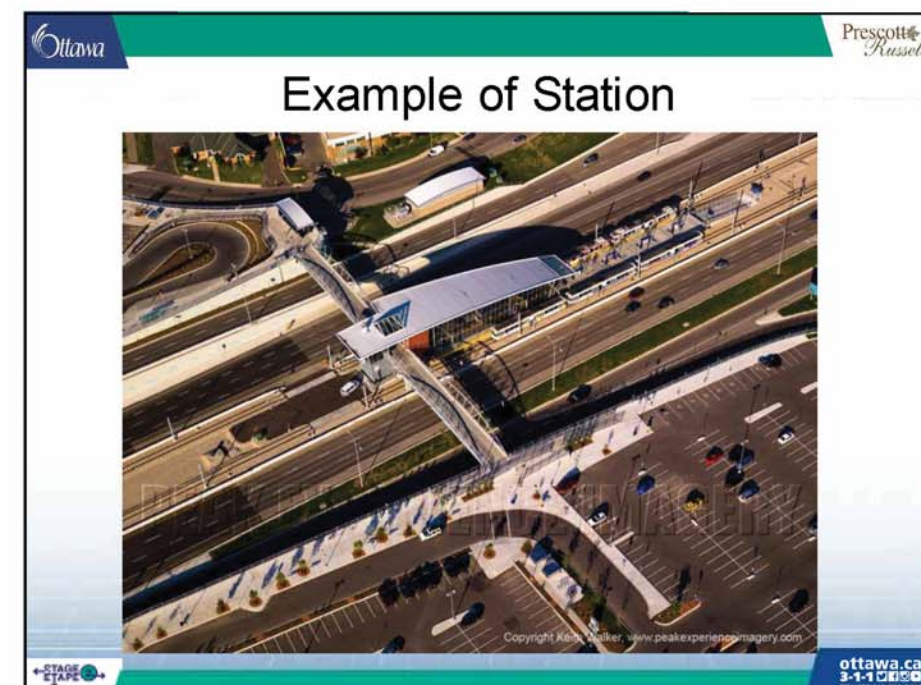
ottawa.ca
3-1-1
TTY: 613 580-2461

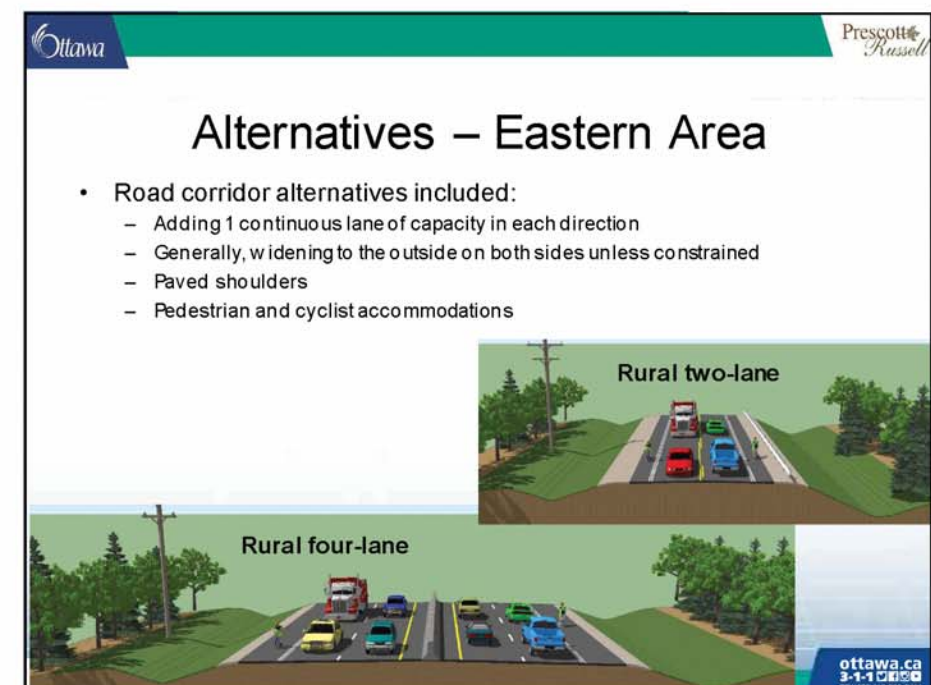
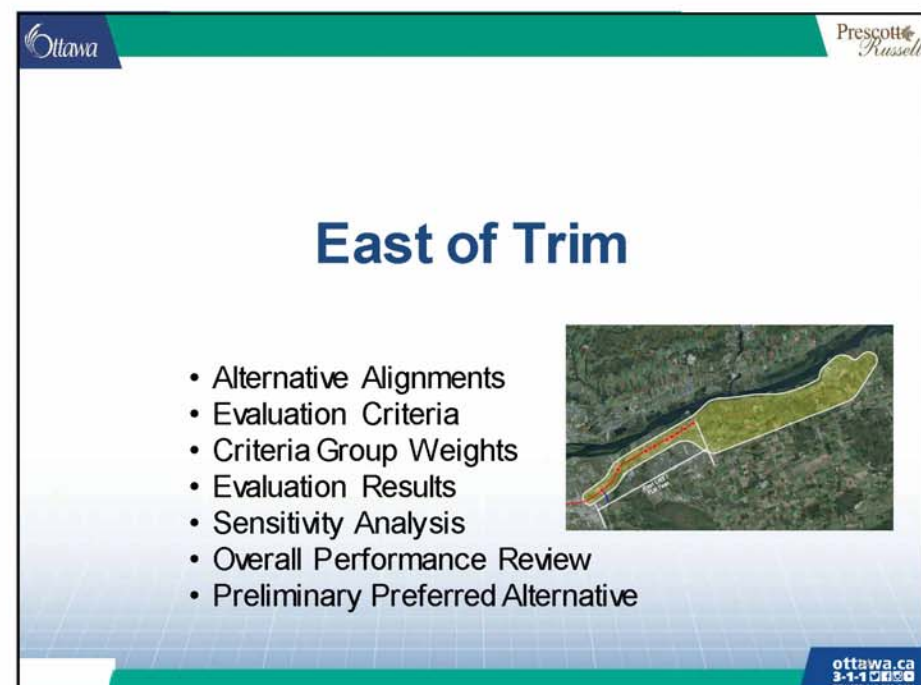
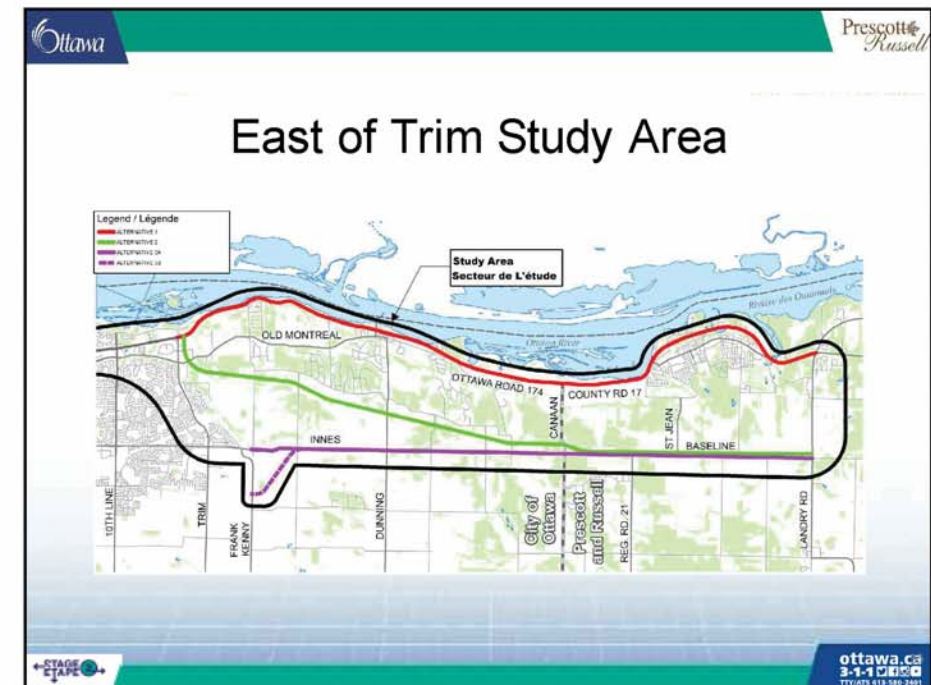


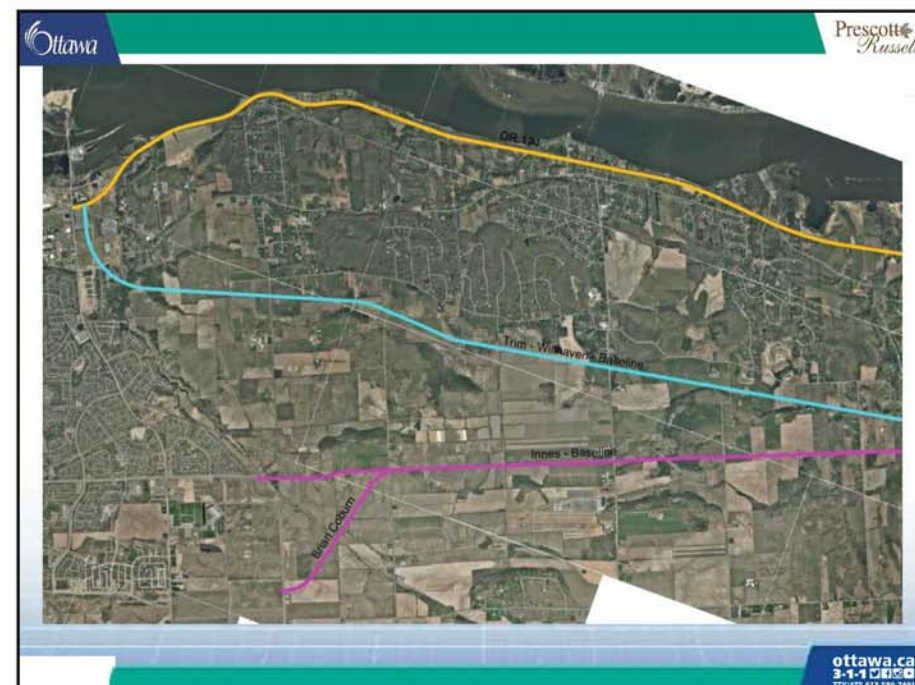
- Ottawa

Prescott & Russell
- Hybrid Alignment
- Hybrid alignment reviewed to further improve alignment
 - Hybrid solution uses north corridor to Greens Creek area then transitions into the Median
 - Advantages of hybrid solution:
 - Close to residential and employment areas from Blair to Montreal Road
 - Promotes transit oriented development
 - Better connections to multi-use pathways
 - Fewer structures required
 - Maintains the existing Transitway overpass during construction
 - Results in fewer construction staging impacts on both existing transit and vehicular traffic
- STAGE STAGE

ottawa.ca
3-1-1
TTY: 613 580-2461

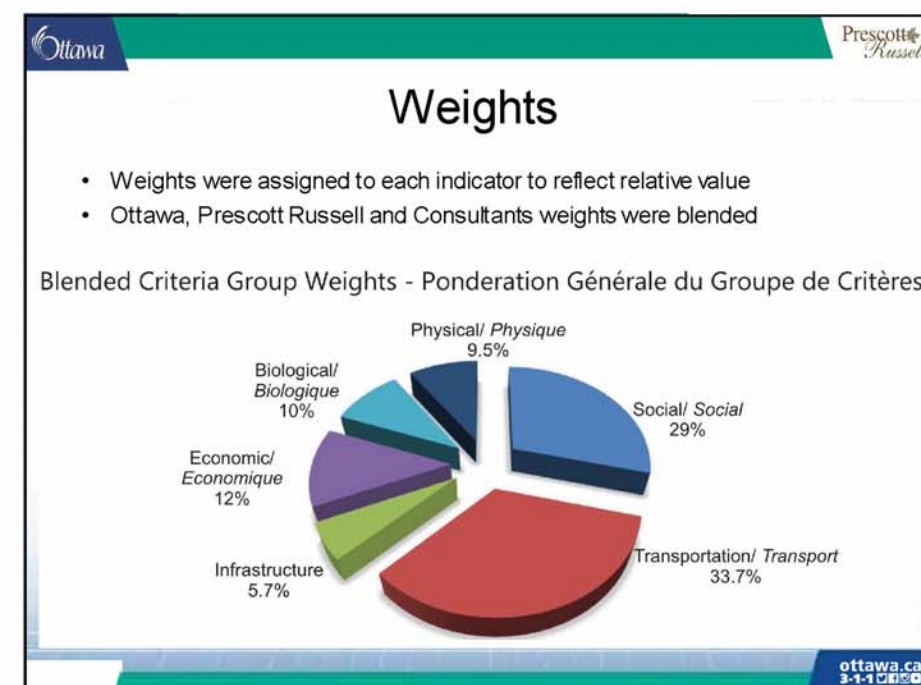
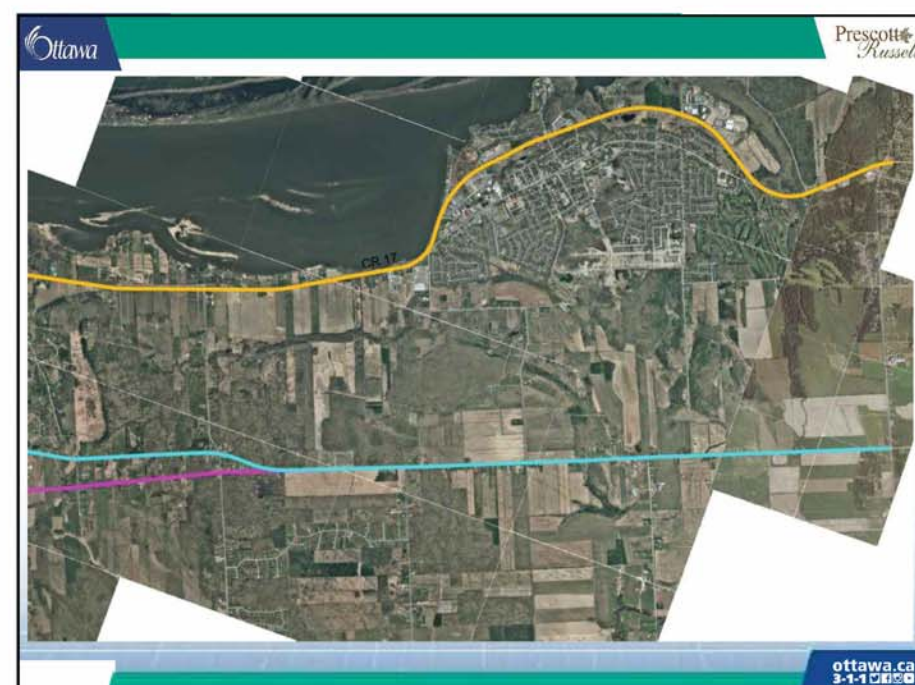






Evaluation Criteria

- **Social Environment**
 - Impact On Existing Residences / Communities
 - Impact On Planned Development
 - Impact On Recreational Features
 - Impact On Archaeological Resources
 - Cultural Resources
 - First Nations
 - Impact On Agricultural Resources
 - Noise
 - Air Quality
 - Views And Vistas
 - Compatibility With Municipal Planning Goals And Objectives
- **Infrastructure**
 - Major Infrastructure Impacts
- **Transportation**
 - Traffic Performance (autos)
 - Road Network Function
 - Safety
 - Transit Network Function
 - Pedestrian Network
 - Cycling Network
 - Ferry
- **Economic Environment**
 - Businesses
 - Capital costs
- **Biological**
 - Aquatic
 - Natural Heritage features
 - Wildlife
 - Species at Risk
- **Physical**
 - Geology
 - Groundwater
 - Potentially Contaminated Sites



Ottawa

Prescott & Russell

Evaluation Results

Alternatives	Alternatives				Sum	Rank
	1	2	3A	3B		
Alt. 1: Widening of OR174-CR 17		0.62	0.63	0.63	1.88	1
Alt. 2: New Route from Trim to Baseline south of Wilhaven	0.38		0.27	0.31	0.95	4
Alt. 3A: New connection from Innes to Baseline Road	0.37	0.73		0.56	1.67	2
Alt. 3B: Innes connecting to Baseline & Brian Coburn Blvd	0.37	0.69	0.44		1.50	3

Details of the Concordance Tables for the evaluation are at the Resource Table

ottawa.ca

3-1-1

TTY: 613 580-2461

Ottawa

Prescott & Russell

Preliminary Preferred Alternative

- Widening of OR 174-CR 17 is preferred
 - Best addresses transportation needs – most direct route
 - Serves the predominant flow of traffic
 - Best connection to road and transit network - ELRT
 - Supports growth and sustainability policies
 - Provides opportunity to improve pedestrian and cycling facilities where needed
 - Supports re-design and improvement of the road environment through communities
 - Allows for consideration of HOV lane designation – promotes Clarence-Rockland transit
 - Does not add infrastructure to undeveloped natural areas
- Mitigation measures will be developed for potential impacts

ottawa.ca

3-1-1

TTY: 613 580-2461

Ottawa

Prescott & Russell

Sensitivity Testing

Sensitivity Test Description:		Alternative Concordance Results				Preferred:
		1	2	3A	3B	
Average Proponent Weights	Sum	1.88	0.95	1.67	1.50	Alternative 1
	Rank	1	4	2	3	
United Counties Weights	Sum	1.88	1.03	1.62	1.47	Alternative 1
	Rank	1	4	2	3	
City of Ottawa Weights	Sum	1.88	0.89	1.70	1.53	Alternative 1
	Rank	1	4	2	3	
Specialists Weights	Sum	1.58	0.98	1.80	1.65	Alternative 3A
	Rank	3	4	1	2	
Proponents & Specialists Weights	Sum	1.79	0.97	1.71	1.54	Alternative 1
	Rank	1	4	2	3	
No Social considered	Sum	1.68	1.08	1.63	1.61	Alternative 1
	Rank	1	4	2	3	
No Transportation considered	Sum	1.28	0.87	2.03	1.81	Alternative 3A
	Rank	3	4	1	2	
No Infrastructure considered	Sum	2.0	0.94	1.61	1.44	Alternative 1
	Rank	1	4	2	3	
No Economics considered	Sum	2.15	0.93	1.55	1.36	Alternative 1
	Rank	1	4	2	3	
No Biological considered	Sum	1.94	0.94	1.64	1.48	Alternative 1
	Rank	1	4	2	3	
No Physical considered	Sum	1.98	0.96	1.63	1.43	Alternative 1
	Rank	1	4	2	3	

ottawa.ca

3-1-1

TTY: 613 580-2461

Ottawa

Prescott & Russell

Community Enhancements

- Reduce speeds
- Pedestrian and cycling facilities
- Pedestrian crossing opportunities
- Provide river access
- Improve lighting
- Provide gateway features to the Nation's Capital

ottawa.ca

3-1-1

TTY: 613 580-2461

Typical Rural Cross Section

Design considerations:

- Median barrier to improve safety
- Enhanced paved shoulders
- Intersection design to manage speed

ottawa.ca
 3-1-1
 TTY/ATS 613-580-2401

Closing

ottawa.ca
 3-1-1
 TTY/ATS 613-580-2401

Refinements/Potential Mitigation Measures



- Brief overview of measures:
 - Roundabouts, streetscaping for speed management
 - Enhance/protect wildlife habitat
 - Build critter crossings
 - Upgrade existing culverts and structures
 - Maintain traffic/transit operations and property access during construction
 - Provide multi-use pathways, enhanced shoulders

ottawa.ca
 3-1-1
 TTY/ATS 613-580-2401

Next Steps

- Confirm preferred alternatives based on feedback received
- Develop design details:
 - Transit station locations and design
 - Interchange configurations including Trim Road-174 grade separation
 - Intersection design (roundabouts versus signals)
 - Cumberland Village and Rockland cross sections
 - Pedestrian and cycling connections
- Review and assess mitigation measures
- Next public consultation late spring 2015
- Present to Councils/Committees
- Public review of study documentation

ottawa.ca
 3-1-1
 TTY/ATS 613-580-2401





Questions?

Thank You

For project updates and Open House information:

www.prescott-russell.on.ca/en/public-works/major-projects
ottawa.ca/easternlt
ottawa.ca/hwy174and17study

www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure
ottawa.ca/tlrest
ottawa.ca/etuderoute174et17



Transit and Road EA Integration

The Confederation Line East LRT Extension (Blair to Trim) will run along the Highway 174 road corridor.

Co-ordinated approach to both EA studies is needed to:

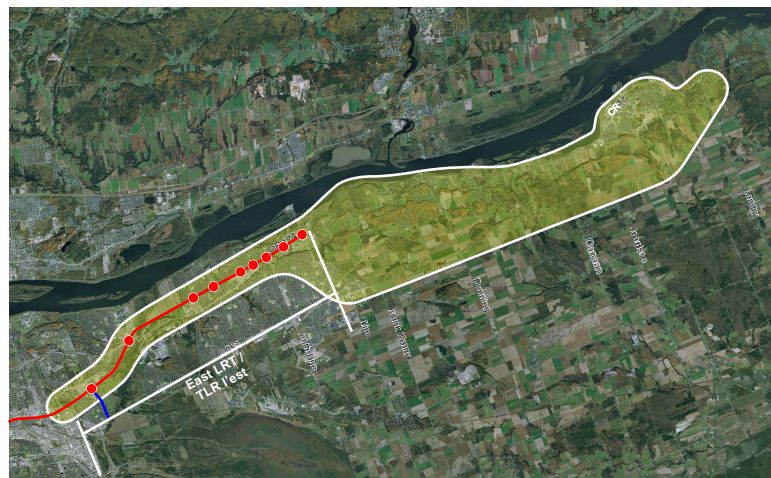
- Maximize the use of the existing road corridor
- Minimize impacts on the environment
- Minimize construction cost
- Build on existing study information
- Integrate stakeholder consultation activities
- Streamline EA process
- Share information, technical and design work, and stakeholder consultation

Intégration de l'ÉE sur les routes et le transport en commun

Le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail (de Blair à Trim) sera parallèle au couloir de la route 174.

Il est important de coordonner la démarche des deux ÉE afin :

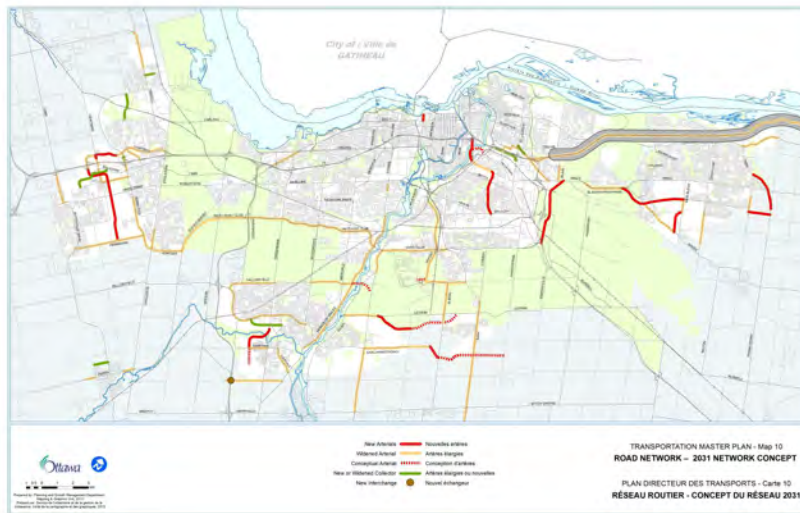
- De maximiser l'utilisation du couloir routier actuel
- De minimiser les conséquences sur l'environnement
- De minimiser les coûts de construction
- De tirer le meilleur parti possible de l'information obtenue dans l'étude
- D'intégrer les activités de consultation avec les intervenants
- De simplifier le processus d'ÉE
- D'échanger de l'information, les travaux de conception technique et les consultations avec les intervenants



2013 TMP - Road Network

An updated Transportation Master Plan was approved in 2013 that placed priority on sustainable and active transportation and emphasized affordability and responsible investment.

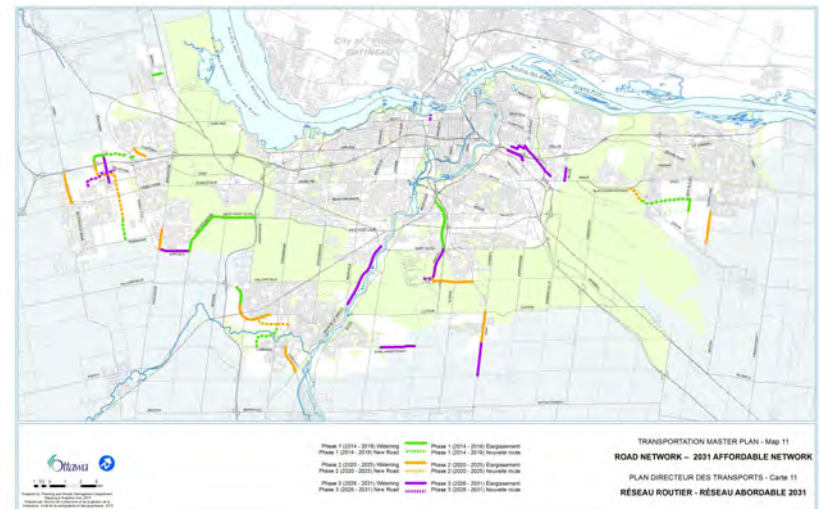
The widening of the Highway 174 to the eastern limits of Canaan (City of Ottawa boundary) is identified in the Ultimate 2031 Road Network but is not identified in the affordable plan. Based on available funding and current City of Ottawa budgeting, its implementation is beyond 2031.



PDT 2013 - Réseau routier

Un nouveau Plan directeur des transports a été approuvé en 2013, qui accorde la priorité au transport durable et actif et met l'accent sur la capacité financière et l'investissement responsable.

L'élargissement de la route 174 à la limite est de Canaan (territoire de la ville d'Ottawa) est établi dans le réseau routier ultime - horizon 2031, mais pas dans le plan abordable. En fonction du financement accessible et du budget actuel de la Ville d'Ottawa, sa mise en œuvre va au-delà de 2031.



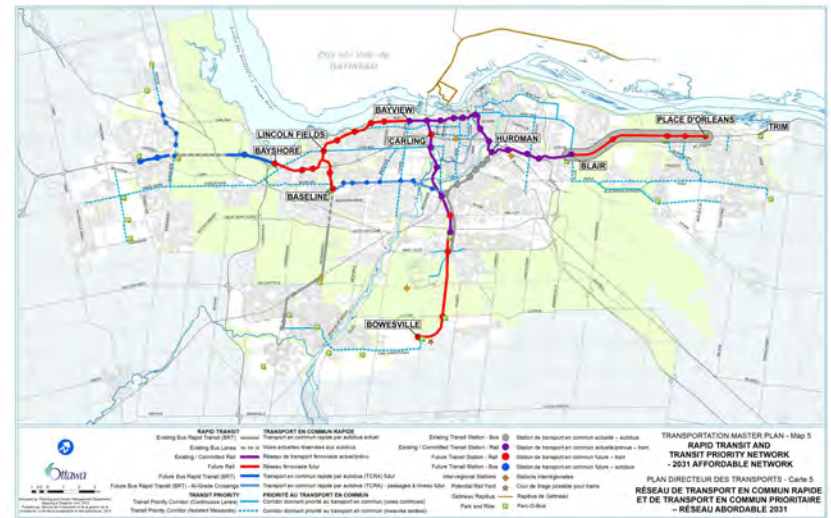
2013 TMP - Transit Network

An updated Transportation Master Plan was approved in 2013 that placed priority on sustainable and active transportation and emphasized affordability and responsible investment.

PDT 2013 - Réseau de transport en commun

Un nouveau Plan directeur des transports a été approuvé en 2013, qui accorde la priorité au transport durable et actif et met l'accent sur la capacité financière et l'investissement responsable.

L'étape 2, soit le prolongement vers l'est de la Ligne de la Confédération du TLR, est comprise de la station Blair à la Place D'Orléans dans le plan abordable du réseau de transport en commun. Le prolongement jusqu'au chemin Trim est compris dans le réseau ultime de transport en commun, qui est la vision à long terme, au-delà de 2031, de la Ville d'Ottawa.



East of Trim - Development of Road Corridor Alternatives

East of Trim Road the alternatives all include:

- 1 continuous lane of capacity in each direction
- Paved shoulders
- Pedestrian and cyclist accommodations



Four Lane Divided Rural Roadway (median barrier type to be determined) /
Route rurale à quatre voies (type de médiane à déterminer)

À l'est de Trim - Élaboration d'autres solutions de couloirs routier

Les options de solutions à l'est de Trim comprennent toutes :

- 1 voie continue dans chaque direction
- Des accotements asphaltés
- Des installations pour les piétons et les cyclistes



Two-Lane Undivided Rural Roadway /
Route rurale à deux voies à chaussée unique

West of Trim - Development of LRT and Road Widening Alternatives

West of Trim Road the alternatives all include:

- Roadway cross section:
 - 3 lanes in each direction
 - Protection for High Occupancy Vehicle (HOV) lanes
 - Paved shoulders and concrete median barrier
 - A continuous concrete barrier between Highway 174 and the ELRT
- LRT cross section includes:
 - Dual tracks
 - Space for overhead catenary poles (generally between the two tracks)
 - Space for walkways
 - Platforms at stations, including track transitions where centre platforms are used



Median Alignment / Tracé du TLR dans la voie médiane

À l'ouest de Trim - Élaboration de solutions du TLR et l'élargissement routier

Les options de solutions à l'ouest de Trim comprennent toutes :

- Chaussée :
 - 3 voies dans chaque direction
 - Protection pour les voies réservées aux véhicules à occupation multiple (VOM)
 - Accotements asphaltés et muret californien en béton
 - Muret en béton continu entre la route 174 et le TLR vers l'est
- Le TLR comprend :
 - Une double voie
 - De l'espace pour les perches de caténaires aériennes (habituellement entre deux voies)
 - Et l'espace pour les voies piétonnières
 - Des plateformes aux stations, y compris des zones de transition entre les voies lorsque des plateformes centrales sont utilisées



North/South Side Alignment / Tracé du TLR Nord/Sud

Evaluation Methodology

The methodology chosen to evaluate alternative solutions addresses the following objectives.

- **Use of Criteria:** Criteria/indicators (i.e., features or considerations) are used as the basis for the evaluation of the alternative solutions.
- **Weighting or Ranking of Criteria:** An ordering (i.e. weighting or ranking in terms of importance) of the criteria is the basis for the evaluation.
- **Sensitivity Analysis:** Some form of sensitivity analysis is completed as part of the evaluation process (i.e. to test the results of the evaluation and to verify the influential factors in the results of the evaluation).
- The **Concordance Method** was chosen as the most appropriate evaluation method as it supports the above-listed objectives.

Méthode d'évaluation

La méthode choisie pour évaluer les solutions de rechange aborde les objectifs suivants.

- **Utilisation de critères :** Des critères/indicateurs (c.-à-d. des caractéristiques ou des facteurs à examiner) devraient être utilisés comme fondement de l'évaluation des solutions de rechange.
- **Pondération ou classement des critères :** L'évaluation devrait s'appuyer sur un ordonnancement (c.-à-d. une pondération ou un classement de l'importance) des critères.
- **Analyse de sensibilité :** Une certaine forme d'analyse de la sensibilité devrait être effectuée dans le cadre du processus d'évaluation (c.-à-d. pour mettre les résultats de l'évaluation à l'épreuve et vérifier les éléments ayant pu les influencer).
- La **méthode de concordance** a été choisie comme méthode d'évaluation la plus appropriée étant donné qu'elle appuie les objectifs énumérés ci-dessus.

Road Corridor Alternatives - East of Trim

Alternative 1: Widen existing Highway 174/CR 17 to 4 lanes.

Alternative 2: Build a new route south of Wilhaven Drive to connect to Baseline Road.

Alternative 3A: Improve existing Innes-Baseline Road with a new connection from Dunning to Canaan Road.

Alternative 3B: Same as Alternative 3A except connecting to Brian Coburn Blvd at Frank Kenny Road.

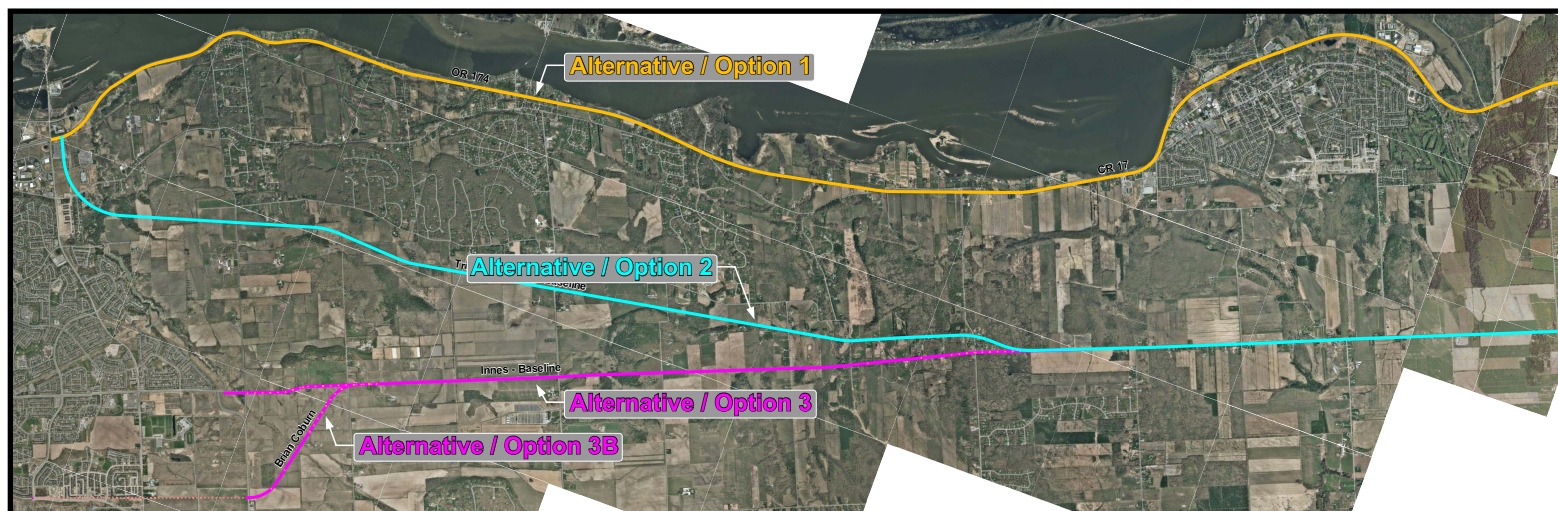
Options de tracés – À l'est de Trim

Option 1 : Élargir la route 174/chemin de comté 17 actuel à 4 voies.

Option 2 : Construire une nouvelle route au sud de la promenade Wilhaven pour rejoindre le chemin Baseline.

Option 3A : Améliorer la connexion du chemin Innes et du chemin Baseline avec une nouvelle connexion du chemin Dunning et du chemin Canaan.

Option 3B : Même que l'option 3A, sauf connexion au boulevard Brian Coburn au chemin Frank Kenny.



Evaluation Criteria - East of Trim

The alternatives were evaluated according to the general criteria below.

Social Environment

- Impact on existing residences / communities
- Impact on planned development
- Impact on recreational features
- Impact on archaeological resources
- Cultural resources
- First Nations
- Impact on agricultural resources
- Noise
- Air Quality
- Views and vistas
- Compatibility with municipal planning goals and objectives

Infrastructure

- Major infrastructure impacts

Physical

- Geology
- Groundwater
- Potentially Contaminated Sites

Transportation

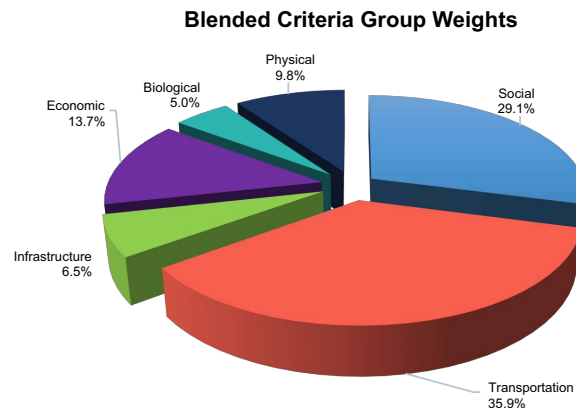
- Traffic Performance (autos)
- Road Network Function
- Safety
- Transit Network Function
- Pedestrian Network
- Cycling Network
- Ferry

Economic Environment

- Businesses
- Capital costs

Biological

- Aquatic
- Natural Heritage features
- Wildlife
- Species at Risk



Evaluation Results - East of Trim

Based on the concordance method of evaluation, Alternative 1, the widening of Hwy 174, is the preferred alignment for the following reasons:

- Best addresses transportation needs – most direct route
- Serves the predominant flow of traffic
- Best connection to road and transit network - ELRT
- Supports growth and sustainability policies
- Provides opportunity to improve pedestrian and cycling facilities where needed
- Supports re-design and improvement of the road environment through communities
- Allows for consideration of HOV lane designation – promotes Clarence-Rockland transit
- Does not add infrastructure to undeveloped natural areas

Résultats d'évaluation – À l'est de Trim

À partir de la méthode de concordance utilisée pour l'évaluation, l'option 1 est le tracé privilégié pour les raisons suivantes :

- La solution qui répond le mieux aux besoins en transport - trajet le plus direct
- Elle tient compte du flux prédominant de circulation
- Meilleure liaison au réseau routier et de transport en commun - TLR de l'Est
- Elle est conforme aux politiques de croissance et de durabilité
- Elle ouvre des possibilités d'amélioration des installations pour piétons et cyclistes selon les besoins
- Elle appuie la nouvelle conception et les améliorations prévues à l'environnement routier dans les collectivités.
- Elle permet d'envisager la désignation de voies réservées aux VTOE – et favorise le transport en commun pour Clarence-Rockland
- Elle évite l'ajout d'infrastructures à des aires naturelles non aménagées.

Alternatives	Alternatives / Options				Sum / Somme	Rank / Classement	Options
	1	2	3A	3B			
Alt. 1: Widening of 174/17		0.62	0.63	0.63	1.88	1	Option 1 : Élargissement de la route 174 / 17
Alt. 2: New Route from Trim to Baseline south of Wilhaven	0.38		0.27	0.31	0.96	4	Option 2 : Nouveau tracé de Trim à Baseline, au sud de Wilhaven
Alt. 3A: New connection from Innes to Baseline Road	0.37	0.73		0.56	1.66	2	Option 3A : Nouveau raccordement du chemin Innes au chemin Baseline
Alt. 3B: Innes connecting to Baseline & Brian Coburn Blvd	0.37	0.69	0.44		1.50	3	Option 3B : Raccordement du chemin Innes à Baseline et au boul. Brian Coburn

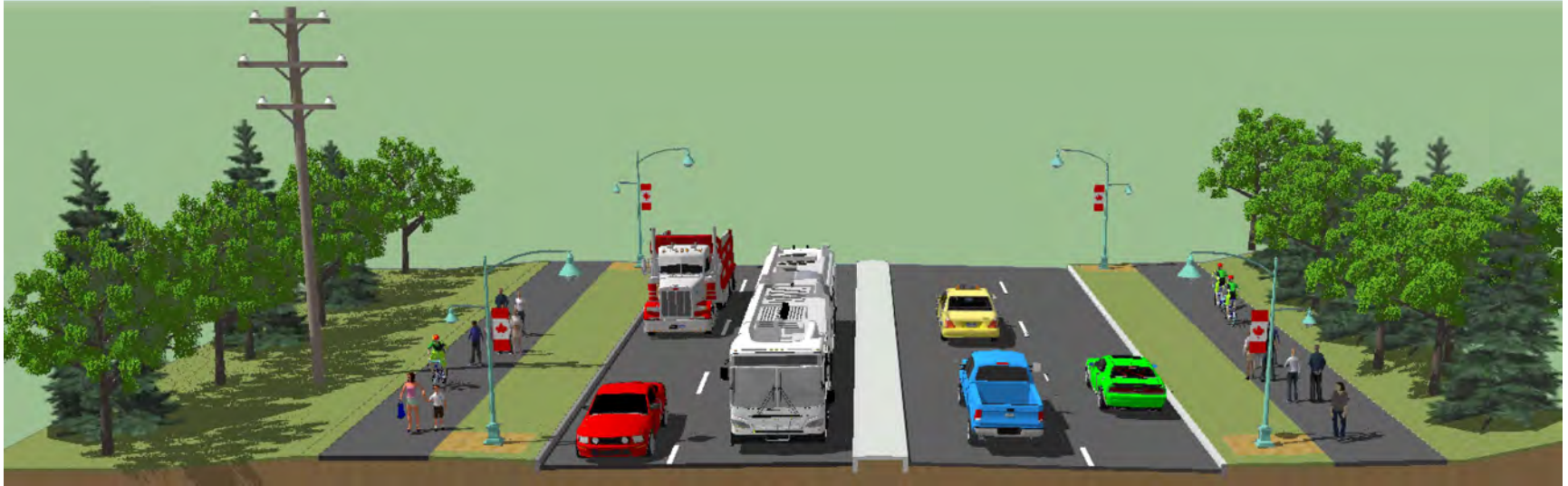
**Preliminary Preferred Road Corridor -
East of Trim Road**

**Tracé privilégié provisoire –
À l'est de Trim**



Typical Road Corridor Design in Cumberland Village

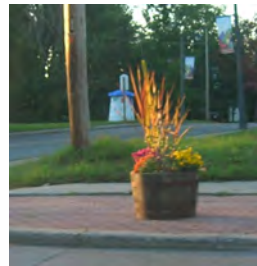
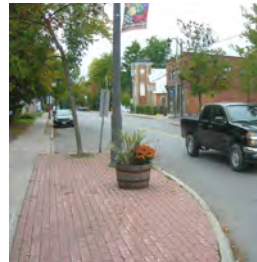
Conception de couloir routier typique dans le village de Cumberland



- Reduce speeds
- Pedestrian and cycling facilities
- Pedestrian crossing opportunities
- Provide river access
- Improve lighting
- Provide gateway features to the Nation's Capital

- Réduction de la vitesse
- Installations pour piétons et cyclistes
- Opportunités de passage pour piétons
- Accès à la rivière
- Amélioration de l'éclairage
- Aménagements pour porte d'entrée à la capitale nationale

Cumberland Village Design Considerations



Considérations pour conception dans le village de Cumberland

Median Station Examples



Exemple de stations dans la médiane



Evaluation Criteria - West of Trim

The alternatives were evaluated according to the general criteria below.

Social

- Archaeological resources
- Noise
- Property Impacts
- Compatibility with Adjacent Communities
- Views and Vistas
- Smart Growth

Transportation

- Safety
- Transit Network Function
- Transit Ridership
- Transit Stations
- Pedestrian and Cyclists
- Road Network Functions

Infrastructure

- Major Municipal Infrastructure
- Utilities
- Major Structure

Cost

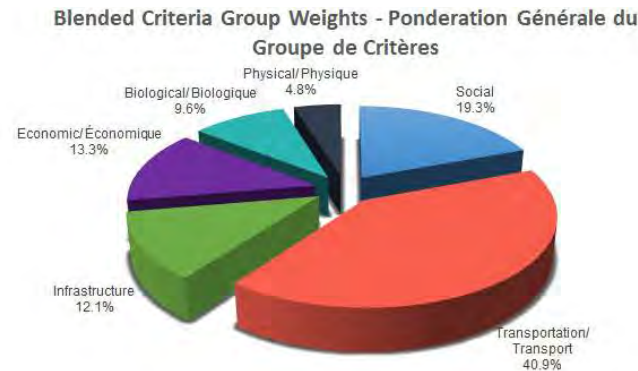
- Capital costs
- Operating costs

Biological

- Natural Heritage features
- Aquatic
- Wildlife
- Species at Risk

Physical

- Slopes and ravines



Evaluation Results - West of Trim

The Median Alternative is the preferred alignment for the following reasons:

- Wide median east of Montreal Road can generally accommodate LRT
- Reduces the number of structures required
- Reduces overall capital cost
- Provides less staging impacts on both existing transit and vehicular traffic

Résultats d'évaluation – À l'ouest de Trim

L'option de voie médiane est le tracé privilégié pour les raisons suivantes :

- La voie médiane large à l'est du chemin Montréal peut, en général, accueillir le TLR
- Réduction du nombre de structures requises
- Réduction du coût global d'immobilisation
- Réduction des conséquences de l'échelonnement des travaux sur le transport en commun et la circulation automobile actuels

Alternatives / Options						
Alternatives	North / Nord	South / Sud	Median / Médiane	Sum / Somme	Rank / Classement	Options
North Alignment		0.59	0.10	0.69	2	Tracé Nord
South Alignment	0.41		0.21	0.62	3	Tracé Sud
Median Alignment	0.90	0.79		1.69	1	Tracé Médiane

Hybrid Alignment - West of Trim

A hybrid solution was developed that utilizes the North ELRT corridor to the Greens Creek area then transition into the median. This hybrid takes advantage of:

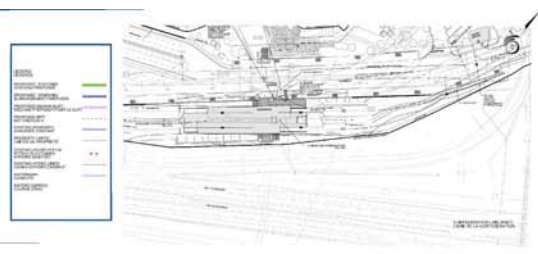
- Better proximity to developed areas that can encourage Transit Oriented Development and ridership
- Better connections to multi-use pathway;
- Further reduces the number of structures required
- Maintains the existing transitway (bus) Highway 174 overpass both during and post construction

Tracé hybride – À l'ouest de Trim

Un tracé hybride qui utilise le couloir nord du TLRE jusqu'au secteur du ruisseau Greens, puis emprunte la voie médiane a été élaboré. Ce tracé hybride tire avantage :

- D'une meilleure proximité des secteurs aménagés, qui peut favoriser le développement axé sur le transport en commun et l'achalandage
- De meilleures liaisons avec les sentiers polyvalents
- D'une réduction du nombre de structures requises
- De la conservation des passages supérieurs du Transitway (autobus) de la route 174 pendant et après les travaux





Next Steps

- Incorporate Comments and Confirm preferred alternatives
- Develop design details:
 - Transit station locations and design
 - Interchange Configurations including Trim Road-Highway 174 grade separation
 - Intersection layouts including roundabouts
 - Cumberland Village cross sections
 - Pedestrian and cycling connections
- Determine mitigation measures
- Next Public Meeting late spring 2015
- Presentation to Councils and Committees
- Environmental Study Reports

Prochaines étapes

- Intégrer les commentaires et confirmer les solutions privilégiées
- Élaborer les détails de la conception :
 - Conception et emplacement des stations
 - Configurations des échangeurs, y compris l'étagement des voies de la route 174 (inclus chemin Trim)
 - Conception des intersections, y compris les carrefours giratoires
 - Coupes transversales du village de Cumberland
 - Liaisons pour piétons et cyclistes
- Déterminer les mesures d'atténuation
- Prochaine consultation publique à la fin du printemps 2015
- Présentation au Conseil et Comités
- Rapports sur les évaluations environnementales

Consultation Round 3 Summary Report (Spring 2015)



AECOM
302 – 1150 Morrison Drive
Ottawa, ON, Canada K2H 8S9
www.aecom.com

613 820 8282 tel
613 820 8338 fax

United Counties of Prescott and Russell and City of Ottawa

Summary Report for April 2015 Consultation Confederation Line East LRT Extension/ Highway 174/ Prescott Russell County Road 17 Widening

Prepared by:

AECOM
302 – 1150 Morrison Drive
Ottawa, ON, Canada K2H 8S9
www.aecom.com

613 820 8282 tel
613 820 8338 fax

Project Number:

60270243 and 6023982

Date:

July, 2015

July 30, 2015

Marc R. Clermont, P. Eng.
Director of Public works
United Counties of Prescott and Russell
59 Court Street
L'Orignal, Ontario
K0B 1K0

Angela Taylor, P. Eng.
Senior Project Engineer
Planning & Growth Management Department
City of Ottawa
4th Floor, 110 Laurier Avenue West
Ottawa, Ontario
K1P 1J1

Dear Mr. Clermont and Ms. Taylor:

Project #s: 60270243 and 60323982

Regarding: Summary Report for April 2015 Consultation, Confederation Line East LRT
Extension and Ottawa Road 174/ Prescott Russell County Road 17 Widening

The following Consultation Report for the April 2015 round of consultation meetings for
Confederation Line East LRT Extension/ Ottawa Road 174/ Prescott Russell County Road 17
Widening project is provided for your review. Comments received on our draft submission have been
incorporated.

Sincerely,
AECOM Canada Ltd.

Valerie McGirr, P. Eng.
Project Manager
AECOM

Encl.
cc: file

Distribution List

# of Hard Copies	PDF Required	Association / Company Name
0	1	Marc Clermont, United Counties of Prescott and Russell
0	1	Angela Taylor, City of Ottawa

Revision Log

Revision #	Revised By	Date	Issue / Revision Description
0	DK	July 2015	Draft for review
1	CG	July 2015	Revised document

Signatures

Report Prepared By:

Catherine Ghiourelotis
Environmental Planner, AECOM

Report Reviewed By:

Kelly Roberts
Environmental Planner, Parsons

Report Reviewed By:

Valerie McGirr, P. Eng
Manager, Ottawa office, AECOM

Table of Contents

Statement of Qualifications and Limitations

Letter of Transmittal

Distribution List

Page

1	INTRODUCTION	1
2	PROJECT WEBSITE.....	2
3	CONSULTATION GROUP MEETINGS	3
3.1	Agency Consultation Group (ACG)	3
3.2	Business Consultation Group (BCG).....	7
3.3	Public Consultation Group (PCG).....	10
3.4	Aboriginal Consultation.....	13
4	PUBLIC OPEN HOUSES	14
4.1	Public Open House in Orléans	15
4.2	Public Open House in Cumberland	17
4.3	Public Open House in Clarence Creek	19
4.4	Summary of Comments Submitted.....	21
4.4.1	Summary of Comments Submitted before the POHs.....	21
4.4.2	Summary of Comments Submitted at the POHs	22
4.4.3	Comments Submitted after the POH Meetings	23
4.4.4	Frequently Asked Questions	24
5	CONCLUSION.....	24

List of Figures

Figure 1 Study Area - East LRT and Highway 174/CR17	1
Figure 2 Study Area – Confederation Line East LRT Extension	2

List of Tables

Table 1. Contacts for Agency Consultation Group.....	4
Table 2. Summary of Discussion at the Agency Consultation Group Meeting	5
Table 3. Contacts for Business Consultation Group	7
Table 4. Summary of Discussion at the Business Consultation Group Meeting.....	8
Table 5. Contacts for Public Consultation Group.....	10
Table 6. Summary of Discussion at the Public Consultation Group Meeting	11
Table 7. Summary of Questions & Answers at the Public Open House in Orléans.....	15
Table 8. Summary of Questions & Answers at the Public Open House in Cumberland	17
Table 9. Summary of Questions & Answers at the Public Open House in Clarence Creek	19
Table 10. Summary of Comments Submitted after the February Public Open House Meetings	21

Table 11.	Summary of Comments Submitted at the Public Open House in Orléans	22
Table 12.	Summary of Comments Submitted at the Public Open House in Cumberland	22
Table 13.	Summary of Comments Submitted at the Public Open House in Clarence Creek	23
Table 14.	Summary of Comments Submitted after the Public Open House Meetings	23

Appendices

Appendix A.	Public Open House Notices
Appendix B.	Information Bulletins
Appendix C.	Open House Comment Log and Correspondence
Appendix D.	Frequently Asked Questions
Appendix E.	Presentations and Display Material

1 INTRODUCTION

The United Counties of Prescott and Russell (UCPR) in partnership with the City of Ottawa are undertaking an Environmental Assessment (EA) study for Highway 174 (HWY174) and Prescott-Russell County Road 17 (CR 17) corridor from Highway 417 to County Road 8 (Landry Road). This Study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class EA process. The study area for the HWY 174-CR 17 project is shown in **Figure 1**.

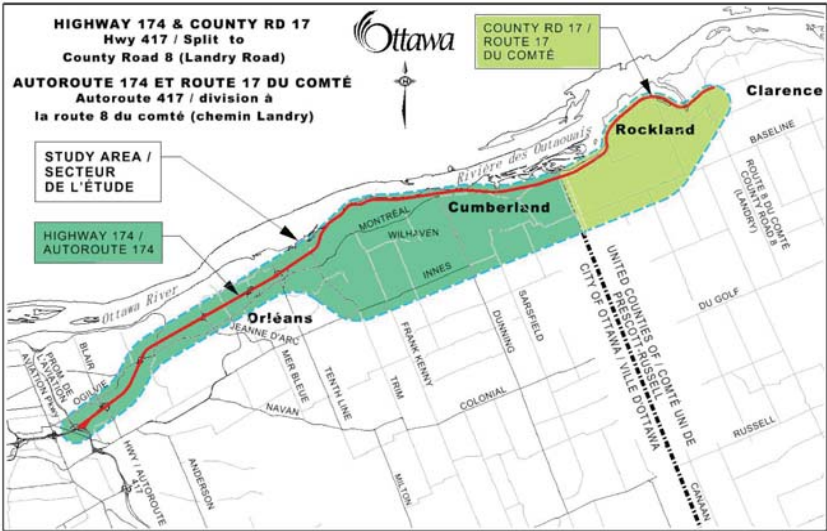


Figure 1 Study Area - East LRT and Highway 174/CR17

The City of Ottawa is also undertaking an EA Study for the easterly extension of the Confederation Line Light Rail Transit (LRT) from Blair Station to Trim Road along the Highway 174 corridor in accordance with the Transit Projects Assessment Process (TPAP). The East LRT will provide fast, reliable service to the communities of Beacon Hill, Blackburn Hamlet, Orléans, and rural Cumberland. A total of eight stations were proposed between Blair Station and the Trim Park and Ride. The study area for the East LRT project is shown in **Figure 2**.

The Class EA and TPAP processes will involve developing, assessing, and evaluating alternatives, which will result in a recommended plan to be presented to the Committees/Councils of the UCPR and the City of Ottawa.

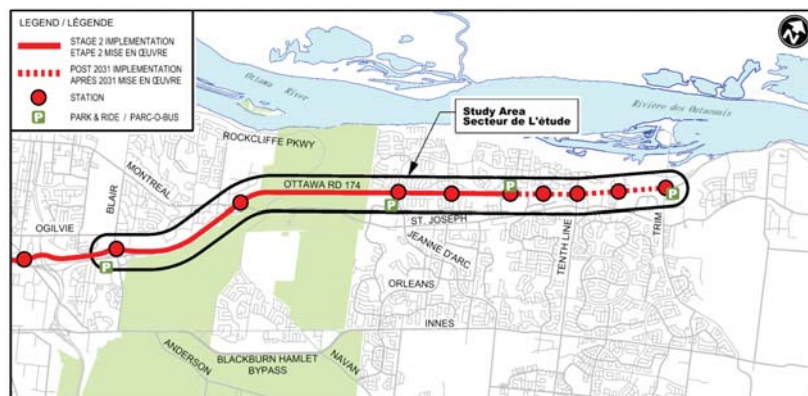


Figure 2 Study Area – Confederation Line East LRT Extension

This was the third round of public consultation for the OR174/CR17 EA study and the second round of public consultation for the East LRT Extension EA Study. Consultation was held in 2013 to introduce the need for alternative solutions to address problems on the OR174 and CR17 corridor. **This consultation summary report covers the consultation that took place in April 2015.**

Consultation is an important part of each of the environmental assessment processes to effectively share project information and obtain feedback in a timely manner. The key activities during this round include:

Consultation Group meetings:

- Agency Consultation Group – April 13, 2015
- Business Consultation Group – April 14, 2015
- Public Consultation Group – April 16, 2015

Open Houses at three locations:

- Orléans – April 20, 2015
- Cumberland – April 21, 2015
- Clarence/ Rockland – April 23, 2015

2 PROJECT WEBSITE

Project information was posted on the UCPR and the City of Ottawa web sites at:

www.prescott-russell.on.ca/en/public-works/majorprojects
ottawa.ca/hwy174and17study
ottawa.ca/easternlrt
ottawa.ca/stage2

Public Open House notices, display material presented at the meetings and project updates will be available online throughout the study. Contact information is also provided on the web site to allow the public to comment throughout the study process.

3 CONSULTATION GROUP MEETINGS

Three separate Consultation Groups were formed to represent various agency, business and public stakeholders, which will provide direct input and feedback during the course of the study.

3.1 Agency Consultation Group (ACG)

An Agency Consultation Group (ACG) was established for this project to address technical issues; comment on the special studies required to fully assess the various alternatives; and help ensure that the UCPR and City of Ottawa follow procedures, legislation and appropriate policies. Members include federal, provincial, UCPR, City of Ottawa and other municipal representatives.

Prior to the round of Public Open Houses, a meeting was held with the ACG to obtain input. This meeting was held on April 13, 2015 from 9:00 am – 11:30 am in the Honeywell Room of Ottawa City Hall. The members of the study team in attendance included:

United Counties of Prescott and Russell	
Jérémie Bouchard	Parks and Recreation Director
Phil Pawliuk	Project Manager

ACG Members in Attendance	
David Atkinson	Michael Boughton
James Bowser	Noreen Mazorra
Steven Boyle	Doug Rathwell
Katarina Cvetkovic	Amira Shehata
Marina Down	Genya Stefanoff
Bruce Finlay	Dennis Gratton
Mike Michaud	Arto Keklikian
Katherine Kirzati	Daniel Taylor
Brittany Theobald	

City of Ottawa, Project Management	
Angela Taylor	Project Manager
Frank McKinney	City of Ottawa
Vivi Chi	City of Ottawa

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Kelly Roberts	Lead Environmental Planner, Parsons
David Hopper	LRT Lead Parsons
Stephen Fitzpatrick	Environmental Planner, Parsons

Invitations to the ACG meeting were emailed to the agencies and utilities listed in **Table 1**.

Table 1. Contacts for Agency Consultation Group

Contacts - ACG	
United Counties of Prescott and Russell:	City of Clarence-Rockland:
• Accessibility Advisory Committee	• Community Services (Recreation)
• Agricultural Advisory Committee	• Fire Department
• Economic Development and Tourism	• Physical Services
• Emergency Services	• Planning
• Planning & Forestry	Ottawa Catholic School Board
• Public Works	Catholic District School Board of Eastern Ontario
City of Ottawa:	Upper Canada District School Board
• Acquisition, Real Estate Partnership & Development Office	• Ottawa Student Transportation Authority
• Emergency & Protective Services	• Student Transportation for Eastern Ontario
• Planning & Growth Management	• Conseil des écoles catholiques du Centre-Est
• Construction Services	• Conseil des écoles publiques de l'Est de l'Ontario
• Design, Review and Implementation	• Conseil scolaire de district catholique de l'Est ontarien
• Development Review - East Unit	• Consortium de transport scolaire de l'Est
• Development Review - Outer Core Unit	Ministry of Infrastructure
• Development Review - Rural East Unit	Ministry of Natural Resources and Forestry
• Environmental Engineering/ Environmental Services	Ministry of Tourism, Culture and Sport
• Infrastructure Assessment - Transportation Assets	Ministry of Transportation
• Infrastructure Assessment - Water Resources Assets	Ministry of Aboriginal Affairs
• Land Use & Natural Systems	OPP
• Municipal Design & Construction East, Infrastructure Services	Rideau Valley Conservation Authority
• OC Transpo	South Nation Conservation Authority
• Planning and Infrastructure	Transport Action Canada
• Ottawa Police Services	National Capital Commission
• Rail Planning, Rail Implementation Office	National Research Council
• Right of Way Info & Approvals	Public Works and Government Services Canada
• Road Safety, Traffic Investigation & Surveys	RCMP
• Rural Affairs Office	Transport Canada
• Special Operations	Bell Aliant
• Traffic Operations	Enbridge Gas Distribution Inc.
• Traffic, Safety & Mobility	Hydro One Networks Inc.
• Transit Priority, Transit Services	Hydro Ottawa
• Transportation Strategic Planning	Rogers Communications Inc.
• Water Distribution	Videotron
• Operations - Paramedic Services, Emergency & Protective Services	

Angela Taylor welcomed ACG attendees and introduced the co-proponents of the study as well as the members of the Study Team. The ACG attendees also introduced themselves. The presentation was jointly given by Angela Taylor, David Hopper, and Valerie McGirr. A summary of the discussion is presented in **Table 2**. Comments from the ACG were considered by the Study team in the public consultation and in the development of the documentation of the studies.

Table 2. Summary of Discussion at the Agency Consultation Group Meeting

Summary of Discussion - ACG
Summary of Comments – West of Trim Road
Q: What are the impacts on the proposed on/off ramps north of Trim Road east of the MTO site? And the ramps to the south?
A: <i>The north portion is owned by the City. The south ramps are on City land as well, and would not impact the existing expansion of the park and ride.</i>
Q: If the Taylor Creek LRT station is not recommended due to ridership and proximity to other stations it shouldn't be recommended for protection.
A: <i>We will review based on proximity to the proposed Trim Road station and the potential for Taylor Creek to undergo a major redevelopment.</i>
Q: Is it worthwhile to consider cash in lieu of parking, instead of decked parking?
A: <i>Typically in TOD development you reduce parking. Cash in lieu requires parking to be expanded, which is counter intuitive to what we are trying to achieve.</i>
Q: Why is the northbound to eastbound ramp at Trim located parallel to the nearby road?
A: <i>The ramp may be moved into the existing alignment to the Bus Loop.</i>
Q: Is there any plan to expand the park and ride at Trim Road?
A: <i>Expansion will occur if and when the LRT expands to Trim Road depending on the most recent ridership numbers at the time.</i>
Q: The station at Place d'Orléans will impact the Champlain overpass structure.
A: <i>We are trying to find way to defer this potential cost to the future road widening by placing the station to the west. It may not be possible depending on construction area requirements. Should it work, it will be very tight.</i>
Q: It would be ideal if developers contributed to a pedestrian bridge in future developments nearby the 10 th Line Station.
A: <i>There is no plan of subdivision in place currently; it could be years until we are at that stage. However future development potential makes sense for station placement and this pedestrian opportunity.</i>
Q: West of Montreal Road there needs to be an emphasis on how to get pedestrians and cyclists to the station safely and efficiently. This should be made an emphasis in the plans going forward.
A: <i>We will take this into account during the design stage.</i>
Q: How much NCC land is required at the Montreal Road station?
A: <i>No Greenbelt land will be required. Additionally there will be no physical impact on the existing Green's Creek culverts.</i>
Q: Will the median stations provide bike parking?
A: <i>Yes, all of the stations will provide bike parking.</i>
Q: What are the NCC key concerns regarding the project?
A: <i>Avoidance of Greenbelt property is key. The elevated LRT should provide opportunities for new views to and from the Greenbelt. NCC will confirm approval requirements.</i>
Summary of Comments – East of Trim Road
General Comment:
East of Trim there is a requirement for a Multi-Use Pathway for development. Just be aware of this further along in your design.
Q: The roundabout in Cumberland Village seems massive, and as though it does not suit the landscape. Would the N-S ever get a break to join E-W traffic?
A: <i>There would be significant delays at the peak hours.</i>
Q: Both roundabouts proposed in Cumberland Village must be considered together in the traffic analysis. There will be additional impact on queuing.

Summary of Discussion - ACG
<i>A: The two roundabouts are not considered to be close enough to have an impact on queuing as other urban roundabouts.</i>
Q: What is the main rationale for the use of roundabouts?
<i>A: Speed management; from the research it is effective urban design for speed management. We are also showing urban design elements for speed management including median lighting, landscaping and roadside elements to change the environment for drivers. Roadside and median changes have contributed to speed management on Highway 417 approaching Carleton Place.</i>
Q: Moving northern service road (east of Cumberland) eastwards would have less of an impact on the adjacent homeowners.
<i>A: This is something that can be examined.</i>
Q: Concerned about the safety of U-turns along a highway.
<i>A: Areas where U-turns are encouraged/permitted will have an actuated signal. EMS will be contacted to review median break opportunities.</i>
Q: Will the left turn be fully-protected?
<i>A: Yes. Signal timing and phasing will be developed in the detail design phase.</i>
Q: Would the crosswalk in Blackburn Hamlet be comparable to the pedestrian crossing proposed nearby the Village Museum?
<i>A: Yes. Another comparable example would be the Trans Canada Trail crossing on Terry Fox Drive.</i>
Q: What is the width of the proposed ROW at the end of Old Montreal Road?
<i>A: We will provide these numbers to you.</i>
Q: What you have as a farm at the end of Old Montreal Road is actually a resource centre owned by the school board. It will have a lot of bus traffic that will make a potential right-in right-out access problematic.
<i>A: We will talk with them to see if they are amenable to extending the service road across their property to connect to the nearby signalized intersection.</i>
Q: The pathway system is not legible on these slides. Property lines should also be emboldened.
<i>A: We will correct this.</i>
Q: What is the cost estimate for this portion?
<i>A: We are in process of developing this – it is a moving target at the moment.</i>
Q: A 3 lane roundabout is unsuited to Cumberland Village. This will make things worse from a resident point of view. Urban design elements can be used alongside signalized intersections that will slow speeds and be less obtrusive. What is going to be shown as the preferred alternative?
<i>A: Direction to date has been the roundabout option. But we will show the signalized intersections as well to provide context.</i>
Q: Recommended design should be signalized intersections, as they better achieve the goals based on technical professional opinion.
<i>A: Signalized intersections was the preferred option technically. We will forward the option that is determined by this working group.</i>
Motion: ACG AGREES TO FORWARD SIGNALIZED INTERSECTIONS THROUGH CUMBERLAND VILLAGE AS THE PREFERRED ALTERNATIVE - MOTION BY VIVI CHI
Note: The advantages and trade-off portion of the presentation has to be revised to accurately reflect the comparison of signalized vs. roundabouts.
ACG COLLABORATIVELY EDITS THE TABLE TO REFLECT THE PROFESSIONAL OPINIONS OF THOSE PRESENT.
Q: Cardinal Creek development plans to have a multi-use pathway potentially crossing 174. This EA should not preclude the ability for that to happen in the future if the culvert is modified to improve safety and sightlines.
<i>A: The option will not be precluded.</i>
Q: When will we be issuing the Notice of Commencement?

Summary of Discussion - ACG
<i>A: We will be meeting with the Ministry of the Environment and Climate Change to determine the best time in order to effectively co-ordinate with the 174/17 ESR.</i>
General Comment:
<i>Please include cross-rides for cyclists in detailed design, in addition to cross-walks.</i>

3.2 Business Consultation Group (BCG)

A Business Consultation Group (BCG) was established for this project to provide input to the study and to advise and comment on local issues and concerns. The BCG is not an approving body. Members may include businesses, business associations, institutions and developers with interest in the area. Prior to the Public Open Houses, a separate meeting was held with the BCG to obtain input. This meeting was held on April 14, 2015 from 2:00 pm-4:00 pm at 255 Centrum Boulevard in Orléans.

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Jérémie Bouchard	Parks and Recreation Director
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Kelly Roberts	Lead Environmental Planner, Parsons
David Hopper	LRT Lead Parsons
Stephen Fitzpatrick	Environmental Planner Parsons
Patrick Déoux	MMM Group

Invitations to the BCG were emailed to individuals listed in **Table 3**.

Table 3. Contacts for Business Consultation Group

Contacts - BCG	
AVW - TELAV Audio Visual Solutions	Bernard Sanscartier Construction Ltd.
Brigil	C.H. Clément Construction
Mr. Gas Limited	Clarence-Rockland Construction Association
Traversiers Bourbonnais (Ferry)	Devcore Group
Swedco	DRB Development
IBI Group	Regional Development
Novatech Engineering Consultants	Taggart
Orléans Chamber of Commerce	Tamarack Homes, Cardinal Creek
Ottawa Chamber of Commerce	Spacebuilders Ottawa Ltd.
Clarence-Rockland Chamber of Commerce	Colliers International
Greater Orleans Economic Development Council	Heart of Orleans BIA
Place d'Orléans	Pro/Com Reality Corp.
Projek Design & Development Inc.	

Angela Taylor welcomed BCG attendees and introduced the co-proponents of the study as well as the members of the Study Team. The BCG attendees also introduced themselves. The presentation was jointly given by Angela Taylor, David Hopper and Valerie McGirr.

A total of five stakeholders attended the Business Consultation Group meeting beyond the project team. A summary of the discussion is summarized in **Table 4**.

Table 4. Summary of Discussion at the Business Consultation Group Meeting

Summary of Discussion - BCG
Q: Is there provision for lengthening the platform in the event of multi-unit trains? A: We are planning for 120 metre platforms, the same as we are planning for in the downtown tunnel. These will provide adequate growth potential and allow us to get up to the 24,000 users an hour anticipated in the TMP.
Q: How many cars and people does that accommodate? A: Plans are for two trains with 2 to 5 units that can accommodate 800 people per train.
Q: What will be the width of the Jeanne d'Arc bridge and will there be access and parking for bicycles? A: The bridge will be 3 lanes in each direction. There will be parking for bicycles at each station.
Q: Currently there is not enough room in Place d'Orléans park and ride. What will be done to accommodate this? A: We are having a number of discussions. One option is structured parking.
Q: What is the size of the Orléans Park and Ride? A: 468 spots.
Q: The park and ride spaces currently available in the Shopping Centre Parking lot will be terminating this year. These spots will have to be accommodated elsewhere. A: We will continue our discussions.
Q: Are new park and ride lots being considered? Development is ready to proceed east of the Town Centre and there is a potential to provide/acquire approx. 4 acres for parking. A: The need for new park and ride lots would be assessed depending on the terminus of the line and will not likely be necessary until after Stage 2 is operational.
Q: Was a survey conducted to determine users of the Trim Park and Ride? A: OC Transpo has surveyed licence plates. It appears to be 50/50 split between Orléans, and communities further east.
Q: Last time you told us that using the hydro corridor as an option for LRT was too costly. How is it that the moving of hydro will not be so costly? A: The section requiring relocation with the median alternative is short. An EA is not required for relocations under 2 km in length, so changes to the hydro corridor north of Place d'Orléans does not trigger the need for an EA as it would for the south LRT alternative.
Q: Couldn't Hydro One just access poles from the south side and lose northern access along this corridor? Can you move the highway north? A: No, Hydro One requires 15 metres on both sides from the centreline as per their guidelines. There is not much flexibility to move the highway in this area.
Q: Will there be work on the highway widening as part of the LRT? During the Confederation Line work, the median lane is being used for construction. A: We are still working on the construction staging. There are only 2 structures requiring work (Parkway and Champlain).
Q: Will the stations you have proposed accommodate the realigned bus service going north-south? A: Yes they will.

Summary of Discussion - BCG
Q: How will Millennium park and ride function in this design? Will there be a shuttle from Trim to Millennium? A: There will be some break point in Orléans where catching the 94 at/from Millennium will still make sense (i.e. be quicker than catching the LRT). When you look at the ridership, the bulk of users get on at Place d'Orléans (6500/9000). It is diminishing returns to go further east or south to simply make use of a park and ride lot. We may see different trains using Blair, Place d'Orléans, and Trim as a terminus depending on ridership.
Q: Why not dip below ground at Trim Road to avoid having to construct ramps and an overpass? A: Dropping down becomes more expensive. Because you would have to drop down earlier, and then not able to rise fast enough, your station would also be below grade. This becomes even more expensive.
Q: Are traffic circles cheaper than signalized intersections? A: Yes and no, depending on the size and elaborate nature of design.
Q: Are we going to see amenities in the stations? Such as newsstands or stores like at Hurdman? A: At the very large ones, we may see kiosks as we see at Hurdman. However at stations like Place d'Orléans and St. Laurent, the focus will be moving people into the shopping centres, not competing with existing retail. Stores in suburban areas are generally not economically viable.
Q: Will there be Wi-Fi at stations? A: Potentially. This is at the discretion and timing of OC Transpo and not part of the Stage 2 planning.
Q: Will there be a physical separation between bikes and the road approaching these stations? A: It depends if lanes are in place. The intent of the city is to create cycle tracks, and we will examine how these facilities may tie in at each station.
Q: What about the salt damage that will occur to the vehicle from being in the median? A: Cars will be maintained accordingly. In other cities and sites with similar installations, there have been no such associated damages.
Q: Can you not just build an on-track maintenance stop? A: No, the requirements of a maintenance and storage facility require more space.
Summary of Comments – East of Trim Road
Q: Nothing has been said of the traffic from the Ferry A: Ferry traffic has been considered and it is handled much better by signalized intersections.
Q: With the money being spent to expand the highway and park and ride, why not just extend the LRT beyond Trim? A: The ridership is not sufficient to warrant this. Bus passenger drop off by CR Transit at the LRT will free up buses that will now terminate at the LRT instead of downtown, allowing for more frequent shorter trips with the existing number of buses.
Q: Will the consolidated driveways accommodate services (such as garbage and large trucks)? A: Yes.
GROUP UNANIMOUSLY AGREES THAT SIGNALIZED INTERSECTIONS THROUGH CUMBERLAND SHOULD BE THE PREFERRED ALTERNATIVE.
Q: What is the anticipated traffic flow along 174/17? A: We anticipate that it will grow to 2200 from 1300 in the peak hour.
Q: Was Rockland staff consulted in the road traffic/volume/planning work? A: Yes.
Closing
Q: Has there been any accounting for development charges or increases in taxes along the stretch of the LRT development? A: Part of funding is coming from the long term development fund, as well as a small built in tax increase until 2048 to pay for the City's portion. There will also be case-by-case zoning/development trade-offs.
Q: Extending to Trim seems like it is being taken more seriously than previous meetings. I am pleased with this. A: Thank you.

Summary of Discussion - BCG

Q: When will the decision be made on which stations to include?

A: *The stations presented today will be the ones that will be going forward to committee/council.*

Q: Could the LRT extend just to 10th Line?

A: *No, it will either terminate at Place d'Orléans, or Trim Road. There will not be an intermediary terminus due to the associated costs of terminus stations.*

Comments from the BCG were considered by the Study Team in the Frequently Asked Questions provided in Appendix D and in the development of the documentation of the EA studies.

3.3 Public Consultation Group (PCG)

A Public Consultation Group (PCG) was established for this project to provide input to the study and to advise and comment on local issues and concerns. The PCG is not an approving body. Members may include community associations and resident groups, Advisory Committees and advocacy groups. Prior to the Public Open Houses, a separate meeting was held with the PCG to obtain input. This meeting was held on April 16, 2015 from 6:30 pm – 8:30 pm at 255 Centrum Boulevard in Orléans.

The members of the project team in attendance included:

United Counties of Prescott and Russell	
Jérémie Bouchard	Public Works Engineer
Dave McAvoy	Project Manager
Phil Pawliuk	Project Manager

City of Ottawa	
Angela Taylor	Project Manager
Frank McKinney	City of Ottawa
Stephen Blais	Councillor for Ward 19 (Cumberland)

Consultant Team	
Valerie McGirr	Project Manager, AECOM
Kelly Roberts	Environmental Planner, Parsons
David Hopper	Parsons
Stephen Fitzpatrick	Parsons

Letters of notification were mailed or emailed to community groups and interested parties listed in **Table 5**.

Table 5. Contacts for Public Consultation Group

Contacts - PCG	
Beacon Hill Community Association	Carson Grove and Rockcliffe Mews Residents Association
Blackburn Community Association	City of Ottawa, Cumberland Heritage Village Museum Board
Bradley Estate Community Association	Citizens for Safe Cycling
Cardinal Creek Community Association	Cycle Vision
Chapel Hill South Community Association	Ecology Ottawa
Cumberland Village Community Association	Federation of Citizens Association of Ottawa-Carleton
Fallingbrook Community Association	Greenspace Alliance of Canada's Capital

Contacts - PCG

Navan Community Association	Heritage Ottawa
Sandpiper Cove Community Association	Ottawa Riverkeeper
Rothwell Heights Property Owners Association	Save the Parkway
Village Association (Chatelaine Village, Queenswood Village, Riverwalk)	Convent Glen / Orléans Wood Community Association (CGCWCA)
Bungalows of Notting Gate Community Association	Access Now
City of Ottawa, Accessibility Advisory Committee	Portobello South Community Development Association
City of Ottawa, Arts, Culture, Heritage and Recreation	Orléans Chaparral Community Centre
City of Ottawa, Community Services	Queenswood Heights Community Association
City of Ottawa, Cumberland Heritage Village Museum Board	Sarsfield Community Association
City of Ottawa, Environmental Stewardship	Société Environnementale de Clarence-Rockland
City of Ottawa, French Language Services	

Angela Taylor welcomed PCG attendees and introduced the co-proponents of the study as well as the members of the Study Team. The PCG attendees also introduced themselves. The presentation was jointly given by Angela Taylor, David Hopper and Valerie McGirr.

A total of 15 people attended the Public Consultation Group meeting. A summary of the discussion is summarized in **Table 6**.

Table 6. Summary of Discussion at the Public Consultation Group Meeting

SUMMARY OF COMMENTS – WEST OF TRIM ROAD
Q: Is the car passenger drop-off at Jeanne d'Arc unique to the LRT?
A: <i>There are three park and ride lots along the line. There is a more formal kiss and ride at Jeanne d'Arc, and we are looking at expanding that to the other stations. We always look for dedicated passenger pickup and drop off space, ideally in manner that does not conflict with bus or vehicular traffic.</i>
Q: You said it is possible to go to Trim – based on savings?
A: <i>We have a dedicated budget – \$500 million – that we are trying to make go as far as possible. We will be doing a cost estimate on both terminus options – Place d'Orléans and Trim.</i>
Q: If money becomes available, is the city open to advancing the extension to Trim?
A: <i>Councillor Blais – Yes. It is in the TMP specifically on the basis that the other levels of government said they are interested in contributing.</i>
Q: Can you provide more detail on the multi-use pathway – and how it may be integrated into the cycling infrastructure system?
A: <i>We believe there is space for pathways most of the way. One of the challenges is the Greenbelt portion. We will have further internal and external discussions on the challenges this poses. The intent is to find something parallel to LRT to achieve maximum connectivity.</i>
Q: To what standard are the elevators accessible? Can they accommodate a stretcher?
A: <i>They exceed AODA standards. We don't know about their ability to accommodate stretchers. We are working alongside emergency services to determine the best course of action.</i>
SUMMARY OF COMMENTS – EAST OF TRIM ROAD
Q: If you go to England, there are 2-3 lane roundabouts in villages with no conflict. It is subjective to say one is more appealing to a village. Look at others to inform your design.
A: <i>Various examples have been considered.</i>
Q: That home in the north-east corner of Cameron would have to be purchased in the case of a roundabout?

A: Yes.
Q: Can the traffic signals be accommodated within existing land ownership?
A: <i>There are some land requirements. The signalized intersection option is much more compact.</i>
Q: Why isn't Brian Coburn used as an arterial? The study area didn't include this.
A: <i>Our Alternative 3B was a connection to Brian Coburn.</i>
Q: We live on the 174. There are certain times we cannot safely get out going right. The only way we can get out is when traffic lights down road from us provide a gap, and we already have a consolidated driveway. I am a fan of roundabouts, but this concerns me.
A: <i>Traffic lights will provide breaks in peak hour traffic.</i>
Q: So the existing highway now is in the middle of the new highway, or is the road widened to the south?
A: <i>the location of the widening depends on the location. In some cases we are widening to the south to avoid impacts to the Ottawa River and minimize property requirements.</i>
Q: How much rock removal will be required to make it wide enough there? That will be an expensive proposition.
A: <i>Refined volumes will be determined during the next design phase.</i>
Q: Does the route of the service road require land?
A: <i>There is land required. This will need to be confirmed later on in detailed design.</i>
Q: Are you proposing multi-use pathways on both sides of the 174 in some locations?
A: The service road would act as the pathway in some areas, and similarly with the paved shoulders. However we are also looking at connections to other multi-use pathways along the 174.
Q: How do you see the speeds along the pathway section in this section? I would not allow my kids bike or walk along a paved shoulder of the 174.
A: <i>We do not determine speed limits in the EA - this is ultimately left up to the road authority. We are investigating the opportunity to maximize off road pathways to ease safety concerns.</i>
Q: There is unstable soil between Trim Road and Cumberland. Where is it located?
A: <i>It is located near the boat launch. It has been studied, and it was determined that the soil could be stabilized should construction ever need to occur along the 174.</i>
Q: If I'm commuting back at the beginning and end of each day, won't all these traffic lights hinder any improvement to capacity?
A: <i>There aren't many properties accessing these signals and so the impact will not be significant. You will generally have most green time favoring the east-west traffic, and many of the signals will not be triggered unless a vehicle is waiting to turn. The gain in capacity isn't 100% of what the widening offers, but it is 80%. This is still sufficient to manage the predicted growth.</i>
Q: How can a lane be designated HOV in a 2 lane road?
A: <i>The two additional lanes could be HOV. Additional width is being protected for the required buffers. This will be evaluated when the time comes to determine what is best for the capacity and performance for the road. The intention of HOV lanes is not solely for buses, we can use this to encourage car with more than one person and increase road capacity.</i>
Q: If you install a roundabout at Cameron, people will slow down right at Mr. Gas entrance, and there will be conflicts.
A: <i>Conflict points will have to be identified and appropriately managed.</i>
Q: For the signal option, wouldn't there be an isolated right turn lane onto Cameron?
A: <i>Currently there is no need for this. However if closer to implementation our traffic analysis supports this, then the design can be adjusted accordingly.</i>
Q: As a member of the community association, I am uncomfortable casting a vote in favor of either roundabouts or signals. The community has to provide their own opinion.
A: <i>In terms of consensus – we are not holding anybody to whatever is said tonight. We are seeing overwhelmingly support for traffic signals at meetings held to date. We encourage you to talk to your associations, and your neighbours. We will continue to consult, and everybody will have the opportunity to provide their input.</i>

Q: Why is there a need for two multi-use pathways in Cumberland at Cameron? Seems like the usage of these pathways would not warrant the additional property.
A: <i>Having two pathways provides full accessibility for residents on the north and on the south. In the EA process are protecting for the maximum right-of-way so not to rule anything out. The location of pathways will be confirmed during detailed design.</i>
Q: I live north of the 174. Is it possible to have median breaks so I may cross the highway to visit my neighbour?
A: <i>It is inadvisable to cross the road except at a formal crossing.</i>
Q: There is a volume issue and it has to be addressed. City has said they consider 2031 volumes, and I suspect that is because the LRT priority. My question is does this design have capacity sufficient for an additional 20 years beyond 2031, or will there need to be further expropriation down the road?
A: <i>We have only predicted to 2031 because we can only predict growth reliably within a certain timeframe.</i>
Q: Do we have a cost estimate? How do we know the extent of the price differential between the options? I am concerned that the cost is enough to push it further back – I think it is premature to plan this until you get a more finalized estimate or dollar value for each option. We may be making this project unreasonable to be funded.
A: <i>While we do not have detailed estimates, we have a good idea of the magnitude of difference between all options. We must carefully differentiate between widening costs and upkeep costs. Doing nothing along 174 would still require rehabilitation and reconstruction expenditures, which are included to a certain extent in the expansion. It is not a direct comparison between new construction and the widening.</i>
Q: Does the Quigley Hill intersection impact the golf course?
A: <i>We do not require active golf course land as part of this, just unused portions of the property.</i>
Q: Is the Barnett Street extension a certainty?
A: <i>It is being planned for. The road opening has been approved pending traffic lights, which would be provided depending on the construction of the gas station. It is a requirement in building the gas station.</i>
Q: The slopes at Cameron & 174 are quite steep. Have you considered this?
A: <i>We have, and would use retaining walls where necessary.</i>

Comments from the PCG were considered by the Study Team in the Frequently Asked Questions provided in Appendix D and in the development of the documentation of the EA studies.

3.4 Aboriginal Consultation

Aboriginal groups were invited to attend the Agency Consultation Group meeting on April 13, 2015 or to contact the study team to discuss an alternative consultation format that better suited their needs. Invitation letters were mailed to:

- Algonquins of Ontario Consultation Office
- Algonquins of Pikwakangan
- Kitigan Zibi Anishinabeg First Nation
- Métis Nation of Ontario

Aboriginal groups were also sent a copy of the public open house notice as part of the general mailing list. The project team is working with the Algonquins of Ontario Consultation Office with respect to their interests along the corridor.

Additional contact is planned to follow up on aboriginal interests during the EA studies.

4 PUBLIC OPEN HOUSES

One of the key objectives of the environmental planning process is to provide the public, interested parties and affected agencies with opportunities for meaningful input. To help ensure this objective is met, comprehensive public and agency notification of the second round of Public Open Houses (POHs) was undertaken. Newspaper notices were placed in local newspapers on April 10, 2015 and April 17, 2015. In addition, the notice was placed on the project websites at:

www.prescott-russell.on.ca/en/public-works/major-projects

or

<http://ottawa.ca/en/city-hall/public-consultations/confederation-line-east-light-rail-transit-lrt-extension>

The notice was also mailed or emailed to individuals identified on the study contact list. A copy of the English and French notice is provided in **Appendix A**.

Due to the size of the study area and the number of communities affected, Public Open House meetings at three different locations on separate nights were included in the process. The information presented and presentation format were generally the same at each meeting with an emphasis on the areas of interest of attendees. Bilingual Information bulletins (handouts) were available at each POH (provided in **Appendix B**).

The POH meetings were held from 6:00 p.m. to 9:00 p.m., with a presentation at 7:00 p.m. on:

Monday, April 20, 2015	Tuesday, April 21, 2015	Thursday, April 23, 2015
Community Pentecostal Church 1825 St. Joseph Boulevard Orléans	R. J. Kennedy Community Centre Hall A & B, 1115 Dunning Road Cumberland	Clarence Creek Community Hall 418 Lemay Street Clarence Creek

At the Public Open House locations, the following was presented:

- Functional design of the preferred integrated LRT and HWY 174 widening west of Trim Road
- Functional design of the widening of HWY 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

Display panels were prepared to aid the explanation of project progress and to highlight key issues. The Open House information was posted to the project website. Project staff members were on hand to answer comments and questions, and directed individuals with other comments to provide them in writing on available comment sheets or by email until May 15, 2015. A real estate officer from the City of Ottawa was also present to discuss the future property acquisition process with residents who may be affected.

Open House Comments

The following table provides a summary of open house concerns expressed by attendees of the April 2015 Open Houses. Comments provided by email or on comment sheets left at the POH are provided in Appendix C.

4.1 Public Open House in Orléans

A total of 80 people signed the registration sheet at the Public Open House meeting held in Orléans. Before the formal presentation, attendees were encouraged to review the bilingual display material and discuss the studies with staff. Bilingual members of the study team were available for discussions.

Angela Taylor, City of Ottawa, welcomed attendees and introduced the Study Team. A PowerPoint presentation was given jointly by Angela Taylor, David Hopper and Valerie McGirr. A question and answer period followed the presentation and discussions are summarized below in **Table 7**.

Table 7. Summary of Questions & Answers at the Public Open House in Orléans

Summary of Questions & Answers – POH in Orléans
Q: How will the new stations be built to ensure people will be comfortable during the winter? A: Suppliers of the vehicles are modifying their trains to accommodate our climate and include air conditioning and heating. Portions of the stations are enclosed and will achieve solar heating and wind protection through the glass enclosures. Additionally, there will be heating in stations with infrared heat.
Q: Are you examining solar heating options for the stations, or other new technology? A: The industry as a whole is looking at these opportunities. In their bid process for construction, the City may consider this.
Q: Regarding the new lane for westbound traffic at Trim. What kind of capacity does this lane have? A: There will be 3 lanes in each direction on the freeway. The project to widen HWY 174 is not in the affordable plan for 2031. The integrated planning process accommodates adequate space for the highway expansion and the LRT.
Q: What kind of parking will there be at Trim? Also, what kind of commercial development can we expect to see within the stations? A: Park and Ride lot capacity will depend on the location of the terminus of the LRT. Structured parking may be considered at Place d'Orléans. Developers are working with the City and OC Transpo to develop these opportunities. There is not going to be much retail space in the stations as the demand for business in stations is typically not high. Also, station retail should not compete with nearby shopping centres. You may see a kiosk in some of the busier stations.
Q: Will there be bathrooms before Blair station? A: The line is being designed to building code with washrooms at terminal stations. Blair will have them when it opens in 2018. Place d'Orléans will when it opens, if it opens as the terminus. And eventually, Trim Road.
Q: The expansion of the highway from Place d'Orléans to 10th Line is already quite noisy from the traffic. What are your plans to deal with noise from increased traffic from this proposed widening, and the frequent train travel? A: Noise evaluation is done in 2 stages – first existing conditions are evaluated and then future conditions are predicted based on the movement of lanes from LRT. Noise impact for the widening and LRT will be examined separately.
Q: At Place d'Orléans there is potential for development now. Is this being explored for the expansion of the park and ride? A: The property is not owned by the city. Discussions with the developer will be initiated to see if there is a collaborative plan that works for everyone.
Q: Are ridership estimates based on Tap and Go? A: Ridership estimates are large scale for the entire Ottawa region, and are based on employment and population forecasts, with consideration for societal impacts and demands on people's time (where they want to get, how, and how quick). The models are based on development estimates for the future. There is enough ridership to meet minimum light rail requirements, and light rail will increase ridership in and of itself.
Q: Is there any plan for transit infrastructure to the south of the City? A: There is the Cumberland Transitway that will run south of Innes, as well as the Trillium line (O-Train) to service

Summary of Questions & Answers – POH in Orléans
<i>the south. Right now travel in Ottawa is predominantly east-west, but there are plans to make connections to the south.</i>
Q: I'm surprised to see that Orléans Boulevard station is being carried forward. A: <i>Orléans Boulevard provides high connectivity with east, west, north, south neighbourhoods.</i>
Q: Will there be bicycle facilities at each station? A: <i>Every station will have bicycle racks, the size and design of which will depend on the levels of cycling in the neighbourhood. Cycling groups are participating in the study.</i>
Q: Will the trains have electronic signage to inform users where their end destination is? A: <i>Yes – the design will be determined during design/implementation.</i>
Q: Are you allowed to take your bike on the train? A: <i>Not during peak hours. Off peak hours they may be permitted. This had been done to reserve priority seating for wheelchairs, mobility devices, service animals, and strollers.</i>
Q: How will you alter Jeanne d'Arc Bridge to make room for dropping off and bus service? A: <i>We are examining all options to make each station a better pedestrian space than they are today. This could include a dedicated lane for bus lay-by, kiss and ride, cycle tracks and wider sidewalks.</i>
Q: Are you going to close down the free flow ramp on the north-west corner of Jeanne d'Arc, for cars travelling south on Jeanne d'Arc accessing the 174 to go west? A: <i>This ramp will remain. The bus only intersection will be removed following the conversion to LRT.</i>
Q: Are you going to have public access to the Orléans Boulevard station from the north? A: <i>Access opportunities will be in all directions.</i>
Q: Why not reduce the speed limit? It would reduce the noise. A: <i>The existing speed limit is not planned to be changed.</i>
Q: The lighting is very poor east and west of the Montreal Rd exit. It is extremely difficult to drive at night. A: <i>The City of Ottawa has a lighting group that is examining 3 lane freeways and lighting – no policy currently exists. If new standards are not developed by the City, MTO standards may be followed.</i>
General Comments: <ul style="list-style-type: none"> - <i>I think there is an opportunity to charge cyclists for parking</i> - <i>I think that congestion at the 174/417 Split will still be a problem</i>
Q: It's pretty tight with the escarpment along your study area. Do you have room to expand the road without moving the hydro poles? A: <i>It is close, but yes – we have adequate space.</i>
Q: Did you consider the traffic impact from Buckingham? A: <i>Yes, regional traffic has been considered.</i>
Q: Is Kinsella planned to be a traffic light or roundabout? I would appreciate you consider a roundabout. A: <i>Currently it is planned as a traffic signal.</i>
Q: Do I understand that there will be no widening until 2031? A: <i>Currently work is scheduled for some time after 2031. However if funding can be secured before then, there is the possibility of an earlier start date.</i>
General Comments: <ul style="list-style-type: none"> - <i>If you look across the river towards Quebec, they've pretty much gone through the roundabout discussion. It's really slowed traffic down. Your design features could be informed by them.</i> - <i>For looking 35 years in the future, your work is to be commended.</i>

4.2 Public Open House in Cumberland

A total of 83 people signed the registration sheet at the Public Open House meeting held in Cumberland. Before the formal presentation, attendees were encouraged to review the bilingual display material and discuss the studies with staff.

Angela Taylor welcomed attendees, introduced the Study Team. David Hopper gave the presentation for the HWY 174 widening and LRT construction from Blair to Trim. Valerie McGirr gave the 174/17 widening presentation for the portion east of Trim. A question and answer period followed the presentation and discussions are summarized below in **Table 8**.

Table 8. Summary of Questions & Answers at the Public Open House in Cumberland

Summary of Questions & Answers – POH in Cumberland
QUESTIONS AND ANSWERS – EAST OF TRIM ROAD
Q: How did we get from the last meeting to here? How was this decision made? A: <i>When the last meeting was held in February, the study team had consulted with specialists and City staff. Four routes were evaluated using 63 criteria - such as impacts to wildlife, costs, property impacts, social impacts. The 174/17 emerged as the preferred corridor even through sensitivity analysis. This was the preferred alternative that was presented at that meeting.</i>
Q: Anybody that is mathematical can make anything work to their satisfaction with manipulation. You can take these criteria and work it in many different ways. A: <i>A sensitivity analysis was done as part of the evaluation, from which the widening of the 174 / 17 emerged as the preferred alternative. There was then a reasoned analysis that examined how each alternative supports the city policies from both Prescott-Russell and the City of Ottawa. The widening of the 174 was preferred through the reasoned analysis as well.</i>
Q: What will be done for the people on the south of the 174 on the escarpment as a result of the noise and vibrations of the widened highway? A: <i>The vibration and noise impacts are being examined and where required, mitigation measures will be implemented.</i>
Q: Will the homes that have direct access retain their access to the 174? A: <i>These properties will become right-in, right-out only. They would make a legal U-turn at the nearest intersection in place of making a left turn.</i>
Q: Why are we widening the 174? A: <i>The TMP that was updated in 2013 identifies a widening of the 174/17 is needed. It is not however identified under the affordability plan before 2031. Before any work is done, an EA must be completed – determining how the project will impact existing social, natural, economic conditions. The EA allows for the protection of the corridor, and for the project to commence as soon as funding becomes available.</i>
Q: What is your anticipated pedestrian and cyclist traffic flow per hour on the 174 through Cumberland? A: <i>There are no pedestrian or cyclist numbers available for 2031, but the number of people the pathway would serve would be considerable.</i>
Q: My concern is that there is not a legitimate requirement for these pathways, particularly on both sides of the road. A: <i>During the consultation and during the Cumberland vision exercise, residents expressed a desire for a community accessible to all modes. In detailed design there may only be a pathway on one side of the 174, but it is shown now to protect for the property required and to keep options open.</i>
Q: This will reduce the value of the properties on both north and south side of the 174. Why wasn't Innes looked at from more of a property value perspective? A: <i>Four alternatives were evaluated against multiple criteria. Property requirements was one of the criteria used during the multi-faceted evaluation. The widening of the 174 emerged from this evaluation, and the sensitivity</i>

Summary of Questions & Answers – POH in Cumberland
<i>analysis, as the preferred option.</i>
Q: Wouldn't Innes be better from a property value perspective?
A: <i>Until the exact property requirements and project design are determined, the impact on property values cannot be estimated.</i>
Q: What would the speed limits be in this proposed plan?
A: <i>80 km/h has been assumed in the rural section, and 60 km/h through the urban areas. However this is determined by the road authority at a later phase.</i>
Q: What is the projected number of cars in 2031?
A: <i>Based on the traffic model, 2300 cars an hour during peak hour is anticipated for 2031. This equates to roughly 23,000 a day.</i>
Q: Is there a plan for sound barriers?
A: <i>The impacts of noise will be assessed, Where there is need for noise barriers, and where installation is feasible, it will be considered.</i>
Q: Is the potential for another route dead?
A: <i>Yes – the previous routes will not enter back into consideration.</i>
Q: There will be 9 lights from Trim to Walmart. How much can this be expected to increase the capacity of the road?
A: <i>These lights are all actuated signals – meaning they will not change unless a car triggers the light. The capacity of the road has been modeled with these considerations, and it will still provide the anticipated capacity.</i>
Q: When was the last meeting?
A: <i>Around February 2-3, 2015.</i>
Q: You have been developing this as the preferred the entire time, even while the alternatives were presented.
A: <i>Four options were put forward that were all fairly well developed to the same level, and were presented to the public with the same level of analysis. At the conclusion of the previous meeting, the widening of the 174/17 was presented as the technically preferred option. The work seen today has built on the work done for the February meeting. There are multiple opportunities to comment further on the plan presented today.</i>
Q: What is the long term vision for what's happening in Rockland, and how will people benefit along this stretch from these projects?
A: <i>The Clarence-Rockland bus service today goes all the way to downtown and comes back empty. As LRT is extended, there can be better, more frequent, and more reliable public transit from Rockland to the downtown core as the buses will now only have to go as far as the railhead to transfer passengers onto the LRT. People will transfer as the train will get them downtown faster and more reliably. The LRT will benefit transit services in Clarence-Rockland as their travel distance will be reduced, and more bus trips can occur in the same time period with the same number of bus hours.</i>
Q: You have to have more park and rides.
A: <i>If there is a demand to support it, additional park and ride lots will be examined.</i>
Q: I am not convinced that your traffic model is accurate. Does it take into account traffic lights and speed changes?
A: <i>Yes, it does. Building additional transit infrastructure builds the transit capacity, and widening the road works in tandem with transit to reduce the traffic volume throughout the city. The model for the whole region considers where people live, how people travel, how people link trips, etc. It is calibrated to see if it mimics behaviour today. Development scenarios are then inputted, and behaviour is applied. This has been found to mimic behaviour that is observed today (to within 1-3 percent), and generates confident travel data for the TMP.</i>
Q: At every meeting I have suggested building an arterial road south, as that is where Ottawa and Rockland are developing.
A: <i>While the City itself is growing south, the 174/17 corridor remains stressed. It is at capacity and subsequently, traffic is filling up alternative routes. Capacity needs to be increased on the existing road before other roads are built or expanded. The traffic model takes into account potentially expanded roads elsewhere and it does not change the result: people still take the 174/17 and it remains overloaded.</i>

Summary of Questions & Answers – POH in Cumberland
Q: Do you have an estimate on the length of the construction period?
A: <i>This will be developed in design.</i>
Q: Will a project of this magnitude be done in phases or one shot?
A: <i>It will likely be done in phases. This will be confirmed in detailed design.</i>
General Comments:
<ul style="list-style-type: none"> - Consider overpasses so people can travel underneath the highways to the river or service roads - All you're doing is forcing people to go into Orléans rather than Cumberland. - You do not have the driveway that was given to my family on your plan. There is also another lot east of Barnett with an access missing. - We have no say – all these meetings are irrelevant and you don't listen to us anyways.
QUESTIONS AND ANSWERS – WEST OF TRIM ROAD
Q: How do you determine the need for Park and Rides? Building them may increase demand.
A: <i>A predictive model is used. Park and Ride lots will exist at the terminus. If the need is identified, then park and ride lots will be examined for expansion.</i>
Q: Would you take parking further out past Trim? Is there potential for structured park and ride in Cumberland?
A: <i>Right now it is expected that rail will go no further than Trim – and as such the larger parking facilities will be located at the terminus (first Place d'Orléans and then Trim). If there is a need for additional parking out in Cumberland or Rockland, this will be examined separately as part of OC Transpo and Clarence-Rockland Transit's operational planning.</i>

4.3 Public Open House in Clarence Creek

A total of 91 people signed the registration sheet at the Public Open House meeting in Clarence Creek. Before the formal presentation, attendees were encouraged to review the bilingual display material and discuss the studies with staff.

Marc Clermont of the United Counties of Prescott and Russell welcomed attendees and introduced the Study Team. A PowerPoint presentation was given jointly by David Hopper, Valerie McGirr and Patrick Déoux. The presentation was provided in both English and French. A question and answer period followed the presentation and discussions are summarized below in **Table 9**.

Table 9. Summary of Questions & Answers at the Public Open House in Clarence Creek

Summary of Questions & Answers – POH in Clarence Creek
QUESTIONS AND ANSWERS – EAST OF TRIM ROAD
Q: How much will it cost? And does that cost take inflation into account?
A: <i>Currently there is \$500 million allotted for the LRT to go from Blair as far east towards Trim as possible. The cost estimate for the 174/17 portion is being determined as the need to replace existing structures and build new structures is finalized.</i>
Q: What is the impact of the lights on commuting time?
A: <i>Most of the traffic lights are actuated, and will only stop CR 17 traffic if there is traffic on side streets. The lights have been considered in the traffic analysis, and the widening still provides the capacity required.</i>
Q: The expansion is planned 16 years from now. By then we will need 6 lanes – I am concerned it is not going to be big enough.
A: <i>The transportation model for the national capital region looks at where people live and work to determine travel patterns. The model is calibrated to within 1-3 percent accurate of today's patterns. It indicated that in 2031 an additional lane will be needed. The additional lane will provide better function and better capacity.</i>

Summary of Questions & Answers – POH in Clarence Creek
Q: Why have you made no suggestion of a three lane highway with a reversible lane? Especially considering the expansion is so far away and uncertain. A: <i>This section is not appropriate for a reversible lane. The need for overhead signs, infrastructure, and the number of driveways on the 174/17 negate this option. Lane indication signals would need to be installed every 200 metres, and there would be substantial operational costs for a 20 km stretch. This is not a viable option, and therefore it has not been carried forward.</i>
Q: I do not believe the property lines west of Laporte are correct. A: <i>Please contact the study team with your concerns and the property lines will be verified.</i>
Q: When is this study to be completed? A: <i>Winter of 2015-2016.</i>
Q: So after that this goes to city for approval and comments? Would it be reasonable to say that everything goes the way you say, we will need to redo this study in 2031? A: <i>An update will be done to the EA should the conditions detailed today change substantially. It is a fairly simple and expedited process to submit a revision. The advantage of completing the EA Study now is that the land required can be protected so that when funding is available the project can go ahead. Until the EA is approved, the City and the United Counties cannot acquire land.</i>
Q: How long would it take to do design, bidding, and then construction? A: <i>Once this is determined, it will be made available. Other projects similar to this have mobilized quickly once funding becomes available.</i>
Q: Three lane highways are done all over the world. Why can't one be done here? A: <i>In Ontario there cannot be controlled access provided on a three lane highway. It is not recommended, and not safe from an engineering perspective.</i>
Q: Have you talked to anybody in Clarence Rockland on your projected numbers? A: <i>Yes, the regional model included data from all of the surrounding counties. The growth plans are amalgamated and patterns in the National Capital Region are accounted for. Professionals from each jurisdiction – planners and engineers - consult on this model and provide input to represent their community.</i>
Q: What is the expected speed limit? A: <i>80 km/h has been assumed in the rural section, and 60 km/h through the urban areas. However this is determined by the road authority at a later phase.</i>
Q: What about a lane reserved solely for buses? A: <i>There are not enough people taking the bus this far out to warrant building a lane solely for buses.</i>
Q: Have you considered an overpass at the ferry at Cameron Road? A: <i>It is not in keeping with the Village and would encourage higher speeds. Traffic volumes do not warrant the cost and land requirements for an overpass.</i>
Q: Why did we choose the most expensive option? The Baseline option made more sense and was cheaper. A: <i>Many indicators were considered in the evaluation of the alternatives – including cost. However the widening consistently emerged as the preferred alternative.</i>
Q: What is the difference in cost between Baseline and the widening? A: <i>The 174/17 is comparatively more expensive.</i>
Q: Is there a chance to improve the road in the short term? A: <i>The City of Ottawa conducted a safety review all along the 174. Changes have been implemented already in certain places such as Quigley Hill Road, and more changes are planned for other identified areas. There will be improvements between now and the commencement of the widening.</i>
Q: How will Cardinal Creek Village access the 174 in this plan? A: <i>Initially Cardinal Creek will use Old Montreal Road. Eventually, there will be a signalized intersection on the 174 to provide another access.</i>
Q: We need to consider removing the bus stops along the 174/17. These bus stops cause accidents.

Summary of Questions & Answers – POH in Clarence Creek
A: <i>The service available today will not be changed at this point and time. Safety issues stemming from the bus stops did not emerge from the safety review of 2009.</i>
Q: Will a project of this magnitude be done in phases or one shot? A: <i>It will likely be done in phases. This will be confirmed in detailed design.</i>
General Comments: <ul style="list-style-type: none"> - <i>I suggest you move the entrance to Voisine, as it is very difficult to get out because of the traffic light.</i> - <i>You should have a pathway west of Voisine to Laporte.</i> - <i>Everyone here needs to work together and lobby our politicians for more support to get this done as soon as possible, or to transfer the road back to MTO.</i> - <i>A three lane highway would serve the whole community here. If we can't get four lanes, at least three lanes would provide some relief.</i> - <i>If we don't have plans in place, we will miss out on the money. I think these plans proposed tonight are a big part of it.</i> - <i>I think the cost will be prohibitive in getting this project done.</i> - <i>Thank you for the presentation and information tonight</i> - <i>When this city gets bigger, and it will, we need a limited access highway</i>
QUESTIONS AND ANSWERS – WEST OF TRIM ROAD
Q: How did you estimate usage of the LRT extension? Will enough people be using it, because you could use that \$500 million on the highway widening? A: <i>The model looks at how many people choose transit and how many people will drive. It considers economic status of individuals, and many social behaviours. In Orléans and within the extent of the LRT, the peak hours usage jumps up to 7000-8500 people an hour. This is the equivalent to a 5-6 lane highway. Additionally, the percentage of people in Orléans who choose transit is around 35%. This percentage will continue to grow, and is too high to be handled effectively by the bus system.</i>

4.4 Summary of Comments Submitted

4.4.1 Summary of Comments Submitted before the POHs

Following the publication of the report covering the February consultation events, an additional 17 comments were received by mail or email past the deadline for comments to be incorporated. A summary of these comments is provided in **Table 10** and copies of the submissions, excluding personal information, are provided in **Appendix C** of this report.

Table 10. Summary of Comments Submitted after the February Public Open House Meetings

Description of Comments	Correspondence Log (#)	Number of Comments by Topic
Noise & vibration, property access, wells & septic, archaeology/heritage issues	3, 7	8
Evaluation Criteria sensitivity/ weightings/ Performance Metrics	4, 13	4
Request for Boards/ Presentation Materials	8, 9, 17	4
Add to mailing list	14, 15	3
Environmental issues – agricultural, City of Ottawa, geology	13, 16	3
Transit – LRT station placement, number of stations	1	2
Transit – Local routes and transfers to LRT	6	2
Open House – general comments (venue, presentation, etc.)	10	2

Description of Comments	Correspondence Log (#)	Number of Comments by Topic
Supports Light Rail Transit (LRT) proposed alignment	1	2
Transit – Support for extending to Trim Road and/or beyond	1	2
Accessibility – priority seating, public washrooms	2	2
Support for other alternatives	4, 13	2
Costs should be identified	16	1
Build new road south of HWY 174 / ring road where development is occurring	5	1
Opposed to widening Ottawa Road 174/route or LRT	12	1
Traffic bottleneck points identified, lack of existing capacity	16	1
Construction Timing	16	1

4.4.2 Summary of Comments Submitted at the POHs

The log of comments and copies of the submissions, excluding personal information, are provided in **Appendix C** of this report. The six comment sheets submitted at the Orléans POH are summarized in **Table 11**.

Table 11. Summary of Comments Submitted at the Public Open House in Orléans

Description of Comments	Comment Sheet #	Number of Comments by Topic
Transit – Support for extending to Trim Road and/or beyond	2,3,5	3
Supports Light Rail Transit (LRT) proposed alignment	1,3	2
Cycling/ Pedestrian issues	5	1
Accessibility – priority seating, public restrooms, signage	4	1
Future capacity/ Growth projections	2	1
Traffic signals	1	1
LRT stations – infrastructure, landscape, security	6	1

The ten comment sheets submitted at the Cumberland POH meeting are summarized in **Table 12**.

Table 12. Summary of Comments Submitted at the Public Open House in Cumberland

Description of Comments	Comment Sheet #	Number of Comments by Topic
Noise & vibration, property access, wells & septic, archaeology/heritage issues	8,13,15,16	4
Cycling/ Pedestrian issues	10,11,14	3
Roundabout concerns	7,10,13	3
Property Impacts and expropriation concerns	9,13	2
Open House – general comments (venue, presentation, etc.)	15	1
Traffic signals	7	1
Construction timing	8	1
Retaining walls and traffic barriers	13	1
Traffic volume, speed	13	1

The nineteen comment sheets submitted at the Clarence Creek POH are summarized in **Table 13**.

Table 13. Summary of Comments Submitted at the Public Open House in Clarence Creek

Description of Comments	Comment Sheet #	Number of Comments By Topic
Roundabout concerns	20, 22, 24, 27,29, 30, 33, 34, 35	9
Traffic signals	21, 24 , 25, 26, 27, 29, 30, 35	8
Supports widening	21, 22, 24, 27, 32	5
Traffic volume, speed	17,18, 20, 22, 26, 29, 32	7
Construction timing	18, 20, 24, 31, 33	5
Property Impacts and expropriation concerns, access	19, 22, 25, 31	4
Cycling/ Pedestrian issues	20, 21, 26, 28	4
Commuting length/ timing	18, 21, 26, 33	4
Other infrastructure - overpasses, collectors, on ramps	25, 24, 33	3
Safety	32, 33, 35	3
Future capacity/ Growth projections	18, 22	2
Parking – park and ride facilities	18, 23	2
Road realignments	17	1
Transit – Local routes and transfers	31	1
Noise & vibration, property access, wells & septic, archaeology/ heritage issues	17	1
Re-route tractor trailers to Highway 417	24	1

4.4.3 Comments Submitted after the POH Meetings

An additional 21 comments were received by mail or email following the public open house meetings. A summary of these comments is provided in **Table 14** and copies of the submissions, excluding personal information, are provided in **Appendix C** of this report.

Table 14. Summary of Comments Submitted after the Public Open House Meetings

Description of Comments	Correspondence Log (#)	Number of Comments by Topic
Noise & vibration, property access, wells & septic, archaeology/heritage issues	2, 3, 7, 9, 12, 15, 16, 20	8
Evaluation Criteria sensitivity/ weightings/ Performance Metrics	3	1
Request for Boards/ Presentation Materials	11	1
Intersection/ Roundabout Configuration	11	1
Add to mailing list	1	1
Environmental issues – agricultural, City of Ottawa, geology	14	1
Cycling/ Pedestrian issues	9, 14	2
Supports widening Ottawa Road 174	4, 5	2
Transit – LRT station placement, number of stations	14	1

Description of Comments	Correspondence Log (#)	Number of Comments by Topic
Transit – Local routes and transfers to LRT	14	1
Open House – general comments (venue, presentation, etc.)	6	1
Supports Light Rail Transit (LRT) proposed alignment	9	1
Transit – Support for extending to Trim Road and/or beyond	9	1
Accessibility – priority seating, public washrooms	6	1
LRT Route Selection	2, 3, 11	3
Parking – existing capacity at park and ride facilities is inadequate	14	1
Traffic bottleneck points identified, lack of existing capacity	7, 13	2
Transit – Bridge infrastructure replacement needed for stations	14	1
Emergency Turnarounds on Highway 174	8, 17	2
Safety Concerns	17, 18	2
Road Configuration	18	1
Land Expropriation	19	1

4.4.4 Frequently Asked Questions

Questions that were frequently raised during the consultation process, including consultation group meetings and Open Houses and corresponding responses from the Study Team were summarized, translated and placed on the web site. These are provided in **Appendix D**.

5 CONCLUSION

There was general agreement at the public open houses that action is needed to address the traffic conditions/ congestion on Highway 174 and County Road 17 in the study area. Participants expressed concern about the length of time before municipal funding is expected to be available for improvements to HWY 174 and CR 17.

Many participants were excited about the extension of LRT from Blair Station to Trim Road and asked about details of the system and how the transit network would change with the introduction of LRT to Orléans. They were also supportive of highway widening from Highway 417 to Trim Road. They recognized and requested that measures be included to mitigate noise and vibration, provide additional park and ride spaces and bicycle parking at LRT stations and to mitigate other environmental impacts resulting from this construction.

Many participants from Cumberland east of Trim Road and from Clarence-Rockland were concerned with the potential impacts of the proposed road widening with respect to noise, property, access and out-of-way travel, and cost. Some residents saw a benefit from the increased safety provided by the new locations where they could enter the highway at traffic signals. Most people expressed a preference for the design with the traffic signals in Cumberland Village rather than the roundabouts. People with driveways along HWY 174-CR 17 noted that existing traffic lights provided them with gaps to enter the road.

People using the highway for commuting were supportive of the widening and maintaining a good level of service along the corridor, while people living along the road were interested in measures to manage speeds and to discourage more traffic. The study team worked to balance these competing perspectives in the functional design of the preferred plan by incorporating mitigation measures that reflect good engineering and environmental practices.

Appendix A

Summary Report for April 2015 Consultation

- Public Notices Round 3

**Confederation Line East Light Rail Transit (LRT) Extension
Ottawa Road 174 / Prescott-Russell County Road 17 Widening
Environmental Assessment Studies
Open Houses
6 to 9 p.m., presentation at 7 p.m.**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

You are invited to the final Open Houses to review and provide feedback on the recommendations for these two environmental assessment (EA) studies. The Confederation Line East LRT Extension study is following the Transit Projects Assessment Process and the OR174 and Prescott-Russell County Road 17 (CR17) corridor is under the Municipal Class EA process for a Schedule 'C' project.

Open House presentation and displays will include:

- Functional design of the preferred integrated LRT and OR174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

The East LRT extension is one of three EA studies underway as part of Stage 2, the City's plan to extend the benefits of rail to residents farther east, west and south that will add 19 new stations and 30 kilometres to the O-Train system– see ottawa.ca/stage2 for further information.

Accessibility is an important consideration to facilitate attendance at these Open Houses. If you require special accommodation, please call 3-1-1 or e-mail one of the project leads below before the event.

If you are unavailable to attend the Open House or would like additional information, please visit the study web site at ottawa.ca/easternlrt or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study or direct your comments and questions to the contact persons below. The Open House presentation material will be posted to the study web site. For further information on this project, or to be added to our mailing list, visit the web site or contact:

East LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

OR174/CR17 Widening

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Funding for the OR 174/County Road 17 Widening EA Study is being provided by the Government of Ontario.

**Prolongement vers l'est de la Ligne de la Confédération du train léger sur rail (TLR)
Élargissement de la route 174 d'Ottawa et du chemin de comté 17 de Prescott-Russell
Études d'évaluation environnementale - Séances « portes ouvertes »
18 h à 21 h, présentation à 19 h**

Lundi 20 avril 2015
Église communautaire pentecôtiste
1825, boulevard St-Joseph
Orléans

Mardi 21 avril 2015
Centre communautaire R. J. Kennedy
Salles A et B, 1115, chemin Dunning
Cumberland

Jeudi 23 avril 2015
Salle communautaire de Clarence
Creek
418, rue Lemay
Clarence Creek

Nous vous invitons aux dernières séances « portes ouvertes » pour examiner et commenter les recommandations à l'égard de ces deux études d'évaluation environnementale (EE). L'étude portant sur le prolongement vers l'est de la Ligne de la Confédération du TLR est effectuée conformément au processus d'évaluation du projet de transport en commun, et l'étude portant sur le couloir de la route 174 d'Ottawa et du chemin de comté 17 de Prescott-Russell est effectuée conformément au projet de l'annexe C de l'évaluation environnementale municipale de portée générale.

La présentation et les expositions dans le cadre de la séance « portes ouvertes » comprendront :

- la conception fonctionnelle du tracé privilégié intégré du TLR et de l'élargissement de la route 174 à l'ouest du chemin Trim;
- la conception fonctionnelle de l'élargissement de la route 174 et du chemin de comté 17 à l'est du chemin Trim;
- un résumé des commentaires et des réponses découlant de la consultation de février 2015;
- les prochaines étapes.

Le prolongement vers l'est du TLR fait l'objet de l'une des trois EE en cours dans le cadre de la deuxième étape du plan municipal visant à faire bénéficier des avantages du train les résidents de la Ville qui habitent plus loin dans l'est, l'ouest et au sud. Ce plan ajoutera 19 nouvelles stations et 30 kilomètres au réseau de l'O-Train – voir ottawa.ca/etape2 pour obtenir d'autres renseignements.

L'accessibilité est un élément important à prendre en compte pour faciliter la participation à ces séances « portes ouvertes ». Si vous avez des besoins particuliers à cet égard, veuillez composer le 3-1-1 ou envoyer un courriel à l'un des responsables du projet indiqués ci-dessous avant la date de l'événement.

Si vous ne pouvez assister à la séance « portes ouvertes » ou souhaitez obtenir des renseignements supplémentaires, veuillez consulter la page Web ottawa.ca/tlrest ou www.prescott-russell.on.ca/fr/travaux-publics/projets-denverquire ou ottawa.ca/etuderoute174et17, ou adresser vos commentaires et questions aux gestionnaires de projet indiqués ci-dessous. Le matériel de présentation de la séance portes ouvertes sera affiché sur le site Web de l'étude. Pour obtenir d'autres renseignements sur le projet, ou pour ajouter votre adresse à la liste d'envoi, veuillez consulter le site Web ou communiquer avec les personnes suivantes :

Prolongement vers l'est du TLR/élargissement de la route 174 et du chemin de comté 17

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Élargissement de la route 174 et du chemin de comté 17

Marc R. Clermont, ing.
Directeur des Travaux publics
Comtés unis de Prescott et Russell
613-675-4661, poste 3100
Courriel :

Le financement de l'étude d'EE portant sur l'élargissement de la route 174 et du chemin de comté 17 est offert par le gouvernement de l'Ontario.

Appendix B

Summary Report for April 2015 Consultation

- Information Bulletins

Études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17

Aperçu

Le présent bulletin d'information a été préparé pour faire connaître les études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17 de Prescott-Russell. Il comprend de l'information sur : la conception fonctionnelle du tracé privilégié intégré du TLR et de l'élargissement de la route 174 à l'ouest du chemin Trim; la conception fonctionnelle de l'élargissement de la route 174 et du chemin de comté 17 à l'est du chemin Trim; et les répercussions et les mesures d'atténuation éventuelles.

Aperçu du projet de prolongement vers l'est de la Ligne de la Confédération du train léger sur rail

La Ville d'Ottawa entreprend une étude d'évaluation environnementale (ÉE) portant sur le prolongement du train léger sur rail (TLR) de la station Blair jusqu'au chemin Trim, le long du couloir de la route 174, conformément au processus d'évaluation des projets de transport en commun. Le prolongement du TLR vers l'est (TLRE) est l'une de trois études d'ÉE en cours à la 2^e étape, qui porte sur le plan de la Ville d'apporter les avantages du transport par train léger aux résidents des secteurs se trouvant plus à l'est, à l'ouest et au sud, qui ajoutera 19 nouvelles stations et 30 kilomètres de voies au réseau de TLR. Le TLRE assurera un service rapide et fiable aux collectivités de Beacon Hill, Blackburn Hamlet et Orléans, ainsi qu'à la localité rurale de Cumberland. On propose un total de sept stations entre la station Blair et le parc-o-bus du chemin Trim. L'emplacement de ces stations a été conçu pour raccorder les services actuels et à venir de transport en commun et les parcs-o-bus du secteur. Le secteur visé par l'étude pour ce projet s'étend du chemin Blair à l'ouest et longe le couloir de la route 174 jusqu'au chemin Trim à l'est.

Aperçu du projet d'élargissement de la route 174 et du chemin de comté 17

Les Comtés unis de Prescott et Russell, en partenariat avec la ville d'Ottawa, entreprennent une étude d'ÉE de catégorie A portant sur le couloir de la route 174 et du chemin de comté 17 (CR17) de Prescott-Russell à partir de l'autoroute 417 jusqu'au chemin de comté 8 (chemin Landry). Cette étude est réalisée conformément aux exigences de l'annexe « C » du document sur les évaluations environnementales municipales de portée générale. Le secteur visé par l'étude pour le projet de la route 174/chemin de comté 17 s'étend de la jonction avec l'autoroute 417 à l'ouest et longe le couloir de la route 174 jusqu'au chemin Trim, où le secteur visé par l'étude s'élargit pour englober plusieurs trajets de remplacement. Le secteur visé par l'étude pour le TLRE chevauche la portion de la route 174 située à l'ouest du chemin Trim.

Coordination de la démarche des évaluations environnementales

Pour trouver une solution optimale pour ces deux projets dont les secteurs visés par l'étude se chevauchent, on coordonne la démarche de ces évaluations environnementales. Il est important de coordonner la démarche des deux ÉE afin :

- de maximiser l'utilisation du couloir routier actuel;
- de minimiser les conséquences sur l'environnement;
- de minimiser les coûts de construction;
- de tirer le meilleur parti possible de l'information obtenue dans l'étude;
- d'intégrer les activités de consultation avec les intervenants;
- de simplifier le processus d'ÉE;
- d'échanger de l'information, les travaux de conception technique et les consultations avec les intervenants.



Secteur visé par l'étude pour le TLRE et la route 174/chemin de comté 17

Processus d'étude d'ÉE

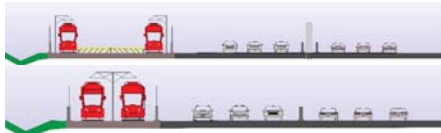
Conformément au processus d'évaluations environnementales municipales de portée générale, l'élargissement d'une route ou la construction d'une nouvelle route ou installation asphaltée de plus de 2,3 M\$ constitue un projet de l'annexe C. Les projets de l'annexe C d'emendement la préparation d'un rapport d'évaluation environnementale, qui sera soumis à un examen public.

Conformément au processus d'évaluation des projets de transport en commun, qui est le processus d'ÉE suivi pour le TLRE, nous en sommes actuellement à l'étape de pré-planification et de consultation. Cette étape comprend l'élaboration et l'évaluation d'options. La présente séance portes ouvertes présente la sélection de l'option de tracé privilégiée pour le TLRE au moment de l'étape de pré-planification et de consultation.

Études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17

TLR de l'est et route 174 dans le secteur à l'ouest du chemin Trim

Le TLR aura trois voies dans chaque direction, une protection pour les voies réservées aux véhicules à occupation multiple (VOM), des accotements asphaltés, un muret californien en béton ainsi qu'un muret en béton continu et une clôture à mailles losangées entre la route 174 et le TLR. Le TLR inclut deux pistes, un espace pour les poteaux de caténaires nécessaires, l'espace pour les allées, et les plates-formes dans les stations.



Couloir Nord (Blair au chemin Montréal)



Voie Médiane (chemin Montréal au Trim)

Questions	Réponse
Le TLR devrait être immédiatement étendu à la station Trim et la construction de certaines stations devrait être reportée	Limité pour cause d'abordabilité
Besoin d'augmenter les parcs-o-bus	Conception fondée sur les besoins
Craintes liées au bruit et à la vibration	Mesures d'atténuation considérées lorsque nécessaires
Craintes liées à l'accès des piétons et des cyclistes aux stations	Les stations sont conçues pour être entièrement accessibles
Craintes pour la sécurité des personnes à l'intérieur des stations	Les stations sont conçues dans le souci de la sécurité des personnes
Craintes que le calendrier des travaux d'élargissement de la route 174 s'étire au-delà de 2031	Limité pour cause d'abordabilité

Sept stations sont proposées à partir du chemin Blair au chemin Trim.

- Chemin Montreal
- Boulevard Jeanne D'Arc
- Boulevard Orléans
- Place D'Orléans
- Tenth Line Est
- Chemin Tenth Line
- Chemin Trim



Exemple - Station Cyrville

Études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17

Routes 174 et 17 dans le secteur à l'est du chemin Trim

Les sections typiques incluent :

- Terre-plein central pour améliorer la sécurité (type à être déterminé dans le plan détaillé)
- Ouvertures du terre-plein central uniquement à des endroits contrôlés
- Accotements asphaltés améliorés
- Sentier polyvalent à distance de la route, lorsque possible

Questions	Réponse
Craintes des conséquences sur les propriétés	Réduction des conséquences, lorsque possible.
Craintes liées à la congestion de la circulation et à la vitesse élevée	Mesures de gestion de vitesse prévues dans la conception.
Craintes liées à l'accès aux berges et aux passages routiers	Passages pour piétons commandés prévus dans la conception
Craintes des conséquences sur la rivière des Outaouais	Le tracé choisi évite le déversement de remblai dans la rivière.
Craintes liées au bruit	Lorsque nécessaire, l'installation d'écrans antibruit est considérée.
Craintes que le calendrier de l'élargissement des routes 174 et 17 s'étire au-delà de 2031	Limité pour cause d'abordabilité

Rurale



Urbaine



Consolider accès charretières



Analyse des feux de circulation et du carrefour giratoire à Cameron et Barnett

Études d'évaluation environnementale portant sur le prolongement vers l'est de la Ligne de la Confédération du train léger sur rail et l'élargissement de la route 174/chemin de comté 17

	Avantages	Compromis
Carrefours giratoires	Gestion de l'accès/ manœuvrabilité	Nécessite plus de terrain
	Certaine gestion de la vitesse	La circulation débalancée donne lieu à des délais lors des heures de pointe
		Moins pratique pour les piétons et cyclistes
		Requiert des signaux de passage piétonnier activés par les piétons afin de traverser la 174
Feux de circulation		Démesuré dans contexte villageois
	Performance opérationnelle améliorée	Requiert des mesures d'atténuation de la circulation afin de gérer la vitesse
	Moins de terrain est requis	
	Pratique pour les piétons et cyclistes	



Prochaines étapes

En fonction des commentaires reçus :

- Confirmer/améliorer la conception fonctionnelle de la route 174 et du TLR de l'est dans le secteur à l'ouest du chemin Trim
- Confirmer/améliorer la conception fonctionnelle des routes 174 et 17 dans le secteur à l'est du chemin Trim
- Mettre la dernière main aux conceptions fonctionnelles
- Mettre la dernière main aux mesures d'atténuation
- Secteur à l'ouest du chemin Trim – présentation en juin/juillet au Comité des transports d'Ottawa
- Secteur à l'est du chemin Trim – présentation à l'automne aux comités et conseils d'administration
- Examen public des documents afférents à l'étude

Questions ou Commentaires

Si vous souhaitez nous faire part de vos commentaires supplémentaires, transmettez-les par courrier, d'ici au 7 mai 2015, à l'adresse suivante

Angela Taylor, ing.
Ingénieure principale de projet
Ville d'Ottawa
110, avenue Laurier Ouest
Ottawa (Ontario) K1P 1J1
☎ : 613-580-2424, poste 15210
Courriel : Angela.Taylor@ottawa.ca

Marc R. Clermont, ing..
Directeur des Travaux publics
Comtés unis de Prescott et Russell
59 rue Court, Case Postale 304
L'Orignal, ON K0B 1K0
☎ : 613-675-4661, poste 3100
Courriel : MClermont@prescott-russell.on.ca

Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening Environmental Assessment Studies

Overview

This Information Bulletin has been prepared to provide an understanding of the Confederation Line East Light Rail Transit Extension and the Highway 174 / Prescott-Russell County Road 17 widening environmental assessment studies. It includes information on: Functional designs of the preferred integrated LRT and OR174 widening alternatives west of Trim Road; Functional design of the preferred widening of OR 174 and CR 17 from Trim Road to Landry Road and Potential Impacts and mitigation.

Confederation Line East Light Rail Transit Extension Project Overview

The City of Ottawa is undertaking an Environmental Assessment (EA) Study for the extension of Light Rail Transit (LRT) from Blair Station to Trim Road along the Highway 174 corridor in accordance with the Transit Projects Assessment Process. The East LRT (ELRT) extension is one of three EA studies underway as part of Stage 2, the City's plan to extend the benefits of rail to residents further east, west and south that will add 19 new stations and 35 kilometres to the LRT network. The ELRT line will provide fast, reliable service to the communities of Beacon Hill, Blackburn Hamlet, Orleans, and rural Cumberland. A total of seven stations are proposed between Blair Station and the Trim Park and Ride. These station locations have been designed to connect to existing and future transit services and park and rides in the area. The study area for this project extends from Blair Road in the west and continues along the Highway 174 corridor to Trim Road in the east.

Highway 174 and County Road 17 Widening Project Overview

The United Counties of Prescott and Russell in partnership with the City of Ottawa are undertaking a Class EA study for Highway 174 and Prescott-Russell County Road 17 (CR17) corridor from Highway 417 to County Road 8 (Landry Road). This study is being carried out in accordance with the requirements for a Schedule 'C' project under the Municipal Class EA process. The study area for the Highway 174/CR17 project extends from the split with Highway 417 in the west and continues along the Highway 174 corridor until Trim Road, where the study

area expands to include several alternative routes. The study area for the ELRT overlaps with the portion of Highway 174 that is located to the west of Trim Road.

A Coordinated Approach to Environmental Assessment

In order to reach an optimal solution for these two projects whose study areas overlap, a coordinated approach to these EAs is being undertaken. Co-ordinated approach to both EA studies is needed to:

- Maximize the use of the existing road corridor;
- Minimize impacts on the environment;
- Minimize construction cost;
- Build on existing study information;
- Integrate stakeholder consultation activities;
- Streamline EA process; and
- Share information, technical design work, and stakeholder consultation.

Study Area for the ELRT and Highway 174/CR17



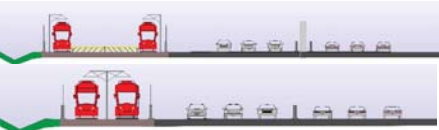
EA Study Processes

In accordance with the Municipal Class EA process, the widening of a road or construction of a new road/paved facility over \$2.3 M is a Schedule "C" project. Schedule "C" projects require the preparation of an Environmental Study Report which will be made available for public review. In accordance with the Transit Projects Assessment Process (TPAP), which is the EA process being followed for the ELRT, we are presently in the pre-planning and consultation phase. This phase involves generating and evaluating alternatives. This open house presents the selection of preferred design for the ELRT within the pre-planning and consultation phase.

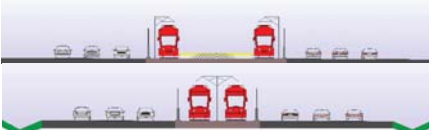
Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening
Environmental Assessment Studies

ELRT and Highway 174 West of Trim Road

The 174 will be 3 lanes in each direction, and contain a paved shoulder, median barrier, and a continuous concrete barrier and fence between the 174 and the ELRT. The 174 will also include a High Occupancy Vehicle (HOV) lane. The LRT will include 2 tracks, space for the required overhead catenary poles, space for walkways, and platforms at stations.



North Alignment (From Blair to Green's Creek)



Median Alignment (From Green's Creek to Trim Road)

Comments Received	Actions Taken
LRT should extend to Trim and defer building some stations	Limited by affordability
Expand park & rides	Design will be based on need
Concern about impact of noise and vibration	Mitigation identified and implemented where warranted
Concern about pedestrian and cyclist access to stations	Stations designed for full accessibility
Concern about personal safety at stations	Design with personal safety considerations
Concern for timeframe of 174/17 widening beyond 2031	Limited by affordability

Seven stations are proposed from Blair to Trim Road.

- Montreal Road
- Jeanne D'Arc Boulevard
- Orléans Boulevard
- Place D'Orléans
- Tenth Line East
- Tenth Line Road
- Trim Road



Cyrville Station Example



Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening
Environmental Assessment Studies

Highway 174 East of Trim

Typical Cross Section will include:

- Median barrier to improve safety (type to be determined in detail design)
- Median breaks
- Enhanced paved shoulders
- Multi-use pathway away from the roadway where possible

Questions	Actions Taken
Concern for property impacts	Impacts reduced wherever possible
Concern for traffic congestion and high speeds	Speed management included in design
Concern for access to waterfront and road crossing	Mitigation identified and implemented where warranted
Concern for impact on the Ottawa River	Mitigation identified and implemented where warranted
Concern about impact of noise	Where applicable, noise barriers will be included in design
Concern for timeframe of 174/17 widening beyond 2031	Limited by affordability

Rural



Urban



Consolidated Driveway access



Confederation Line East Light Rail Transit Extension and Highway 174 / County Road 17 Widening Environmental Assessment Studies

Analysis of signals and roundabouts at Cameron and Barnett

	Advantages	Trade-offs
Roundabouts	Access management/ Manoeuvrability	More property required
	Some speed management	Unbalanced vehicle demand results in delays during peak
		Less convenient for pedestrians and cyclists
		Requires pedestrian-actuated signals to cross 174
Signals		Out of scale for village
	Better operational performance	Requires urban design elements to manage speed
	Less property required	
	Convenient for pedestrians and cyclists	



Next Steps

Based on feedback received:

- Confirm/refine Hwy 174-ELRT functional design west of Trim
- Confirm/refine Hwy 174-17 functional design refinements east of Trim
- Finalize Functional Designs
- Finalize mitigation measures
- West of Trim – June/July presentation to Ottawa Transportation Committee
- East of Trim – Fall presentation to Committees and Councils
- Public review of study documentation

Questions or Comments

If you wish to provide us with more detailed comments, send written submissions by 7 May, 2015 to:

Angela Taylor, P.Eng.
Senior Project Engineer
City of Ottawa
110 Laurier Ave W, Ottawa, ON K2P 2L7
☎: 613-580-2424 ext. 15210
E-mail: angela.taylor@ottawa.ca

Marc R. Clermont, P.Eng.
Director of Public Works
United Counties of Prescott-Russell
59 Court St, PO Box 304
L'Orignal, ON K0B 1K0
☎: 613-675-4661 ext. 3100
E-mail: mclermont@prescott-russell.on.ca

AECOM

United Counties of Prescott and Russell and
City of Ottawa

Summary Report for April 2015 Consultation
Confederation Line East LRT Extension/ Highway 174/
Prescott Russell County Road 17 Widening

Appendix C

Summary Report for April 2015 Consultation

- Comments Received Following February 2015 Consultation
 - Comment Sheets Received at the Open Houses
 - Correspondence Received during the comment period

Open Letter to the Mayor of Ottawa

February 19, 2015

Office of Mayor Jim Watson
110 Laurier Avenue West
Ottawa, Ontario
K1P 1J1City of Ottawa
Ville d'OttawaFEB 19 2015
TO COTRA
Mayor's Office
Bureau du Maire

Dear Mayor Watson,

The Orléans Chamber of Commerce, GOEDC and the Heart of Orleans BIA would like to thank you, all City Councillors and City Staff, in reviewing the Master Transportation Plan and the Official Plan. We agree that the two main issues facing our Bilingual and Multicultural community are transit and economic development.

The Second Phase of the Light Rail Transit identified the extension of the LRT from Blair Road to Place d'Orléans with the potential to extend to Trim Road, as a priority and this is something that our Community has been patiently waiting for, and is needed very soon.

Orléans has demonstrated that, as a community, it supports public transit. It has one of the highest rates of public transit use in North America – in terms of accessing the downtown. Yet there is significant potential to expand ridership further, particularly with respect to the employment in Tunney's Pasture and the 160,000 jobs east of the downtown. The eastern LRT will increase public transit ridership and ease potential LRT/bus transfer problems at Blair Road. It will also significantly reduce transit operating costs, thus easing pressure on both transit fares and taxes.

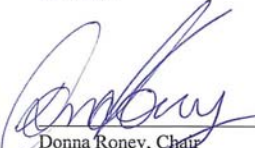
The LRT in our Community will link pedestrian, bicycles, vehicle and buses along Highway 174. If the LRT is extended east of Place d'Orléans, it will enhance the role of Centrum as a central hub linking with employment lands east of Place d'Orléans and with Petrie Island Conservation Lands, Petrie Island Public Beach and the Marina.

We are thankful for Council's approval of the Community Improvement Plan initiatives which provide new businesses with economic incentives to consider employment expansion into Orléans. The eastern LRT will further support that effort, both by improving the linkages with the downtown and by ensuring that Orléans can maintain, and grow our labour force, that is proving to be a significant draw for new businesses.


• COMMENTS RECEIVED FOLLOWING FEBURARY 2015 CONSULTATION

We are pleased that the Environmental Assessment will look at the potential to extend LRT east of Place d'Orléans. Locating a stop near Tenth Line or Trim Road would unlock employment / development lands that are serviced ready and could assist in providing new revenues in development charges and realty taxes to the City of Ottawa, and most importantly bringing employment and assisting in solving the employment imbalances ratios in our community.

Sincerely,


Donna Roney, Chair
Orléans Chamber of Commerce




Jasmine Brown, Executive Director
The Heart of Orléans BIA



cc:
Royal Galipeau, MP
Marie-France Lalonde, MPP
Bob Monette, Councillor
Stephen Blais, Councillor
Jody Mitic, Councillor
Tim Tierney, Councillor

From: [REDACTED]
Sent: February-24-15 2:27 PM
To: Monette, Bob
Subject: Toilets on Transitway

Dear Mr. Monette:

Ottawa needs a network of safe, free, clean and environmentally responsible public toilets and water fountains that are accessible to persons with disabilities in major transit stops, parks and key public place.

Available toilets that are accessible to persons with disabilities are a public health issue and a key to sustainable cities. The real costs of not providing these facilities is born by the people who limit their activity in the City because of their fear of an "accident" and by those businesses and residents that are expected to cope with the results of unmet needs.

Design choices can be made that allow for easy cleaning and management, resistance to vandalism, and low maintenance requirements. We can create useful jobs building and maintaining public toilet facilities.

I support the GottaGo! proposals:

In Ottawa, the National Capital Commission and the City should:

- * Invest in signage, appropriate staffing, improvements and maintenance for existing public toilets (recreation centres, libraries, parks)
- * Build in at least one 24/7 unisex direct access facility to public buildings like firehalls, ambulance garages, recreation centres, libraries, etc.
- * Provide subsidies to private operations like coffee shops in return for improvements, open access, oversight and clear signage
- * Require new developments to provide for unisex direct access toilets accessible to persons with disabilities as part of their permitting; invest in some adult "changing stations" for users who require this level of support at key places in the City..
- * Build new toilets where required.
- * Many people are unable to access toilet facilities at existing transitway stations. It is a necessity as we move forward with transit expansion. I believe it would increase bus riders who would know such facilities are available.

Yours truly,

[REDACTED]
[REDACTED]

On Feb 26, 2015, at 3:00 PM, Monette, Bob
<Bob.Monette@ottawa.ca<<mailto:Bob.Monette@ottawa.ca>>> wrote:

Good afternoon [REDACTED],

Thank you for your email and for taking the time to write to me. I met with representatives from the Gotta Go campaign on February 5th and listened attentively to their comments and concerns in terms of needing washrooms at all light rail transit station. I understand the concerns of residents who have disabilities or conditions that require them to use the washroom on a frequent basis or families with young children. The Confederation Line will include a washroom at the start and end point of the line which will mean a public washroom located at the Blair Station as well as one at Tunney's Pasture. This does represent an improvement to the current transit system which to my knowledge, do not offer any washrooms within the station itself.

As part of phase two of the Confederation Line, I would encourage you to submit comments in regards to the need for washrooms as part of the currently ongoing environmental assessment. If you would like, I would be happy to forward your email below to Angela Taylor who is the project manager for the environmental assessment to ensure that your comments are included in the public record and also to ensure that the need for washrooms is studied as part of the cost analysis that will be done for this next phase of light rail in Ottawa.

I look forward to obtaining more information from City staff on the cost analysis of public washrooms as well as how we can ensure the safety of residents should public washrooms be built.

Thank you again for taking the time to write to me and sharing your concerns with me.

Bob

Bob Monette
Deputy Mayor - Maire suppléant
City Councillor Orléans Ward - Conseiller municipal du quartier Orléans

Orléans Ward - Proud to be home to 50,000 residents Quartier Orléans - Fier de compter plus de 50,000 résidents

T: 613-580-2471
F: 613-580-9624
www.BobMonette.ca<<http://www.bobmonette.ca/>>

From: [REDACTED]
Sent: Wednesday, February 25, 2015 9:15 AM
To: [REDACTED]
Subject: Proposed Route for LRT on Hwy 174

Dear [REDACTED]

I live at [REDACTED]. I purchased my home in 1985 when the road behind my house was highway 17 with a very modest amount of traffic passing each day. Since then Orleans has grown to more than 100,000 residents and the road behind my house has been expanded twice, to both increase the volume that can be handled and to add east and west bound bus lanes on either side. This has had the effect of increasing both the amount of traffic using 174 and significantly increasing both the noise level and the dust that is generated by so many cars and buses passing by.

Although I agree that the logical choice for the LRT would be the median between the east and west bound lanes, given that it is approved, there would be another increase of noise and pollution that would be dangerous to the health of anyone living on Taffy Lane. Therefore, in order to mitigate this issue, a barrier fence should first be erected along the fence line that is facing highway 174. This barrier should be high enough and of such audio absorbing material to minimize both the sound and the dust that is generated by the increased traffic. If this is done I fully support the installation of the LRT in the centre of Highway 174.

Should you wish to contact me my telephone number is [REDACTED]

Yours truly,

[REDACTED]

PS: I suspect that this is not the forum to express my concerns but perhaps you can refer this to the appropriate section of the LRT project. I could not find the means to express my concerns online. Thank you.

From: [REDACTED]
Sent: February 13, 2015 4:37 PM
To: Taylor, Angela
Cc: [REDACTED]

Subject: Comment on 174/17 EA Study - Weighting of indicators / measurements

To: Angela Taylor, City of Ottawa

The Evaluation Report, Ottawa Road 1174 / Prescott-Russell County Road 17, Environmental Assessment Study identifies Alternative 1, widening of the OR 174-CR 17, as the preferred option in comparison to Alternatives 2,3A and 3B. The selection of this preferred option is based on the “average weights submitted by proponents of this study (United Counties of Prescott Russell and the City of Ottawa)” that are applied to the selection criteria.

With respect to selecting a preferred option, the report states on page 67 that “The identification of a technically preferred route is therefore, to some extent, dependent on the weights given to the various indicators/measurements”. Given the fact the scoring of “preferred option” Alternative 1 at 1.88 is only slightly greater than the second ranked Alternative 3A at a score of 1.67, the weighting of the indicators and measurements is critical to the outcome and deserves a re-examination.

The call for a re-examination of the weighting is further supported by the results of the Sensitivity Analysis presented in Table 4-2 on page 66 of the report. One of the sensitivity tests used only the weighting provided by “specialists representing a variety of disciplines were invite to provide weights for review”.

When Specialist Weights were used alone, the preferred option was Alternative 3A and not Alternative 1 (widening OR174-CR17) which ranked 3rd! This suggests that either the weights given to the various indicators/measurements by the proponents of the study (that being the City of Ottawa and the United Counties of Prescott Russell) have a bias in favor Alternative 1 – widening the OR 174-CR17 or the “specialists” got it wrong !

Either way, these two different results based on different weights illustrate the need to re-examination the difference in weights assigned by the proponents of the study and the “specialist” consultants participating in the environmental assessment. Until this is done to the satisfaction of all stakeholders, the recommended “preferred option” will remain questionable.

For your consideration.

[REDACTED]
Chair 174 Working Group
Cumberland Community Association

**Cumberland Community Association 174 Working Group
Questions Concerning the Widening of Ottawa Road 174 East of Trim Road as Presented at the OR 174/County Road 17 Widening Open House – February 4, 2015**

1. Feasibility and Cost Preferred Option: Have detailed engineering studies been undertaken to determine the feasibility and cost of the proposed parkway? If not, how can the study have concluded that route #1 is the most favoured option?

2. Traffic Throughput: To what extent would the proposed widening increase the throughput of traffic? What assumptions about future growth in traffic from Clarence/Rockland does this accommodate? If, when the proposed parkway is eventually built, it can't accommodate the traffic from Clarence/Rockland, would the road need to be widened again?

3. Parkway versus Freeway: Can the study group provide a guarantee that any eventual widening of 174 would take the form of a parkway, and not a freeway? If speeding is already an issue on the two-lane road, how will a proposed reduction in speed on a new four lane be enforced without official 'parkway' status?

4. Cost: What is the total detailed financial cost of the proposed widening? How does that compare with the detailed financial cost of building corridors through options 2 or 3?

5. Hybrid Model: Has a hybrid model been considered where 174 is upgraded as a two lane (similar to work done this summer Trim to Cameron) to Rockland and a new two-lane is built as the Brian Coburn, Innes Rd/Boundary Rd option effectively creating four lanes desired but spreading out the traffic.

6. Impact on Residents: How many residents along the existing highway and in adjacent areas (including the Village of Cumberland) would be directly and significantly impacted? How does this compare to the other two corridors?

7. Impact on the Ottawa River: What will the impact be on the Ottawa River? Increased salt and contaminant runoff, increased noise, risk of accidents/oil spills/road kill? Why were biological considerations of this rural area only given a weighting of 5% in the environmental assessment?

8. Road Crossing: How would pedestrians/cyclists cross the new four-lane road with a proposed concrete median wall? If there would be pedestrian tunnels, how far apart and at what cost?

9. Turning Points: How would vehicles coming from properties abutting the highway access the road? It seems that under the current proposal, due to a concrete median wall, vehicles on the north side would only be able to access westbound lanes, and residents on the south side would only be able to access eastbound lanes, and then would have to proceed to the nearest junction to turn around if they wanted to go the opposite way (like St. Joseph/Montreal Road between Jean D'Arc and the Cartier, formerly Rockcliffe, Parkway). How far apart would the turnarounds be?

10. Blocked Lanes: If the lanes in one direction were blocked due to an accident or other circumstance, would vehicles in those lanes be trapped? Would people living on that side of the road have no way to leave their houses or emergency vehicles access them?

Submitted by [REDACTED] Chair of the 174 Working Group, Cumberland Community Association
5 February, 2015

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]

Sent: Tuesday, March 03, 2015 10:23 AM

To: [REDACTED]

Cc: [REDACTED]

Subject: RE: Comment on 174/17 EA Study - Weighting of indicators / measurements

Hello [REDACTED], my apologies for the delay to this response and is due to the overwhelming comments and inquiries we received and continue to receive for both studies. Please find attached a copy of your questions accompanied by responses to your questions. With respect to your comments below, it should be noted that the purpose of the concordance evaluation methodology is to assist in the decision making process. Whatever the outcome, a reasoned argument (page 67 of the report) is then applied to the results as a final check and balance of the outcome to ensure it makes sense. While the outcome of the OR174/CR17 widening is not overwhelmingly strong under the blended City and Prescott-Russell weights, it is justifiably supported in the sensitivity analysis as a robust solution.

At this stage, the City is currently proceeding with the functional design of the OR174/CR17 widening and the results will be presented in late April, when the final round of consultations are anticipated. We hope to see you there.

Regards,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
☎ 613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Cumberland Community Association 174 Working Group

Questions Concerning the Widening of Ottawa Road 174 East of Trim Road as Presented at the OR 174/County Road 17 Widening Open House – February 4, 2015

1. Feasibility and Cost Preferred Option: Have detailed engineering studies been undertaken to determine the feasibility and cost of the proposed parkway? If not, how can the study have concluded that route #1 is the most favoured option?

City – This study reviewed four technically feasible options and carried out a preliminary review of each option, including costs, for comparative purposes. This is needed in order to understand how each option ranks among the other options when applying the 63 factors established in the evaluation criteria. From the concordance evaluation method as well as a reasoned argument perspective, the OR174/County Road 17 was the preferred option.

2. Traffic Throughput: To what extent would the proposed widening increase the throughput of traffic? What assumptions about future growth in traffic from Clarence/Rockland does this accommodate? If, when the proposed parkway is eventually built, it can't accommodate the traffic from Clarence/Rockland, would the road need to be widened again?

City – Widening of OR174/CR17 to one lane in each direction can theoretically accommodate up to 1,200 additional vehicles per hour. Throughput is dependent on intersection operations and commonly the busiest intersection controls the capacity of a roadway. Travel demand is based on growth projections to the 2031 planning horizon and we have considered growth for Cumberland and Clarence-Rockland to 2031.

3. Parkway versus Freeway: Can the study group provide a guarantee that any eventual widening of 174 would take the form of a parkway, and not a freeway? If speeding is already an issue on the two-lane road, how will a proposed reduction in speed on a new four lane be enforced without official 'parkway' status?

City – the OR174 is classified as an arterial road and not a freeway. Only the section west of Trim Road is considered a freeway with controlled access. The OR174 east of Trim Road will be designed to urban and rural arterial standards. Within the Cumberland Village and Rockland urban areas, the road corridor design will include design features for speed management.

4. Cost: What is the total detailed financial cost of the proposed widening? How does that compare with the detailed financial cost of building corridors through options 2 or 3?

City – As mentioned earlier, comparative costs were prepared during the evaluation phase of the study process. Although the OR174/CR17 widening has the highest cost of the four options, cost was only 1 out of 63 factors considered in the evaluation criteria and represented 12% of the total criteria. A more detailed cost will be developed during the functional design phase.

5. Hybrid Model: Has a hybrid model been considered where 174 is upgraded as a two lane (similar to work done this summer Trim to Cameron) to Rockland and a new two-lane is built as the Brian Coburn, Innes Rd/Boundary Rd option effectively creating four lanes desired but spreading out the traffic.

City – This option is the Innes/Baseline Road extension alternative that was not the preferred alternative from the assessment.

6. Impact on Residents: How many residents along the existing highway and in adjacent areas (including the Village of Cumberland) would be directly and significantly impacted? How does this compare to the other two corridors?

City – The number of properties impacted for each option is identified in the Evaluation Report that was forwarded to you earlier on page 8. Note that this is the total number of properties along the OR174/CR17 corridor between Trim Road and Clarence-Rockland. More detailed information will be available when design refinements and the functions designs are completed.

7. Impact on the Ottawa River: What will the impact be on the Ottawa River? Increased salt and contaminant runoff, increased noise, risk of accidents/oil spills/road kill? Why were biological considerations of this rural area only given a weighting of 5% in the environmental assessment?

City – Once the functional design has been completed, the road widening footprint will be confirmed. Mitigation measures for any potential impacts on the environment will be recommended.

As for biological considerations, it should be noted that while an EA study's objective is to avoid or minimize potential environmental effects, the City recognizes that it may not be possible to do so for all impacts, as is the case for this study and most studies in general.

To understand the environmental impacts for this study, 63 factors were identified in the Evaluation Report. While this report provides the detailed evaluation process for each of the 63 factors, it also illustrates the trade-offs of competing priorities when each factor is assessed for each alternative.

8. Road Crossing: How would pedestrians/cyclists cross the new four-lane road with a proposed concrete median wall? If there would be pedestrian tunnels, how far apart and at what cost?

City – The road widening design will include pedestrian crossings at the intersections. Pedestrian actuated signal crossings will be reviewed for midblock locations.

9. Turning Points: How would vehicles coming from properties abutting the highway access the road? It seems that under the current proposal, due to a concrete median wall, vehicles on the north side would only be able to access westbound lanes, and residents on the south side would only be able to access eastbound lanes, and then would have to proceed to the nearest junction to turn around if they wanted to go the opposite way (like St. Joseph/Montreal Road between Jean D'Arc and the Cartier, formerly Rockcliffe, Parkway). How far apart would the turnarounds be?

City – With a median barrier design, u-turns will be required to access property. This is similar to many areas in the City such as Innes Road, Tenth Line, Woodroffe Avenue to name a few.

10. Blocked Lanes: If the lanes in one direction were blocked due to an accident or other circumstance, would vehicles in those lanes be trapped? Would people living on that side of the road have no way to leave their houses or emergency vehicles access them?

City – The road widening design will include opportunities for u-turns in accordance with design standards.

Submitted by [REDACTED] Chair of the 174 Working Group, Cumberland Community Association
5 February, 2015

From: [REDACTED]
Sent: Monday, February 16, 2015 1:15 PM
To: [REDACTED]
Subject: Feb 4 plan to widen 174 meeting or build ring road

Good afternoon, it appears that I missed this meeting on Feb 04. I live on [REDACTED] and I am curious what the possible plan are for a ring road around that area. I tried to look it up on line but was unable to find anything on it. One option apparently was to have a ring road parallel to Wilhaven drive from Trim to Baseline road (Cumberland)? And another option was to extend Innes to Baseline road. If you would have a map or a web site that would show/tell me where this road could possible reside that would be most great full.

Thank you,

[REDACTED]

From: [REDACTED]
Sent: Wednesday, February 18, 2015 11:42 AM
To: [REDACTED]
Subject: RE: Feb 4 plan to widen 174 meeting or build ring road

Hello, please find map attached. Also, please see links to the study's websites ottawa.ca/easternlrt or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study.

You can also direct questions and comments to:

East LRT and HWY 174/CR17 Widening
Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Take care,

[REDACTED]

From: [REDACTED]
Sent: Wednesday, February 18, 2015 1:10 PM
To: [REDACTED]
Subject: RE: Feb 4 plan to widen 174 meeting or build ring road

Thank you for the information, would it be possible to get information on when the next meeting on this is? Is there an email alert?

Thanks,

[REDACTED]

From: [REDACTED]
Sent: February 18, 2015 1:21 PM
To: [REDACTED]
Cc: Taylor, Angela
Subject: RE: Feb 4 plan to widen 174 meeting or build ring road

Our pleasure for the information and thank you for your interest. I am copying Angela Taylor whose contact information is below. Angela will add this email address, [REDACTED] to the distribution list. We are planning additional meetings with the public this coming spring, hopefully in April, however details are yet to be finalized. Once they are, you will be notified by email.

As always, please do not hesitate to email Angela or myself for additional information at anytime.

Have a great day,

[REDACTED]

From: [REDACTED]
Sent: March-07-15 10:14 PM
To: Monette, Bob
Cc: [REDACTED]
Subject: Planned transportation routes

Hi Bob,

My husband and I are looking for clarification of a few points. We were unable to attend the public consultation meetings in February.

We live on [REDACTED] and currently use the express bus route 34 to get downtown. We have heard from neighbours that when the Confederation Line is finished, the express routes will no longer exist. Instead, we would catch a local bus to Place d'Orleans, another bus from Place to Blair, and then use the LRT to get downtown. Is this correct, and will there be any other options (such as catching a bus from the highway at Jeanne d'Arc)?

We have some major concerns with this proposal--mainly the amount of time it will ADD to our commute. In fact, it has us so concerned that we are considering a move outside of Orleans, which would be upsetting, as we love our Convent Glen neighbourhood.

We are hoping that you can clarify this matter for us so that we can make plans for the future.

Thank you for your time,

[REDACTED]

From: Monette, Bob
Sent: March 07, 2015 10:45 PM
To: [REDACTED]
Cc: [REDACTED] Taylor, Angela; [REDACTED]
Subject: RE: Planned transportation routes

Good evening [REDACTED],

Thank you for your email and for taking the time to write to me. As far as I am aware, there have been no final decisions made as far as details of local routes are concerned. We are currently in the preliminary planning phases and the environmental assessment currently underway is studying which corridor will be the most efficient while taking into account a variety of factors. The public meeting that was held was to present the option which at this time has been identified as the preferred option being having the train run on the Northern side of the 174 from Blair Road to Montreal Road and then crossing into the median up to Place d'Orléans.

I am however copying Angela Taylor to this email as she is in charge of the environmental assessment requesting that she include your comments and concerns in the public record of the environmental assessment.

Light rail will change our community and in fact, Orléans will be the first suburban community to benefit from it. The morning and afternoon frustrations currently being felt by transit riders in terms of reliability and efficiency will be a thing of the past as the trains will no longer face delays due to traffic congestion, the weather or accidents on our roads. It will ensure residents of Orléans with a reliable service to and

from our community and provide a direct connection all the way to the Bayshore Shopping Centre. This being said, I am copying Evelyn Danilko who is a program manager in the light rail implementation team so that she is aware of your concerns in regards to transfer times once phase 2 is functional.

Thank you,
Bob

Bob Monette
Deputy Mayor - Maire suppléant
City Councillor Orléans Ward - Conseiller municipal du quartier Orléans

Orléans Ward - Proud to be home to 50,000 residents
Quartier Orléans - Fier de compter plus de 50,000 résidents

T: 613-580-2471
F: 613-580-9624
www.BobMonette.ca

From: Taylor, Angela
Sent: March 09, 2015 3:50 PM
To: [REDACTED]
Cc: [REDACTED]; Danilko, Evelyn; Monette, Bob
Subject: RE: Planned transportation routes

Hello [REDACTED], your comments are important for the study process and will be included as part of the public record. I would like to respond to your concern about the express bus routes in Orleans once the Confederation Line is operational. OCTranspo has confirmed that Route 34 will be routed directly to Blair Station, and not Place D'Orleans, where one can then transfer to LRT. While OCTranspo continues to review the bus routes in anticipation of the Confederation Line, in general, the express busses will likely be routed to Blair Station to transfer to LRT.

Regards,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
☎ 613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

From: [REDACTED]
Sent: Sunday, March 15, 2015 12:20 PM
To: [REDACTED]
Subject: Hwy 174 widening study

Dear sir,

I am a resident of Cumberland whose property at [REDACTED] overlooks Hwy 174. The present 2-lane highway already represents a source of considerable noise, particularly in summer when motorcycle traffic is added to year-round car and truck traffic and more time is spent outdoors or with windows open. The proposal to add extra lanes of traffic is of great concern as this would likely encourage additional traffic and increase vehicle speed which would exacerbate the noise issue.

I would urge planners to consider other options such as Innes Road which is more rural in the section south of Cumberland, or to limit widening to certain sections of 174 such as was done recently at the Quigley Hill intersection. The current highway seems able to handle the volume of traffic as traffic continues to flow even during morning and evening rush hours. Widening the highway would only encourage higher speeds.

Sincerely yours,

[REDACTED]

From: [REDACTED]
Sent: Monday, March 30, 2015 2:02 PM
To: [REDACTED]
Subject: RE: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell
County Road 17 Widening Environmental Assessment Studies

[REDACTED],

Any chance that the summary of comments can be provided prior to the Open Houses to allow us to prepare any further comments/ concerns?

Thanks

[REDACTED]

Ghioureliotis, Catherine

From: Euan Swan <eswan@cda-adc.ca>
Sent: Tuesday, March 24, 2015 4:16 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Minutes - OR 174 / Prescott-Russell CR 17 and Confederation Line Eastern LRT Extension Public Consultation Group (PCG) meeting

Follow Up Flag: Follow up
Flag Status: Flagged

Thank for your email and minutes of the February meeting.

For the approaching April round of meetings, I wish to suggest the following:

1. A public address system to ensure the presenters can be heard;
2. Microphones set up for audience members to address their questions to the presenters. Audience members would line up at a microphone and questions would be taken one at a time; and
3. A chairperson or facilitator to control the meeting.

I believe the above steps would contribute to a more orderly and productive meetings.

For your consideration.

Euan Swan
Cumberland Community Association

From: Kielstra, David [<mailto:David.Kielstra@aecom.com>]
Sent: March-23-15 11:29 AM
Cc: Taylor, Angela; 'Clermont, Marc'; Kelly Roberts; McGirr, Valerie
Subject: Minutes - OR 174 / Prescott-Russell CR 17 and Confederation Line Eastern LRT Extension Public Consultation Group (PCG) meeting

Good morning,

Thank you for your ongoing interest in the OR 174 / Prescott-Russell CR 17 and Confederation Line Eastern LRT project. Please see the email attachment for minutes from the February 2, 2015 Public Consultation Group meeting.

If you have any comments or questions regarding the project, please contact the following members of the project team:

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
Phone: 613-675-4661 ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
Phone: 613-580-2424 ext. 15210
E-mail: Angela.Taylor@ottawa.ca

Thank you,

Ghioureliotis, Catherine

From: [REDACTED]
Sent: Monday, March 30, 2015 1:15 PM
To: Taylor, Angela
Cc: [REDACTED]
Subject: RE: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies

Hi Angela... could we please get a pdf of the Open House boards for information purposes?

Thanks.

[REDACTED]

From: Kielstra, David [mailto:David.Kielstra@aecom.com]
Sent: March-30-15 9:43 AM
Cc: Taylor, Angela; 'Clermont, Marc'; Kelly Roberts; McGirr, Valerie; philip.pawliuk@sympatico.ca; David McAvoy
Subject: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies

Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies **Open Houses: 6 to 9 p.m., presentation at 7 p.m.**

You are invited to the final Open Houses to review and provide feedback on the recommendations for these two environmental assessment (EA) studies. The Confederation Line East LRT Extension study is following the Transit Projects Assessment Process and the OR174 and Prescott-Russell County Road 17 (CR17) corridor is under the Municipal Class EA process for a Schedule 'C' project.

Monday, April 20, 2015 Community Pentecostal Church 1825 St. Joseph Boulevard Orléans	Tuesday, April 21, 2015 R. J. Kennedy Community Centre Hall A & B, 1115 Dunning Road Cumberland	Thursday, April 23, 2015 Clarence Creek Community Hall 418 Lemay Street Clarence Creek
---	---	--

Open House presentation and displays will include:

- Functional design of the preferred integrated LRT and OR174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

The East LRT extension is one of three EA studies underway as part of Stage 2, the City's plan to extend the benefits of rail to residents farther east, west and south that will add 19 new stations and 30 kilometres to the O-Train system— see ottawa.ca/stage2 for further information.

Accessibility is an important consideration to facilitate attendance at these Open Houses. If you require special accommodation, please call 3-1-1 or e-mail one of the project leads below before the event.

Ghioureliotis, Catherine

From: Michel <mllaflam@rogers.com>
Sent: Monday, March 30, 2015 11:08 AM
To: [REDACTED]
Subject: Re: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies

Follow Up Flag: Follow up
Flag Status: Flagged

Will this be the same presentation on LRT than the one we went a month ago to in the over-crowded small room at the Orleans Sportplex on Youville Drive?

That presentation was followed by the boring waste-of-time presentation on the future of the 174 that concentrated on widening the part from Trim road to Prescott-Russell, that 80% of us not interested in it and not wanting this because the part from Trim Road to the 417 split is currently a parking lot morning and night due to all the blind rubber stamping for new home and condo development in Orleans that we don't want since 10 years until the 174 and 417 infrastructure (and alternate 417 from Trim to the 416) is dealt with and completed.

Our quality of life in Orleans Convent Glen has been going down and down since the forced amalgamation of Orleans by Ottawa, with most of our tax \$ going to repairing Ottawa crumbling infrastructure that should have been redone prior to that amalgamation. The 174 is so often blocked for a long time and it has hundreds of pot holes.

Ah shoot! It's the 30th today! The City just rob our bank account again of another \$1,000 for which we'll only get less than \$100 of value in return.

RSVP

Michel

On Monday, March 30, 2015 9:43 AM, "Kielstra, David" <David.Kielstra@aecom.com> wrote:

Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies **Open Houses: 6 to 9 p.m., presentation at 7 p.m.**

You are invited to the final Open Houses to review and provide feedback on the recommendations for these two environmental assessment (EA) studies. The Confederation Line East LRT Extension study is following the Transit Projects Assessment Process and the OR174 and Prescott-Russell County Road 17 (CR17) corridor is under the Municipal Class EA process for a Schedule 'C' project.

Monday, April 20, 2015 Community Pentecostal Church	Tuesday, April 21, 2015 R. J. Kennedy Community Centre	Thursday, April 23, 2015 Clarence Creek Community Hall
---	--	--

From: [REDACTED]
Sent: April 09, 2015 7:37 PM
To: [REDACTED]
Cc: [REDACTED] Taylor, Angela
Subject: Public Consultation Group Meeting On the highway 174 widening

Dear [REDACTED];
I received an invitation to the meeting on 14 April. Unfortunately, I am out of town on the 14th of April and cannot attend.

I would like to reiterate the following;

- a; I believe the weighting process which selected the widening of the 174 is flawed. Options one and three are essentially the same given the subjective nature of the analysis.
- b; The widening of the 174 will gut the village of Cumberland and create much more social and physical disruption than the option 3 alternative.
- c; I believe your weighting of only 5% for environmental impact is ludicrously low considering the proximity of this road to the Ottawa river.
- d; I do not believe your study adequately addressed the southerly growth patterns in Prescott-Russell. Essentially instead of connecting new developments directly you move the traffic north before moving it east -west.

In summary, I do not accept the conclusions of your study.

[REDACTED]
From: Taylor, Angela <Angela.Taylor@ottawa.ca>
Sent: April 10, 2015 10:07 AM

To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Public Consultation Group Meeting On the highway 174 widening

Good morning [REDACTED], we have rescheduled the Public Consultation Group meeting to Thursday April 16 at the same time and location as noted below. Invitations are being sent out today. We hope you are available for the revised date. As always, thank you for your comments and will be included as part of the public record.

Revised Meeting Date for the Public Consultation Group

Thursday April 16, 2015
6:30 p.m. to 8:30 p.m.
Room #340, 255 Centrum Boulevard, Ottawa

Best regards,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

From: [REDACTED]
Sent: March 27, 2015 2:25 PM
To: Taylor, Angela
Subject: East LRT

Hello Angela,

I have reviewed the Confederation Line East LRT Extension and the Highway 174 through to Prescott-Russell County Road 17 Widening EA Studies information on the City website, and I am wondering if there has been any further development on the transit station locations and designs? I am specially looking for more information on the Jeanne D'Arc station. I am also wondering if there have been any discussions/plans related to the expansion of a new Park & Ride lot at this location?

Could you please add me to your contact list to receive future notifications reading this project.

Thank you in advance for your assistance.

Regards,



Jill Stewart, LEED GA
Planner
LLOYD PHILLIPS & ASSOCIATES LTD.

T 613 236 5373 x 3 F 613 236 5776 jill@lloydphillips.com

1827 Woodward Dr., Suite 109, Ottawa, Ontario Canada K2C 0P9 • <http://www.lloydphillips.com>

Please consider the environment before printing this e-mail. / Pensez à l'environnement avant d'imprimer ce courriel

From: Taylor, Angela
Sent: March 31, 2015 11:37 AM
To: [REDACTED]
Subject: RE: East LRT

Hi [REDACTED] we will be presenting draft functional designs for both the LRT and Hwy 174 widening at the next round of consultations coming up in the next few weeks. I attach the ad for reference and is also posted on the project websites. We will also add you onto our general distribution list for future notices.

Best regards,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports

Ghioureliotis, Catherine

From: Peter Levick <plevick@sympatico.ca>
Sent: Wednesday, April 01, 2015 7:07 PM
To: [REDACTED]
Subject: RE: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies

Follow Up Flag: Follow up
Flag Status: Completed

Hello [REDACTED]

Thank you for the invite and update. I will be out of town that week so will miss the presentation. Can you keep me up today as things progress. These two projects are important for our community.

Regards
Peter Levick

From: Kielstra, David [<mailto:David.Kielstra@aeom.com>]
Sent: Monday, March 30, 2015 9:43 AM
Cc: Taylor, Angela; 'Clermont, Marc'; Kelly Roberts; McGirr, Valerie; philip.pawliuk@sympatico.ca; David McAvoy
Subject: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies

Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies
Open Houses: 6 to 9 p.m., presentation at 7 p.m.

You are invited to the final Open Houses to review and provide feedback on the recommendations for these two environmental assessment (EA) studies. The Confederation Line East LRT Extension study is following the Transit Projects Assessment Process and the OR174 and Prescott-Russell County Road 17 (CR17) corridor is under the Municipal Class EA process for a Schedule 'C' project.

Monday, April 20, 2015
Community Pentecostal
Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community
Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community
Hall
418 Lemay Street
Clarence Creek

Open House presentation and displays will include:

- Functional design of the preferred integrated LRT and OR174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

The East LRT extension is one of three EA studies underway as part of Stage 2, the City's plan to extend the benefits of rail to residents farther east, west and south that will add 19 new stations and 30 kilometres to the O-Train system— see ottawa.ca/stage2 for further information.

Accessibility is an important consideration to facilitate attendance at these Open Houses. If you require special accommodation, please call 3-1-1 or e-mail one of the project leads below before the event.

From: [REDACTED]
Sent: April-12-15 9:37 PM
To: 'Taylor, Angela'
Subject: RE: hwy 174

Hi Angela

I have had a chance to quickly review the report on the EA recommendations and have a few questions. But first I like to give you my perspective.

The biggest issue that stands out for me is the cost comparison between the options. Firstly the report suggest that no homes will need to be expropriated however I find that very short sighted. The city has said that they have no intentions on committing to this project until 2031 or 16 years from now. The City of Ottawa's present infrastructure focus is on the second phase of the light rail. Obviously the time lines for such major endeavors almost always gets extended as well cost estimates surpass original estimates. With an aggressive plan on phase 2 for light rail the city will be looking at substantial and exceptional spending for both the existing as well the proposed second phase of the light rail. This will likely lead to major debt to the city for several years to come. Resulting in fiscal restraints to infrastructure projects for several years after completion of light rail. This results in the city likely extending the start of this project further past the proposed date. We all know all so well how politicians can make budgetary promises to only see years go by before an actual fiscal commitment occurs, if it actually even does. For argument sake I suggest we are looking at 20 or more years from now before this project can start. I would believe this brings the traffic count for that period at far greater levels then what is being suggested in the present outlook. So that being the case we should be looking at needs that will require even more expansion than that is presently being suggested. These points of concern all leads me to question the depth of consideration of option 1 over the others. I am the president and owner of [REDACTED] and have over 42 years of experience in heavy civil construction projects. At first glance at the issues of expanding Hwy 174 I see several items that will drive the price difference between option 1 and the other options by significant measures.

1. The cost of construction will be greatly affected by the traffic volumes that will need to be maintained. Property owners will not accept night work on a project of this magnitude. The twinning of this highway will take years to complete. Slower production results in greater cost. To properly construct this expanded highway the existing 2 lanes should be upgraded at the same time in order to have uniformity with the expanding lanes. So I believe the work will require all 4 lanes have some level of upgrade, the two existing lanes with the two new ones. Far greater scope of work than required on the other options that only have a two lane road instead of four. Compounded by the construction of a barrier wall down the middle of the road on option 1 (while maintaining traffic flow) will contribute more cost in comparison to the other options who do not show a barrier wall..
2. Substantial rock excavations need to be performed to create the needed corridor. That will have to be performed by hydraulic hammers and not drilling and blasting a substantial cost increase. I'm not aware of any expensive rock excavation operation on the other options.
3. A major hydro electrical transmission line will most likely need to be moved.
4. I was informed by a MTO representative at public information session a few years ago that they recognized when they owned hwy 17 that there was soil stability issues along the highway near the village of Cumberland, that would come into play for any widening. This also can result in substantial cost with this option.

5. The expropriation of property of home owners along the river will come at far greater cost than that along the other options.
6. I saw an earlier response by you to the question of the option of three lanes on hwy 174 with the heavier flows alternating on the extra lane. The response was that consideration was not reasonable due to the cost of overhead signs would be significant. I do not question that there is a cost for these overhead signs but this response from the city only reinforces my concern that the city does not have a true realization of the magnitude of the overall cost with option 1 in comparison to say option 3a & 3b.

Conversely if one of the other options is selected then construction could occur in a far quieter (less traffic) stretch either Willhaven option 2 or Innes Road option 3a & 3b as the bulk of the traffic will be maintained on the existing 2 lanes of hwy 174. Again this leads to quicker construction and substantial construction cost reduction.

The magnitude of the cost difference has to be established with some reasonable level of confidence at this time to properly evaluate this project.

The plan also is proposing cyclist traffic next to the new highway, I fail to see cyclist wishing to use it for safety reasons as well I do not see any ease or reasonable ability to cross the road with the barrier wall. Conversely if option 3 was selected that would see the threat of vehicle traffic to cyclist drastically reduced if those proposed cyclist paths were put on the Hwy 174 where most people reside. To me that is getting the best of two worlds. Getting the vehicular traffic away from the cyclist and putting the cyclist on a quieter scenic route where more people reside.

My questions for you are as follows.

- a) What are the estimates for cost all three options for construction?
- b) What is the estimate of time for construction for all three options?
- c) What is the estimated cost for the expropriation of land for all three options?
- d) With this project not likely occurring for minimum of 16+ years why are we not looking at traffic flows for 20 years past 2031 to 2051?
- e) Does the report take into account the social cost and construction cost if the traffic demands in 2031 are exceeded at the time of construction? If the proposed additional two lanes prove to be insufficient would there not be the need to expropriate more land to put in a total of 6 lanes and not just 4? Would option 1 in that scenario still get the same ranking as number 1?
- f) Was the option considered with options 3 to have the cyclist path put on hwy 174 instead of Innes road so that you get the traffic away from the cyclist and closer to the area where the people live and are more likely to use? Highway 174 could be seen more as a parkway and better way of connecting with the existing bike routes into the city.

Although I realize there is a weighting for various aspects of the options, I'm concerned that the economic weighting needs to be looked at closer. From my understanding of the scope of work to be done with the various options I suggest that the cost difference from option 1 to 3a & 3b is a major difference and not in the magnitude of percentage increase but more likely by factors 2X and greater. I fear that if a true cost estimate is not properly compared at this time with the optional routes, the project will get delayed again in an attempt for various governments like the City of Ottawa and Prescott & Russell to try to raise sufficiently greater amount of funds for option 1 because an earlier commitment to this option at all cost. This may lead to even more years of delay because of this enormous price tag that makes the project even more difficult to achieve.

I wait your reply.
Thank you



From: [Redacted]
Sent: April 20, 2015 4:35 PM
To: Taylor, Angela
Subject: FW: hwy 174

Hi Angela
I'm just following up on my emails I had sent to you last week. I was hoping to get reply to my emails prior to our public meeting tomorrow night, so that I would better understand many of the issues. When can I expect your reply?
Thank you



From: "Angela Taylor" <Angela.Taylor@ottawa.ca>
To: [Redacted]
Cc: [Redacted]
Sent: Tuesday, 21 April, 2015 10:28:27 AM
Subject: RE: hwy 174

Hi [Redacted] my apologies for the delay, but as you know, the study team is currently carrying out public consultations that started last week and this period is the busiest time. I have endeavoured to respond to your inquiries and is noted further below against your questions.

In general, the City is aware that the widening of Hwy174 and County Road 17 is more costly than the other corridor options. However, cost is but one factor out of 63 identified environmental impacts and while it is a significant factor in measuring the municipalities ability to construct the project, its relative importance is a competing priority amongst the 63 factors established in the evaluation and assessment of all the options. It should be noted that the assessment of the options must be consistent with the City's Official Plan policies of growth and development.

Many of your questions relate to the broader City planning documents and I encourage you to get involved during the City's next Official Plan review. A link to the City's Official Plan is attached for reference.

[http://ottawa.ca/en/city-hall/planning-and-development/official-plan-and-master-plans/official-plan\[ottawa.ca\]](http://ottawa.ca/en/city-hall/planning-and-development/official-plan-and-master-plans/official-plan[ottawa.ca])

If you have any further questions or concerns, please do not hesitate to contact me and look forward to seeing you at the Open House.

Regards,
Angela

a. What is the estimates for cost all three options for construction?

City - regrettably we are unable to provide you with cost estimates for each of the alternative road corridors because these costs were developed solely for the purpose of comparing among the alternatives and may be misinterpreted and/or misleading when viewed out of context. Proper cost estimates can only be prepared once the functional design of the road corridor has been established and this occurs once the preferred road corridor has been selected. It would be very costly and time consuming to undertake a functional design and prepare cost estimates for each road corridor option.

It should also be noted that for this study, cost is one factor out of 63 environmental factors used to assess the road corridor options and while it is a significant factor, representing 11% of the overall weight, its relative importance must compete with the other 63 factors.

I can however, offer background on how the costs were developed. During the evaluation phase, the road corridor footprint is estimated using established design criteria that are applied consistently among all the alternatives. Estimated unit prices are established for major items involved in road construction. In these estimates, we focus on major cost items such as asphalt, granular, earthworks, concrete items and structures. We calculate or measure quantities of these items using AutoCAD Civil 3D software to generate cost comparisons. Some minor items are also included in the estimate, based on a percentage of other items (e.g. dust suppression quantities are based on the tonnage of granular).

Assumptions are made, recognizing that the actual design will be refined later. For instance, a general cost for installation of signalized intersections was assumed where controlled intersections were required, even though we will be reviewing both roundabouts and signals during this next functional design phase.

During the functional design of the preferred corridor, the grading limits (toe of fill slope or top of cut slope) will be reviewed and refined to reduce property acquisition and other impacts. Once functional design is complete, the City will carry out a comprehensive review of property costs based on the functional design.

b. What is the estimate of time for construction for all three options?

City – The duration would be greatest for the widening of 174-17 since more roadwork is required and traffic must be maintained throughout construction.

c. What is the estimated cost for the expropriation of land for all three options?

City – The cost estimates prepared during the evaluation phase includes contingencies for property.

d. With this project not likely occurring for minimum of 16+ years why are we not looking at traffic flows for 20 years past 2031 to 2051?

City – As discussed at the Public Consultation Group meeting last week Thursday, the City's Official Plan (OP) documents have established population and development forecasts to the 2031 planning horizon and not beyond. These Environmental Assessment and planning studies adhere to the City's OP and supporting Transportation Master Plan (TMP).

e. Does the report take into account the social cost and construction cost if the traffic demands in 2031 are exceeded at the time of construction? If the proposed additional two lanes prove to be insufficient would there not be the need to expropriate more land to put in a total of 6 lanes and not just 4? Would option 1 in that scenario still get the same ranking as number 1?

City – The City's TMP (link attached) identifies the road infrastructure needs in Map 10 titled "Road Network – 2031 Network Concept." According to this document, the plan is to widen Hwy 174 to one additional lane each way and not beyond.

[http://ottawa.ca/en/city-hall/planning-and-development/official-and-master-plans/transportation-master-plan\[ottawa.ca\]](http://ottawa.ca/en/city-hall/planning-and-development/official-and-master-plans/transportation-master-plan[ottawa.ca])

f. Was the option considered with options 3 to have the cyclist path put on hwy 174 instead of Innes road so that you get the traffic away from the cyclist and closer to the area where the people live and are more likely to use? Highway 174 could be seen more as a parkway and better way of connecting with the existing bike routes into the city.

City – The Ottawa Cycling Plan (link attached) identifies Old Montreal Road as the cycling spine for this area and identifies paved shoulders to be implemented in Phase 2 (2020 – 2025) from Trim Road to Dunning Road. Further improvements east is also planned post 2031. However, as part of any road infrastructure project, the City's policy is to include provisions for cyclists and pedestrians in the design of a road project to encourage other modes of transportation.

http://documents.ottawa.ca/sites/documents.ottawa.ca/files/documents/ocp2013_report_en.pdf#document s.ottawa.ca

• OPEN HOUSE COMMENT SHEETS



Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Pleased to discover that there will be leaf at Tatam Rd.
- lights on 174 - consistent through

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- I think the need for stage 2, in particular key stations like place & Trim are key.
- My top concern is for a stage 3 - though there doesn't 'appear' to be a need; unfortunately stage 1 and 2 were not planned and by the time they were planned the demand is on the extreme side of high.
- On the south side of Orleans development has boomed, equally in the south/west area the area is booming. The likelihood that there are transit oriented and extremely high - currently they are driving significant distances to get to transit stations in both the east and west end of the city.
- A 'circle' line similar to what is in London, UK would ease the # of drivers on arterial roads like Hunt Club and the amount passengers on the Confederation line if they do not need to go through downtown - such as students from the east end.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

I think that it really is about time that LRT came to Ottawa, and also that extending the Confederation Line to Trillium is a good idea, because Trillium is a major stop on the current Transitway and there is no point in having a bus that runs between there and Place D'Oro when it doesn't make sense without any stops in between.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL :

ADDRESS:

PHONE :

Personal information on this sign is shared collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Public bathrooms at many more stations than just the terminal ones would be super useful!
- Trains labelled for destinations needed, not finding out you're on a train that doesn't go to the terminal you want only once you're on the train.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: _____

E-MAIL :

ADDRESS :

PHONE :

Personal information on this sign is shared is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Q: WITH THE ANTICIPATION ANNOUNCEMENT THAT THE FEDERAL GOVERNMENT PLANS TO PROVIDE ONE BILLION DOLLARS ANNUALLY TO SUPPORT TRANSPORTATION, MIGHT IT BE POSSIBLE TO BRING FORWARD THE EASTWARD EXTENSION OF THE LRT BEYOND PLAZE D'ORLÉANS TO TRIM BEFORE THE CURRENT PLANNING DATE OF 2031?
- Q: THE DIAGRAMS OF HIGHWAY 174 EAST OF TRIM DEPICT CYCLING LANES IN THE "RURAL" VERSION BUT THE DOES NOT SEEM TO BE PRESENT IN THE "URBAN" VERSION, WHERE WILL CYCLISTS TRAVEL (HOPEFULLY NOT ON THE SIDEWALK)?
- Q: WHERE THERE IS CYCLE PARKING AT AN LRT STATION WILL THERE BE SECURITY CAMERAS? IF SO, WILL THESE BE MONITORED AND WILL THERE BE INTERVENTIONAL POLICING?

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Will station design undergo TOD studies?
Landscape design of each station? PLS
AVOID a concrete jungle.
Security cameras at stations?
Allow bikes on train always!! Have a
designated car for bikes. Not universal!
Improved access from Transit Place.
Park & Ride to station.
Improved pedestrian Transit Place
station.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Cameron / 174. intersection.
- prefer traffic light to roundabout
- * - need timing adjusted with lights.
- traffic going north on Cameron often have to wait for traffic from ferry to trigger light change
- * concerns with traffic crossing from Cameron entering and that due to volume on 174.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional info:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Sound barriers are required east of Jeanne D'Arc on North side (already installed west of Jeanne D'Arc)
- I am very concerned about the closure of the east transit way from Blair to University for 3 years
- Orléans has the highest transit usage, so to divert 100% of the passengers onto Highway 174/417 will result in unacceptable time increases.
- 3 years is very long - Could this not have been done in stages?

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

We are located at [redacted] we would appreciate to be notified of the decision made which may affect our property. Please contact us as soon as possible.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Thank you for the opportunity to provide input.

① Roundabout vs traffic lights: I am concerned about the management of traffic for those trying to access the highway from the secondary roads, and those on the highway who need to cross the main traffic flow. Both options are difficult e.g. current traffic lights at Cameron and Canada already force drivers to turn when lights just turn red (there are never openings in the traffic to be able to cross). This scenario also applies to roundabouts (I prefer overpasses).

② Multipurpose pathway in Cumberland: I believe that there is a need for a pathway. Currently cyclists use Old Montreal Rd or St. Haven to travel between Cumberland and Orleans. These roads are not safe for cyclists (old Montreal Rd is speed limit is 50km/h between Cumberland & Orleans). A pathway on one side may be sufficient.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

THE CONCEPT OF CYCLING LANES
ALONG A WIDENED 174 THROUGH THE
VILLAGE OF CUMBERLAND IS TOTALLY
UNNECESSARY - WASTE OF FRONTAGE LAND
& ROAD ALLOWANCE.

NO BIKER WANTS TO RIDE BESIDE
BESIDE THIS TRAFFIC VORUNTE - FROM
NO WHERE TO NO WHERE!

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional NAME:	E-MAIL:
ADDRESS:	PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell

Lundi 20 avril 2015
Église communautaire pentecôtiste
1825, boulevard St Joseph
Orléans

Mardi 21 avril 2015
Centre communautaire R. J. Kennedy
Salles A et B, 1115, chemin Dunning
Cumberland

Jeudi 23 avril 2015
Salle communautaire de Clarence C
Creek, 418, rue Lemay
Clarence Creek

Dites-nous ce que vous pensez...

I would like to receive
a ^{hard} copy of the Plan of
widening "174"
in my Area.

Thank you

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi **15 mai 2015**, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:	
NOM:	COURRIEL:
ADRESS:	

Les renseignements personnels recueillis au cours de la présente consultation en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Retaining walls? Roundabouts - Will it start traffic?
Heavy line at my property. - Taking land would mean you would fall into my home.
Jersey Barrier?
Property value - 2 may look OK, but too noisy for future buyers!
Use Innes Road - Make exits before new industry arrives - This would allow people to walk near their homes and decrease the traffic moving in and out of Ottawa.
Fire farms more important than Route 174 homes?

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by May 15, 2015 to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

The 174 going through Cumberland along the water does not need sidewalks because we need an 80 km/h fast road for cars. No body lines around them to walk. (Not in village) We need sidewalks on old Montreal Road from Trill to Dunning. No sidewalks are needed on 174 to Rockland.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by May 15, 2015 to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

With the Rock cut through the old quarry, how do so
go that do not have more sound resonating off a
stone wall? The option of Sound barrier on House
side properly located would become major
eyesore as well as major decrease in property
value. How can this be addressed.

What options do we as property owners have
if we do not feel our questions are being properly
addressed?

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela
Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to
contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County
Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management,
110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Cameron should not have a roundabout
ever.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela
Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to
contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County
Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management,
110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning

ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Please realign 17/174 Rd to be perpendicular off Hwy 17. It would become more secure, reduce dead end and permit a sound barrier.

The present road alignment is very dangerous especially during the winter time.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

The community along the 17/174 is already exceedingly long and congested. It is the single largest factor impacting growth, property values and quality of life in Prescott-Russell. It is a highway and the major artery to all points east. It is not a local road. Why would 70 not be sufficiently low? Local buses are great but pierce beyond reach, and don't allow us to get home in an emergency. Superior Park + kids + easy transitions and the only way to encourage people to use Ottawa's public transit is widening of 17/174 up to the transition at LRT would ease congestion from that point, would have an immediate impact. The time frame proposed is not acceptable time to sell + move to QC.

203127

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Lundi 20 avril 2015 Église communautaire pentecôtiste 1825, boulevard St Joseph Orléans	Mardi 21 avril 2015 Centre communautaire R. J. Kennedy Salles A et B, 1115, chemin Dunning Cumberland	Jeudi 23 avril 2015 Salle communautaire de Clarence C Creek, 418, rue Lemay Clarence Creek
---	---	--

Dites-nous ce que vous pensez...

*Il faut absolument "re-penser" à l'entrée
du chemin voisine.
Changer l'entrée d'endroit!
Nous serons coincés dans notre rue.
Impossible d'entrer!
Impossible de sortir!
Passer la route par l'arrière de Rockland!*

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 15 mai 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM: [REDACTED] COURRIEL: [REDACTED]

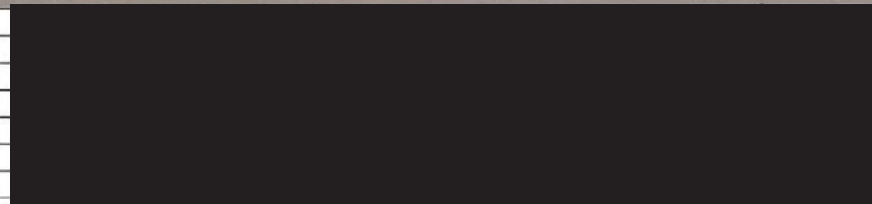
ADRESS: [REDACTED] TÉL: [REDACTED]

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'accès à l'information, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Lundi 20 avril 2015 Église communautaire pentecôtiste 1825, boulevard St Joseph Orléans	Mardi 21 avril 2015 Centre communautaire R. J. Kennedy Salles A et B, 1115, chemin Dunning Cumberland	Jeudi 23 avril 2015 Salle communautaire de Clarence C Creek, 418, rue Lemay Clarence Creek
---	---	--

Dites-nous ce que vous pensez...



Commentaires:

- ① le minimum de carrefours giratoires
- ② Débuter plutôt pour terminer en 2031.
- ③ Corriger voisine pour les piétons et les motoristes

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 15 mai 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM: [REDACTED] COURRIEL: [REDACTED]

ADRESS: [REDACTED] TÉL: [REDACTED]

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'accès à l'information, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

I feel that you have lost sight of the issue, which is provide better commuting for residents EAST OF TRIM. We don't need more traffic lights or pedestrian walkways by the water or roundabouts. We need a faster, multi-lane roadway for our 50 km⁺ into Ottawa to work.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

As far as using Roundabouts - might be OK for city roads (if people knew how to use them, which they don't!) but should not be a consideration for ANY Highway.
your traffic numbers should also include the traffic on the ^{baseline/line} ~~baseline/line~~ + Old Hwy 17 through Cumberland since the ^{Horrendous city Rd!!} ~~Horrendous city Rd!!~~
We need a 4 lane highway now (we've needed it for a long time - years!) east of Trim Rd to past Rockland - Now - not in 16 years!
And highway traffic numbers shouldn't be based on those who work but many of us are retired and many travel to + from Rockland → Ottawa just to shop/eat/visit etc... which increases the traffic numbers quoted to me exponentially! So by 2031 - is 4 lanes going to be enough?
Having been one of the many people stuck on Hwy 17 traffic east of Trim for the past (20) years, the Highway is LONG past due and by 2031 - that's just dangerous and reminds to wait that long.

Just a comment - TransCanada Highway 17 used to be a "controlled access highway" before it was transferred to the various provinces - a suggestion would be that the [new] widened highway be designated as a controlled access highway to control ingress & egress to minimize + control them (so you don't run into future problems like the ones in Cumberland).

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Having travelled Hwy 17, I find traffic lights on a highway to be quite detrimental to a great number of drivers for a limited benefit to local traffic. As per on/off ramps work much better in all situations including hwy and local traffic users. Can you have a look at Amper / Hwy 17 results when considering traffic impact.

Have you considered adding a 2nd level to the existing park-and-ride at Place d'Orléans? It could line up with the existing pedestrian overpass.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prescott
Russell

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

RE: WIDENING OF THE 174, TAIL TO LANDAY:

- ① CONSTRUCTION 2031? THAT DOES NOT HELP US AND MORE LIVES WILL BE LOST, DO WE NOT MATTER? WE PAY TAXES TOO.
- ② TO MANY LIGHTS. COLLECTOR ROADS ARE THE ANSWER.
- ③ ROUNDABOUTS ON A 100KM HWY? NOT A GOOD IDEA IN MY OPINION, ESPECIALLY WITH ALL THE TRUCKS/TRAILERS.
- ④ THERE IS NOTHING STOPPING THE WIDENING OF THE 174 NOW, EXCEPT FOR POLITICS. THE LAT BEING IS NOT IMPACTED BY DOING SO.
- ⑤ PEDESTRIAN CROSSING WITH LIGHTS @ CUMBERLAND? IT'S NO DIFFERENT THAN STOPPING 100KM TRAFFIC ON THE QUEENSWAY. IT WON'T BE HE USING IT. RUSSIAN ROULETTE.
- ⑥ TRUCK TRAILERS SHOULD BE RE-ROUTED TO THE 417.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

OVER →

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Lundi 20 avril 2015 Église communautaire pentecôtiste 1825, boulevard St Joseph Orléans	Mardi 21 avril 2015 Centre communautaire R. J. Kennedy Salles A et B, 1115, chemin Dunning Cumberland	Jeudi 23 avril 2015 Salle communautaire de Clarence C Creek, 418, rue Lemay Clarence Creek
---	---	--

Dites-nous ce que vous pensez...

- ⑦ PEDESTRIAN CROSSINGS SHOULD BE CONSIDERED IN STRATEGIC LOCATIONS.
- ⑧ WHAT CONTROL MEASURES ARE BEING IMPLEMENTED TO KEEP CARS FROM PASSING ON THE RIGHT IN BIKE LANES?
- ⑨ LEFT TURN LANES @ CUMBERLAND FERRY NEED TO BE LENGTHENED CONSIDERABLY GOING EAST. THIS REALLY BACKS UP, ESPECIALLY ON FRIDAYS. (SARGENT)
- ⑩ WITH ALL THE LIGHTS, TRAFFIC CIRCLE, U-TURN, ETC., THIS IS NO LONGER A HIGHWAY, BUT MORE LIKE A BOULEVARD THROUGH A RESIDENTIAL AREA. DURATION OF TRAVEL TIME IS GREATLY IMPACTED.
- ⑪ WHAT HAPPENS WHEN OREA/MORSE CROSS THE ROADS WITH ALL THE BARRIERS?

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 15 mai 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADRESS :

TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Lundi 20 avril 2015 Église communautaire pentecôtiste 1825, boulevard St Joseph Orléans	Mardi 21 avril 2015 Centre communautaire R. J. Kennedy Salles A et B, 1115, chemin Dunning Cumberland	Jeudi 23 avril 2015 Salle communautaire de Clarence C Creek, 418, rue Lemay Clarence Creek
---	---	--

Dites-nous ce que vous pensez...

- Trop de lumières de circulation
- Need overpass instead.
- Besoin autoroute Urgent. car l'économie de Richmond est en souffrance.
- A l'ouest d'Ottawa il y a une autoroute pour une grande distance à l'est vers Richmond avec une autoroute. Pourquoi?
- Sur la 17 il ne devrait pas avoir de left turn par les résidences, les propriétaires devraient celles faire un détour pour entrer sur le boulevard du chemin.

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 15 mai 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADRESS :

TÉL :

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell

Lundi 20 avril 2015
Église communautaire pentecôtiste
1825, boulevard St Joseph
Orléans

Mardi 21 avril 2015
Centre communautaire R. J. Kennedy
Salles A et B, 1115, chemin Dunning
Cumberland

Jeudi 23 avril 2015
Salle communautaire de Clarence C
Creek, 418, rue Lemay
Clarence Creek

Dites-nous ce que vous pensez...

No more traffic light !!
No road point !!
Keep it simple - already ~~have~~ take a
lot of time to get to work and back
home!
Keep it simple !!!
Pas besoin de pathway au long de l'autoroute
pas de pistes dans cette région
Don't reduce speed limit!

Veuillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 15 mai 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM: [REDACTED] COURRIEL: [REDACTED]
ADRESS: [REDACTED] TÉL: [REDACTED]

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Confederation Line East LRT Extension/ Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

① Round-A-Road "NO" NEVER ON A MAJOR ROAD!
② TRAFFIC LIGHTS. "YES"
③ WIDENING 174 IS ABSOLUTELY NECESSARY!
Other locations would not be suitable.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: [REDACTED] E-MAIL: [REDACTED]
ADDRESS: [REDACTED] PHONE: [REDACTED]

Personal information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Keep Studying !

15 years Late!

Relocate Bike Path
away from High
traffic Area!

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Cumberland: à carrefour pas de carrefours giratoires mais plutôt favoriser des feux de signalisation.

Clarence: à Brandon: garder les feux de signalisation

Pistes cyclable + trottoirs sont inutiles sur une highway

Avec la limite de vitesse réduite à 80 km/h & 60 km/h plus les feux de signalisation additionnels, je ne crois pas que ça vaudra la peine de dépenser autant d'argent pour ce projet puisque ce sera aussi long se rendre en ville.

Une highway est fait pour rouler plus vite que 80 km/h.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal Information on this sign is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

No Pound aboute in kendry
and Cumberland Keep light's
we work in Ottawa we pay tax

im NOT Shure to see the day that will take
Place ---

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional info:

NAME:

E-MAIL:

ADDRESS:

PHONE:

Personal information on this sign-in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1



Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell

Lundi 20 avril 2015
Église communautaire pentecôtiste
1825, boulevard St Joseph
Orléans

Mardi 21 avril 2015
Centre communautaire R. J. Kennedy
Salles A et B, 1115, chemin Dunning
Cumberland

Jeudi 23 avril 2015
Salle communautaire de Clarence C
Creek, 418, rue Lemay
Clarence Creek

Dites-nous ce que vous pensez...

un feu de circulation pour les deux sens
cameron et Barnett

close the ~~two~~ road fambert and
Pot pour Construction acheter une maison sur Edward
et faire leur sorti la

Se fait beaucoup transfer pour les gens bus train
bus

due the road faster than 9031

Il vont fu faire la route avant le train la route
devrais faire en premier

Veuillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi **15 mai 2015**, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL:

ADRESS

TÉL:

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

I live in Rockland and work in downtown Ottawa.
I use public transit - reduce bus lines.

I applaud the plan for extension of Highway 17. I have
seen many near accidents on this simple & late Highway
which is closer to a country road. The traffic volume
dictates a better highway.

I cannot believe in this day and age that we are forced
to use this UNSAFE highway. Aside from budgetary
considerations, I am in a state of disbelief that this
unsafe and SCARY situation can continue.

We are poorly served to the point of being put in dangerous
situation.

If I do drive to Orleans, I will take Old Montreal Road
but much as I can go at least I can drive slower on
a country road. I am afraid to use the 17.

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by May 15, 2015 to: Angela
Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: [REDACTED]

E-MAIL: [REDACTED]

ADDRESS: [REDACTED]

PHONE: [REDACTED]

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to
contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County
Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management,
110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

Confederation Line East LRT Extension/

Ottawa Road 174 / Prescott-Russell County Road 17 Widening

Monday, April 20, 2015
Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015
R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015
Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

- Roundabouts along Hwy 174 between
Trim Rd and Rockland are a bad idea;
traffic flow during rush hours will be negatively
affected

- Need FUNDING! Hwy 174 widening must be
done as soon as possible! cannot wait
until 2031!

- congestion on Hwy 174 is a serious problem
right now, not in 2031

- several fatalities and serious injuries
because of dangerous 2 lane road with
too much traffic (need 4 lanes now!)

- Need at least a dedicated ramp (right turn) from Hwy
Cumberland Ferry at Cameron Road or an
overpass since this traffic light is heavily
congested (back ups to Trim Rd sometimes)
particularly in summer for cottage traffic

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by May 15, 2015 to: Angela
Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: [REDACTED]

E-MAIL: [REDACTED]

ADDRESS: [REDACTED]

PHONE: [REDACTED]

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to
contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County
Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management,
110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

This intersection has a huge impact on level of service
of the entire Hwy 174 from Trim to Rockland.

ottawa.ca/planning
ottawa.ca/urbanisme

110 av. Laurier ave. Ouest/West, Ottawa (Ontario) K1P 1J1

*** more on back page

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Lundi 20 avril 2015 Église communautaire pentecôtiste
1825, boulevard St Joseph
Orléans

Mardi 21 avril 2015 Centre communautaire R. J. Kennedy
Salles A et B, 1115, chemin Dunning
Cumberland

Jeudi 23 avril 2015 Salle communautaire de Clarence C
Creek, 418, rue Lemay
Clarence Creek

Dites-nous ce que vous pensez...

- you seem to place too much emphasis on traffic calming and slowing down traffic; the emphasis should be on getting the most traffic flow from Rockland to Ottawa to minimize congestion
- 60 and 80 km/hr is too slow for a major highway !!
- the design proposed today is a good secondary road (80 km / 100 km / hr) but Rockland needs a real highway (120 km/hr)

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi 15 mai 2015, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM: Luc Frechette COURRIEL: luc.frechette@pm.com

ADRESS: 496 Du Roussau Rockland TÉL: 613-415-7382

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, C. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur la Prolongement vers l'est de la Ligne de la Confédération du système (TLR) Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la cueillette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

**Confederation Line East LRT Extension/
Ottawa Road 174 / Prescott-Russell County Road 17 Widening**

Monday, April 20, 2015 Community Pentecostal Church
1825 St. Joseph Boulevard
Orléans

Tuesday, April 21, 2015 R. J. Kennedy Community Centre
Hall A & B, 1115 Dunning Road
Cumberland

Thursday, April 23, 2015 Clarence Creek Community Hall
418 Lemay Street
Clarence Creek

Tell us what you think...

Carefree Traditional
(no runabouts)
CLOSE CAMERON ST NORTH

Please leave your comment sheet in the box on the table or mail/fax/e-mail your comments by **May 15, 2015** to: Angela Taylor, City of Ottawa, 110 Laurier Ave. W, Ottawa, ON K1P 1J1
Tel: 613-580-2424, ext. 15210, Fax: 613-580-2578, e-mail: angela.taylor@ottawa.ca

Optional information:

NAME: [REDACTED] E-MAIL : [REDACTED]

ADDRESS: [REDACTED] PHONE : [REDACTED]

Personal Information on this sign in sheet is collected under the authority of the Planning Act, R.S.O. 1990, c. P.13 and may be used by the City of Ottawa to contact attendees for the purpose of providing further information about the Confederation Line East LRT Extension/Ottawa Road 174 / Prescott-Russell County Road 17 Widening. If you have any questions about the collection and use of your information, please contact Angela Taylor, Planning and Growth Management, 110 Laurier Avenue West, Ottawa, Ontario K1P 1J1, 613-580-2424, ext. 15210, angela.taylor@ottawa.ca.

**Prolongement vers l'est de la Ligne de la Confédération du système (TLR)
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell**

Lundi 20 avril 2015

Église communautaire pentecôtiste
1825, boulevard St Joseph
Orléans

Mardi 21 avril 2015

Centre communautaire R. J. Kennedy
Salles A et B, 1115, chemin Dunning
Cumberland

Jeudi 23 avril 2015

Salle communautaire de Clarence C
Creek, 418, rue Lemay
Clarence Creek

Dites-nous ce que vous pensez...

Les pistes cyclable devraient être détaché de la route car trop dangereux pour les enfants.

Ne serait-il pas possible de faire des sorties et entrées par l'un de faire des carrefours giratoires ou mettre des lumières de police.

Combien de lumières sont prévues ds ce trajet?

- beaucoup trop.

Est-ce que Des abris de train seront chauffé pour l'hiver?

- coût très cher pour l'entretien.

Il serait ridicule de prendre l'argent pour faire un train plutôt que de faire un 4voies puisque le 17 restera Achalandé quand même.

Veillez déposer votre fiche de commentaires dans la boîte placée sur la table ou nous transmettre vos commentaires par la poste, par télécopieur ou par courriel d'ici au vendredi **15 mai 2015**, à l'attention de : Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424, poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

Optional information:

NOM:

COURRIEL :

ADDRESS :

TÉL:

Les renseignements personnels sur cette feuille de présence sont recueillis en vertu des pouvoirs conférés par la Loi sur l'aménagement du territoire, L.R.O., 1990, c. P. 13 et peuvent être utilisés par la Ville d'Ottawa afin de communiquer avec les personnes présentes pour leur fournir d'autres informations sur l'étude de gestion de la circulation locale sur le Prolongement vers l'est de la Ligne de la Confédération du système (TLR).
Élargissement de la route 174 d'Ottawa / chemin de comté 17 de Prescott-Russell. Si vous avez des questions sur la candellette et l'utilisation des renseignements personnels, veuillez communiquer avec Angela Taylor, Ville d'Ottawa, 110, av. Laurier O., Ottawa (Ontario) K1P 1J1, 613-580-2424 poste 15210, téléc. : 613-580-2578, courriel : angela.taylor@ottawa.ca

ottawa.ca/planning

ottawa.ca/urbanisme

From: [REDACTED]
Sent: Monday, April 20, 2015 10:29 AM
To: [REDACTED]
Subject: RE: Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies

I am unable to attend any of these meeting and am looking for additional information. Could you please tell me if the train is supposed to go on the north or south side of the 174? The maps on the city of Ottawa's website are not clear with regards to its placement.

Regards,

[REDACTED]

From [REDACTED]
To: [REDACTED]
Subject:
Date: Mon, 20 Apr 2015 16:31:03 +0000
Hi [REDACTED]

Our preferred plan shows the LRT on the north side of 174 from Blair Station to Montreal Road (new station). Just east of Montreal Road, the LRT will transition into the median of 174 and continue in the median as far as Trim Road. The new plans will be on the web site soon (say next week) for your information.

[REDACTED]

[REDACTED]

From: [REDACTED]
Sent: Monday, April 20, 2015 1:34 PM
To: [REDACTED]
Subject: RE:

Hello,

Thank you for your response. Is there a proposed sound barrier with this project?

[REDACTED]

From: [REDACTED]
To: [REDACTED]

Subject: RE: RE:
Date: Mon, 20 Apr 2015 17:38:12 +0000

Areas that are not currently protected as expected to have a noise wall. We are documenting these locations.

If I don't remember to send you a picture of your area, send me a reminder email next week. With the Open Houses this week, I don't have time to find what I want to send you.

Val
Valerie McGirr, P. Eng.
Manager, Ottawa Office
D 613.820.8282 ext 243

AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

From: [REDACTED]
Sent: April 22, 2015 8:29 AM
To: Taylor, Angela
Subject: Highway 174 Widening Meeting

Good morning, I was at meeting in Cumberland last night. Can you send me the document pertaining to the four options for the east end (with the 63 criteria and the matrix)? I just want to see where the Innes Road option rated in comparison to the Highway 174 option.

I live in Chatelaine Village so my concern is the sound, and the need for a sound barrier. I believe the sound barrier installed west of Jeanne D'Arc on North side of Highway 174 has helped, so I look forward to this being continued along the highway east.

Thank you.

From: Taylor, Angela
Sent: April 22, 2015 8:37 AM
To: [REDACTED]
Subject: RE: Highway 174 Widening Meeting

Hello [REDACTED], as requested, I am forwarding you the Evaluation Report for the Hwy174/County Road 17 Environmental Assessment Study and look forward to your comments. Note that this evaluation report only reviewed the route corridors between Trim Road and Clarence-Rockland and that your area of Chatelaine Village is outside of this road segment. If you have any question or concerns, please don't hesitate to contact me.

From: [REDACTED]
Sent: April 22, 2015 12:54 PM
To: Taylor, Angela
Subject: Comments on 174 EA

Hi Angela,

I attended the meeting last night and want to tell you how impressed I am with the work being done on this project. I was originally opposed to widening the 174, but I think it will be a big improvement for our community. The combination of HOV lanes and the connection to light rail will relieve traffic congestion in the future. I don't think we can (or should) keep building new roads -- public transit is the way to go.

I have been active in the Cumberland Community Association as a board member and president over the last 12 years. My interest is in community planning, protecting the character of our historic village and encouraging new business along Old Montreal Road. The plan that you proposed reflects our community vision and demonstrates that you have listened carefully to our views and values. I don't think that city staff always receives the praise it deserves, so I wanted to take time to thank you.

I think the recreational paths will encourage residents to take time to enjoy our community and stop at a local coffee shop, boutique or restaurant that we'd like to have on our main street in the future.

My only concerns are that the new "urban design" of the street scape should reflect the rural, historic character of our community. We consider ourselves more rural than urban, but I realize that this term is probably used to distinguish residential areas from the farmland along the 174.

Secondly, I strongly agree that roundabouts would not be suitable in the village core, especially at Cameron Street intersection.

all the best,

From: [REDACTED]
Sent: April 21, 2015 10:22 PM
To: Taylor, Angela
Cc: [REDACTED]
Subject: HWY 174 Meeting

Dear Angela,

The meeting on February 2, 2015 was the first meeting that I was able to attend on the widening of HWY 174. At that meeting I felt like it was being rammed down our throats, and remember being told that the only thing that can stop it is if City Council votes against it.

At this evening's meeting it became apparent to me that you and the consulting team are actually looking at finding positive solutions for the issues, and today I am much more comfortable with the prospect of the highway widening. I guess you really do know what you are doing! 😊

While my property is not one of those along the highway, I am just off Cameron Street at the ferry, I am affected when I need to get into rush hour traffic in the morning, and to a lesser extent, in the afternoon rush hour.

Initially I thought that having a round-about at the intersection would be wonderful. As I explained to you after the meeting, I love round-about, but after thinking about it I started wondering, because the traffic flow is east/west, if I would even be able to get onto the highway with the heavy morning flow which is pretty much bumper to bumper.

Coming home at night would be less of a problem because I would be in the flow of traffic and just keep going round until I came to the north side of Cameron.

Another problem at that intersection is traffic lineups on Cameron Street to the ferry. This is an issue on Friday evenings in the summer as well as 3 to 4 PM weekdays (year round), when all the contractors are taking the ferry home to Quebec. Often the lineup of vehicles backs up to the 174 intersection, (and onto the south section of Cameron), which would mean that cars exiting the round-about would not be able to clear the round-about and therefore traffic would get backed up on the 174 until the cars were able to board the ferry. At least they can currently sit in the left turn lane until they can turn onto Cameron, and the other cars can pass them in the main lane.

Another issue is the loss of property affecting my friends and neighbours at the intersection if a round-about is installed. I would hope that this widening has as little impact as possible on adjacent properties.

All of that said, I would like to offer my opinion that the intersection at 174 and Cameron have traffic lights instead of a round-about.

Angela, initially I was opposed to the widening of 174... prepared to chain myself to a bulldozer... but I'm starting to see how it might actually benefit the Village of Cumberland. I understand the fear of those who's property will be affected and hope that it won't be as bad for them as they think it will be.

By the way, if West Street is closed at the 174, do you really need the turnabout area that will affect property of my very vocal neighbour (lady with the short gray hair)? Duke Street, which is one street to the west, is closed at the highway and has no turnaround space. The only people who use it are the property owners, their visitors, the occasional sightseer, Google and the garbage truck, which backs in, and the snow plow. No one seems to have a problem with it. Just a thought!

Thank you for an excellent presentation this evening. When can we expect to hear more?

Best wishes,

From: [REDACTED]
Sent: April 21, 2015 9:40 PM
To: Taylor, Angela
Cc: [REDACTED]
Subject: Montreal Road LRT station

Hello,

I attended last night's information session and found it informative, however, the discussion never got to our local station! I think general questions should have been left until the end.

I am a [REDACTED] woman and live on [REDACTED]. Several years ago, OC Transpo eliminated our local bus along Montreal Rd. Since then, outside of rush hour, I have about a 12 minute walk east to take the 95 downtown. This walk is not an easy one, especially in winter conditions. Coming home, the walk is about five minutes longer and takes me under an overpass. It is totally unsafe for a woman after dark. Also, a few weeks ago, there was a combination of huge puddles and ice and I had to walk almost perpendicular along an embankment to get through the area – very hazardous. My point is – **please make the route to this station safe and accessible for seniors and women of all ages!!!**

From: [REDACTED]
Sent: Thursday, June 25, 2015 7:45 AM
To: Taylor, Angela
Subject: Stage 2 Confederation Line East Report

Hello,

I have read the report concerning the Confederation line East and would like to know where the sound barriers will be erected. I live at [REDACTED] which backs on to the south side of the 174 and am very concerned about the current noise levels and can't imagine what it will be like with more traffic and a train.

The report mentions that 5km have been identified as needing a wall but it did not specify which 5km would be receiving the barriers. I was hoping you could give me more information concerning this part of the project as well as a timeline as to when the sound protection would be put in.

Regards,

From: Taylor, Angela
Sent: Thursday, June 25, 2015 1:36 PM
To: [REDACTED]
Subject: RE: Stage 2 Confederation Line East Report

Hello [REDACTED], the 5 km of noise barriers is only an approximation based on preliminary findings. We are currently finalizing the noise study as well as the location of sound barriers where they are warranted. Once complete, we would be in a position to provide you with specific information in your area. If you wish to receive further information, please check back with us in a few weeks.

From: [REDACTED]
Sent: Thursday, April 30, 2015 10:29 AM
To: McGirr, Valerie
Subject: FW: OR 174 plan for road widening: input request from Emergency Responders

Valerie,

Looks like you have addressed frequent breaks in the median allowing emergent access. I have no further concern at this point from a communications/dispatching point of view.

From: [REDACTED]
Sent: April 25, 2015 11:55 AM
To: Taylor, Angela
Cc: [REDACTED]
Subject: Confederation Line LRT Extension/Highway 174 Widening

Hello Angela

I attended the meeting held in Cumberland Village last week, April 21. I would like to provide a couple of comments and questions.

First, thanks for the information provided at the meeting. With regard to the widening of Highway 174, maps of the design, staff available to answer questions and the overview by AECOM staff were helpful.

Comments

Re the LRT, given the development of Cardinal Creek I think extending the line to Trim Road as soon as possible is necessary.

Re the widening of Highway 174, I believe

-traffic lights at Cameron and Barnett streets are a better option than the roundabout option
-that pedestrian and cycle pathways must be included in the design. Besides adding to the quality of life for residents those using the highway should realize they are driving in a residential area and not on a throughway there for their convenience

A couple of questions

Are there plans for some traffic calming on Old Montreal Road as it is often used by motorists trying to avoid congestion on the highway?
Will sound barriers be considered for East Shore and Morin Road? Many of the backyards face south west or north east. I imagine some of the trees that provide a buffer now will be removed and noise will be an issue.

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Thursday, April 30, 2015 2:31 PM
To: [REDACTED]
Cc: McGirr, Valerie; Kelly Roberts
Subject: RE: Confederation Line LRT Extension/Highway 174 Widening

Hello [REDACTED], thank you for your comments and positive feedback. They will be included in the documentation as part of the public record. To respond to your inquiry on traffic calming on Old Montreal Road, there is currently no plan in place. Although you did not indicate a specific section of Old Montreal Road, the Cardinal Creek Village community development is underway and there may be opportunities to improve Old Montreal Road in the near future.

With respect to sound barriers, the study team is currently assessing the sound levels for the future widening and where sound barriers are warranted, the study will identify areas requiring barriers. While East Shore Road and Morin Road properties will not be impacted by the widening, the proposed service road will require clearing and grubbing of existing trees, shrubs, foliage, etc.

Please check back in the next few months for an update to the sound barriers.

Regards,

From: [REDACTED]
Sent: May 10, 2015 3:10 PM
To: Taylor, Angela
Subject: Material from Ottawa east LRT April meetings

Hi Angela,
We were unable to attend the April public meetings and are curious to see the material that was presented. It is not yet up on the website, could you possibly email me the preferred alternative and design?

Will the ESR be available to review on line or only in hard copy?
Thanks very much,

From: McGirr, Valerie
Sent: Monday, May 11, 2015 8:09 AM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Material from Ottawa east LRT April meetings

Dear [REDACTED]
The text from the display boards is attached. I will send you the drawings separately as they are larger. The ESR will not be available on-line. We will add you name to our mailing list, if you aren't on it already, so that you will get an email when the ESR is ready for public review (later in the fall/winter).

From: [REDACTED]
Date: Mon, 11 May 2015 12:11:08 +0000
To: [REDACTED]
Subject: RE: Material from Ottawa east LRT April meetings

Is there a particular area you would like to see? The full 34 km from Highway 417 to Rockland includes 6 large files so if you are only interested in the Highway 417 to Trim Road section, I will just send you that one. Otherwise I will send you all five.

From: [REDACTED]
Sent: Monday, May 11, 2015 9:56 AM
To: [REDACTED]
Subject: Re: Material from Ottawa east LRT April meetings

Hi,
Yes just the section from Hwy 417 to Trim Road please, including anything to do with the proposed interchange.
Thanks for the prompt reply,

From: [REDACTED]
Sent: May-08-15 2:35 PM
To: Monette, Bob
Subject: Website Inquiry

Good day

My name is [REDACTED] I live at [REDACTED] in Orleans. We moved into this house 9 years ago. I would like to invite you to visit my house and backyard. Since Ottawa hydro cut down most of the trees beside my house and the Queensway the noise has gotten worst. We cannot open windows or leave the screen on the patio door open. We have trouble speaking to each other in the back yard. I couldn't make you public meeting on the sound wall. Will we be getting a wall? We are thinking of moving because of the noise. Which will mean we would not get the real value of our house.

When the big trucks go by the house vibrates. When rocket bikes or cars go by this is the loudest.
Please come and visit.
If you help in getting a sound wall I am willing to help out.

On May 9, 2015, at 10:49 PM, Monette, Bob <Bob.Monette@ottawa.cawrote>:

Good evening [REDACTED],

Thank you for your email and taking the time to write to me. The City is currently conducting an environmental assessment for phase 2 of LRT which will see it extended to Place d'Orléans; we will be the first suburban community with LRT in the City. As part of this EA, several public consultations have been held in order to gather residents comments including on the need for sound attenuation barriers. A number of residents have come out to provide this feedback and I hope that you also had the opportunity to participate. If not, please let me know and I will ensure to forward your comments to the group leading the EA.

As far as being your elected official, I fully support sound attenuation barriers and have made my position clear to staff and residents alike. I look forward to seeing the recommendations that arise from the assessment and hope to see this recommendation included.

Thank you again for your comments

Bob

Bob Monette
Deputy Mayor - Maire suppléant
City Councillor Orléans Ward - Conseiller municipal du quartier
Orléans

Orléans Ward - Proud to be home to 50,000 residents Quartier Orléans -
Fier de compter plus de 50,000 résidents

From: [REDACTED]
Sent: Saturday, June 06, 2015 9:16 AM
To: [REDACTED]
Cc: Taylor, Angela; [REDACTED]
Subject: Re: OR 174 / CR 17 Class EA Study - Étude d'évaluation environnementale de portée générale: Route 174 d'Ottawa et route 17 du comté de Prescott-Russell

Lowering the current speeds and adding a late number of new stop lights will lengthen the current commute enormously. Regardless of the intersections containing "actuated lights". This is not a cynic drive nor a parkway, this road is the principle commuter highway for all towns east along the river. Putting the preferences of citizens living in one section (that are relatively few) over the needs of entire towns hardly seems fair. Perhaps the current Titus quo is actually preferable at that rate.

From: [REDACTED]
Sent: Friday, May 15, 2015 11:29 AM
To: Taylor, Angela
Subject: Comments: Confederation Line East RLT

Overall: I like the route and much of the plan. But I'd build fewer stations that are proposed at this stage. I would reserve the space but my first priority for building stations is stations where currently OCTranspo has a major transfer point or where there is space for extensive parking.

Deer: What plans have been made to maintain wildlife access across the LRT route? Part of this corridor has frequent deer sighting at dusk as the animals move from the greenbelt and farm land towards the river. My concern is that the jersey fence keeping cars from careening into the rail line will also trap deer between the fence and the road.

Orleans Boulevard station: I am opposed to this station as a waste of money. I think reversing the space is good but I don't think a station at this location makes sense. This road is not a high pedestrian, car or bus road now. It has very little commercial development. I don't think adding a station here makes sense based on the population of the area.

I understand that the station is intended for north-south bus traffic when the routing changes. But I don't think this makes sense either. If you run buses up Orleans Blvd, you get to Jeanne d'Arc and you would cross the bus routes running from Jeanne d'Arc. If you intend to run a loop from Jeanne d'Arc to Place d'Orleans, it will be difficult to service this station too. I think it makes more sense to have the local bus service on St Joseph and Jeanne d'Arc and skip Orleans Blvd. There is little need for a bus down that small stretch unless you add a LRT station there.

Jeanne d'Arc station: I remain concerned about how we will add everything needed to that bridge. But I assume the plan will require the replacement of the entire bridge to add at least one bus lane on the each side to support bus loading and unloading. We will also need a much wider sidewalk (and likely a bike lane) on both sides. I am curious how you can realign the 174 on ramps if the bridge is widened. I understand the plan if the bridge is not widened. But I can't see how the LRT passengers can be accommodated without widening the bridge.

Proposed Shrenkman station: I don't understand this proposed location. It doesn't make sense to me because it would require quite a long pedestrian bridge to support access to the houses on the other side of the highway. I suspect that better bus service on Centrum is a cheaper safer approach than LRT access to this site. I would reserve the land but not develop the station at this stage.

Tenth Line: I am not sure about this stop. I am inclined to think that Orleans South would have faster access via Trim than via Tenth Line. Reserving this on-ramp for cars only might speed everyone along. The on-ramp at Trim Rd will be complex enough to slow cars. So perhaps running Orleans South buses via a hub at Trim road makes more sense.

Place d'Orleans: I am concerned that the changes to local bussing will destroy this commercial centre. Currently the hub and spoke bus system provides many opportunities for commuters to shop in this centre.

Night time service: I would like to see the proposed route for night time bus service when the LRT is closed for maintenance. My attempt to plot the run runs into hiccups if I assume all stations must be serviced all night. Is the plan to run to each station or will only some stations get bus service in the wee hours of the night?

Bike storage: I don't think the current station design has sufficient bike storage. We know that many people who are bike users may become dual bike people with one bike parked at each end of their daily commute. So I don't think the 25 spaces will be enough. I think the plan needs to provide much more space to extend bike storage as demand grows in the next 25 years. Climate change may also make this a more attractive option.

There is likely demand for indoor storage (for a charge). I would recommend you look at the bike room in the new 90 Elgin st building (opposite the Melcalfe Eastbound station). That model might be a revenue stream for the city.

Parking: I am a bit concerned that we aren't planning for enough surface parking at some locations. I wish the plan included parking garages at Jeanne D'arc and Trim. (The location at Jeanne d'Arc isn't ideal unless a parking garage is built on the property of the rec centre. But at the current time there would be space across the street for temporary parking while a garage was built.) My feeling is people are more likely to adopt LRT use if they don't have to take a bus to the LRT and another bus at the other end of the LRT. Two transfer routes just are more of a pain than one transfer routes. So I'd prefer to see LRT stations with plans for compact but extensive (not free) parking.

From: [REDACTED]
Sent: May-20-15 8:59 PM
To: Monette, Bob
Subject: Sound Barrier along 417

Good evening Mr. Monette

I have lived at [REDACTED] for five years now. Our townhouse is situated directly across from the 417. Noise pollution is great in this area due to the loud traffic, even in our backyard. We keep our windows closed most of the time due to the thunderous cacophony nearby.

In the past few years, I have noticed that sound barriers have been constructed along the 417 near the Jeanne D'arc exit. I would greatly appreciate it if our stretch of highway near the Place D'Orleans exit would also be considered to have a sound barrier erected. My neighbour across the street, [REDACTED], suggested that I write to you and ask if you could kindly forward my email to the Environmental Assessment Group.

Thank you for your consideration in this matter.

From: Monette, Bob
Sent: Wednesday, May 20, 2015 9:56 PM
To: [REDACTED]
Cc: Taylor, Angela
Subject: RE: Sound Barrier along 417

Good evening [REDACTED],

Thank you for your email. This is important feedback to hear from residents particularly with the current environmental assessment taking place for the arrival of light rail to Orléans as well as the potential widening of Highway 174. I have taken the liberty of copying Angela Taylor who is in charge of the assessment to ensure that your comments are included.

As City Councillor, I have been clear that I support noise attenuation measures where warranted and this continues to be my position. Thank you again for sending me your feedback, hopefully other residents in the area have also done the same or will do so shortly.

Thank you,
Bob

Bob Monette
Deputy Mayor - Maire suppléant
City Councillor Orléans Ward - Conseiller municipal du quartier Orléans

Orléans Ward - Proud to be home to 50,000 residents Quartier Orléans - Fier de compter plus de 50,000 résidents

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: April-30-15 12:31 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: ELRT/Hwy174-CR17 EA Study comments received

Hi [REDACTED], I am currently reviewing comments received at the Open Houses and am working on providing responses. I received your comment regarding noise barriers as well as concerns raised on previous questions that have not been answered. If you would provide these questions, I would be happy to provide a response.

With respect to noise barriers, the City's Environmental Noise Control Guidelines is used to assess the noise for transportation projects. Under these guidelines, and where noise barriers are warranted, properties with outdoor living areas (generally backyards) adjacent to a road corridor would qualify for a noise barrier. Since your home at [REDACTED] fronts onto Hwy174, your outdoor living area/backyard is shielded from the highway noise by your home, and therefore would not qualify for noise barriers.

From: [REDACTED]
Sent: Sunday, May 24, 2015 2:32 PM
To: Taylor, Angela
Cc: [REDACTED]
Subject: RE: ELRT/Hwy174-CR17 EA Study comments received

Hi Angela

In response to your email our outdoor living consist of our backyard and probably as much on our dock on the east side of our property. In the summer we spend as much of our time down at the dock. Which due to the alignment of the road we are not being sheltered here by a house. With respect to the rock wall, the escarpment will be modified with more of a vertical feature when completed to accommodate the extra lanes to the south. This wall proves to be a feature that bounces all the noise back out towards our direction. Although there now existing rock wall, much of the existing escarpment that will need to be excavated is rounded in shape and is covered with vegetation such as grass and bush, once that is removed it will result in a substantially longer and vertical faced rock face that will resonate more noise back to our outdoor living. Please review this matter and get back to me.
Thank you.

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Monday, May 25, 2015 8:38 AM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: ELRT/Hwy174-CR17 EA Study comments received

Hi Mike, according to the City's Environmental Noise Control Guidelines, the sound level calculation in the outdoor living area is based on 3m from the dwelling unit at a vertical height of 1.5m above the existing corridor surface. I attach a link to the guidelines for reference. All other areas are outside of the noise control guidelines.

<http://ottawa.ca/en/city-hall/planning-and-development/community-plans-and-design-guidelines/design-and-planning-0-1-1-4#o>

From: [REDACTED]
Sent: Thursday, June 04, 2015 2:04 PM
To: [REDACTED] (OPP); [REDACTED]
Cc: 'Taylor, Angela'; [REDACTED]
Subject: 174-17 notes of meeting at OPP Rockland

[REDACTED]
Please let us know if I have missed any points. Thanks for taking the time to meet with us yesterday.

Minutes of Meeting

Date of Meeting	June 3, 2015	Start Time	1:00	Project Number	60270243
Project Name	OR 174-CR17 EA Study – Split to Rockland				
Location	OPP Rockland Detachment				
Regarding	174-17 Functional Design Meeting				
Attendees	Peter Di Rinaldo (PD), Marc Clermont (MC), Valerie McGirr (VM)				
Distribution	Attendees, Project Team				
Minutes Prepared By	VM				

PLEASE NOTE: If this report does not agree with your records of the meeting, or if there are any omissions, please advise, otherwise we will assume the contents to be correct.

	Action
1. Introductions and EA Process	
<ul style="list-style-type: none"> The purpose of the meeting is to obtain comment from the OPP responsible for CR 17 in the study area from the City of Ottawa boundary to Landry Road. MC noted that this study is in Phase 4 of the Municipal Class EA process. Previously in the study, the need was confirmed, alternatives were developed and evaluated and now the recommended plan is being refined. Later, study documentation will be provided for public review. 	
2. Functional Design	
VM presented the plans for 174-17 east of Cumberland. Comments discussed include:	
<ul style="list-style-type: none"> PD expressed support for. <ul style="list-style-type: none"> Using roundabouts where these are feasible. Eliminating side road and driveway accesses where possible through consolidation and alternative connections. Adding an EB left turn lane at the arena access (MC noted that this was supposed to be built by the developer but this did not get completed). Adding turn lanes at intersections in general to allow turning traffic to move out of the through lane. Improving the Voisine intersection. In his experience, the frequency and number of access points is an important safety issue. PD supports a four lane road but expressed a desire for an alternative route to CR 17. MC described the evaluation process used to select the widening of 174-17 as the preferred including future travel patterns and transit. Changes shown to the OPP detachment parking access and service road are fine. With respect to setting speed limits, PD recommended that they be logical and that transitions in speed limits be clear. Speed limits should not increase and decrease frequently and should be consistent with the intercity nature of the road and adjacent land use. The location and number of turnarounds (at intersection median breaks) are adequate for emergency response. The paved shoulder will help police during heavy traffic in emergencies. 	

	Action
<ul style="list-style-type: none"> At Pouliotte, there is the potential for a new police building to be located south of CR 17. If this happens, a one-way northbound connection is suggested to link the police building with CR 17, forming the fourth leg of the Potvin driveway median break. Vehicles going to the police building would enter via Edwards Street so no provision for southbound travel is required. 	
<ul style="list-style-type: none"> East of the Caron Extension, PD would like to see the number of intersections with CR 17 reduced and access provided via an internal road network. For example, the arena access could be closed with access provided via both the Caron extension/ Industrielle Road and the new access to the Regional Group development. 	
<ul style="list-style-type: none"> PD noted that there is a blind spot on westbound CR 17 on the curve east of Laurier East. The sight distance is deceptive. 	
<ul style="list-style-type: none"> PD asked about the timing of four-laning. MC noted that this work is expected to be many years in the future, depending on funding and priorities. In the City of Ottawa, it is identified as part of the ultimate transportation network but is not included in the Affordable Plan. 	
<ul style="list-style-type: none"> VM will make change to the functional design, adding RT lanes and u-turn lanes where they are not currently shown. 	VM

From: [REDACTED]
Sent: Wednesday, May 20, 2015 9:57 AM
To: Taylor, Angela
Subject: 174/CR17 Widening

Hello Ms Taylor

This email is regarding the proposed widening of the 174/CR17.
I apologize I was unable to attend the open houses in April 2015, so I may not have the full context of the proposals presented.
I was disappointed to see the choice of the 174/CR17, though I appreciate the work and context that went into the decision.

I would like to enquire about the decision to alter current exiting of the 174 on to Kinsella drive and if modification might still be considered.
Currently there is a small right exit lane off the 174 eastbound to Rockland that goes to Kisella. I was pleased to see that a traffic signal would be installed during the widening development at this intersection, but perplexed that the right exit lane would not be retained.
It would appear that there is unused land between Old Montreal and the 174 that could be used. For vehicles travelling at 90 or 100 km (I didn't see what the speed limit would be on the new 174) it would be particularly dangerous to right exit at 10-20 km, with a slight down-hill approach onto Kinsella.
As there isn't a right-side exit on the 174 past Barnett Extension, the traffic speed would be at full.
Traffic would either back up for commuters turning right or be prone to accident, especially in the winter during slippery conditions.
This would serve a large number of houses Kinsella, Royal Orchard, Quillivan, Pierrette, Willhaven etc area with very minimal costs of additional paving.

I hope this option is considered.



Regards

[REDACTED]

PS I would also suggest this for Cameron street though no exit lane there currently exists rightside eastbound.

From: Taylor, Angela
Sent: Friday, May 22, 2015 3:39 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: 174/CR17 Widening

Dear [REDACTED] thank you for your suggestion of an eastbound right turning lane into Kinsella. We have reviewed this proposal and will be incorporating it into the design to improve safety. Once we have updated our plan, I will forward you this segment of the road widening design with the right turn lane.

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Wednesday, May 27, 2015 3:45 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: FW: 174/CR17 Widening

Dear [REDACTED] as requested, we have incorporated turning lanes at the intersection of Highway 174 and Old Montreal Road/Kinsella Drive as part of the road

widening design. Attached is a an image of the intersection design. If you have any further questions or concerns, please don't hesitate to contact me.



From: [REDACTED]
Sent: Thursday, May 28, 2015 9:33 AM
To: Taylor, Angela
Cc: [REDACTED]
Subject: RE: 174/CR17 Widening

Hello Angela

Thank you very much for considering the request for the change. I'm not sure if it was previously planned and just not shown on the original open house drawings, or if the changes were to be in the future details, but I greatly appreciate your work to likely incorporating it into the final design. I also note that it appears as though the turning lane has been extended from about 150 meters to I'm guessing about 250m which will allow for safer deceleration. Thank you for this improvement to safer roads in Ottawa.

A final comment I would have is that Old Montreal Road is about 1 meter lower in elevation than the 174. Would there be a change of debris or rocks coming from a plow on the 174 striking a vehicle on Old Montreal given the proximity of the roads and lower elevation? Is there a barrier that might be consider that might reduce this potential?

Thank you again for the work put into the study and the proposals.

Regards

From: 'McGirr, Valerie'
Sent: Thursday, May 28, 2015 9:33 AM
To: [REDACTED]
Cc: Taylor, Angela <Angela.Taylor@ottawa.ca>; [REDACTED]
Subject: OR 174 at Old Montreal West/Kinsella

Hi [REDACTED]

A retaining wall with a barrier on top will be required. As you point out, Old Montreal is lower than OR 174 at this location and with the widening of the road, there is insufficient room for a slope, necessitating construction of a retaining wall, which is shown on our drawing. A vertical drop adjacent to the shoulder requires protection. The selection of the retaining wall and barrier design will be completed in the future during detail design.
Hope this helps.

From: [REDACTED]
Sent: Tuesday, June 23, 2015 3:02 PM
To: Taylor, Angela
Subject: STAGE 2 LRT
Importance: High

Hi Angela,

I received an update via email but land expropriation is not clear to me. I am most interested to know if land will be expropriated along highway 174 on the NORTH side of the road as my property backs onto this part of the highway.

Many thanks,

From: Taylor, Angela
Sent: Tuesday, June 23, 2015 3:17 PM
To: [REDACTED]
Subject: RE: STAGE 2 LRT

Hello [REDACTED] the Confederation Line East LRT Extension as well as the Highway 174 widening will be constructed within the Highway 174 road corridor and no property is required. To reassure you, I would be happy to provide you with a plan view of our functional design for both the highway widening and LRT in your area if you provide me with your address. Please let me know if you require anything further.

From: [REDACTED]
Sent: Tuesday, June 23, 2015 3:22 PM
To: Taylor, Angela
Subject: East bound train

Hello

Thank you for putting a train in my backyard. I now have to sell my house. It is already so noisy in my backyard that we cannot sit outside. Now with a train it will be like living on the tracks.

Thank you City of Ottawa. If you thought about it you should of put it on the other side of the 174.

From: Taylor, Angela
Sent: Thursday, June 25, 2015 11:58 AM
To: [REDACTED]
Subject: RE: East bound train

Hello [REDACTED] the Confederation Line East LRT Extension to Orleans will be located in the median, which is the space between the eastbound and westbound lanes of Highway 174 and not directly behind the homes along Du Bois Avenue. The LRT vehicles are electrically powered and much quieter than the diesel powered busses currently in operation and will have minimal impact on noise levels. To date, we are completing our noise study and noted that in some areas, the existing highway noise levels warrant sound barriers and these locations are being finalized. Where warranted, sound barriers will be constructed with the LRT implementation.

Once our noise study is complete, we would be in a position to provide you with specific information on sound barriers in your area. If you wish to receive further information, please check back with us in a few weeks.

Appendix D

Summary Report for April 2015 Consultation

Frequently Asked Questions

Frequently Asked Questions based on April 2015 consultation events:

Question	Answer
General	
Why are we studying the widening of Highway 174, CR 17 and the Eastern LRT?	The 2013 Transportation Master Plan (TMP) has identified a shortfall in transportation capacity for residents of the City of Ottawa and the United Counties of Prescott-Russell, primarily to access jobs and activities in the central area of Ottawa. In addition to providing additional transit capacity to meet the objectives of the TMP, additional road capacity is also required. Through the corridor the transportation model indicates that one additional lane of road capacity is needed in each direction. This could be either a new two-lane roadway, or a widening of the existing roadway. The model also indicates that the majority of these trips want to access Highway 174 at Trim Road and Highway 417 west of the split. Road safety is another concern on the existing road. The additional lanes will allow for many of the safety issues to be addressed. Dividing the highway to prevent vehicles from crossing over the centre line and finding ways to consolidate driveway accesses will improve safety.
Why was the 174/17 corridor selected over the others considered?	There is less travel demand across the City by a southern route than travel demand to the downtown area and areas close to Highway 417 via Highway 174 and Highway 417. Routes that connect south of Highway 174 along Trim Road will require additional capacity to manage the downstream traffic volumes. Development continues in the east end of the City of Ottawa and in counties outside the City, placing pressure on the transportation network to accommodate more trips. West of Trim Road, the implementation of LRT from Blair Station east to Place d'Orléans (and eventually to Trim Road) will provide additional travel capacity and connection to a widened 174/17 will help to improve ridership.
What is the timeline for the LRT extension and the widening of Highway 174 and CR 17?	The Confederation Line East LRT Extension is a Stage 2 project planned for 2023, subject to funding availability. The widening of 174/17 is not currently identified in the City of Ottawa's Affordable Plan to 2031 and will be dependent on funding availability
Eastern Light Rail Transit	
Will noise be an issue?	The LRT will have a continuously welded track, is electrically driven, and will be generally quiet. The change in road proximity and traffic volumes will affect noise levels. Impacts are being investigated in greater detail, along the full length of the corridor and mitigation measures will be provided where warranted.

Question	Ans	WER
How were the station sizes, design and locations determined		Stations are located based on a number of criteria including connection to north-south bus routes (at arterial road crossings of the corridor), serving existing and projected ridership, and a spacing that maximizes the number of people who can access the station within a 600 metre walk zone. There will be an emphasis on connectivity, accessibility and personal safety by designing stations that provide good connections to local bus routes, sidewalks, pedestrian pathways, and bicycle facilities. Stations will be fully accessible to passengers with disabilities through the use of elevators, logical arrangement of station elements and clear, concise signage. Creating logical stations with clear sight lines assist all members of the community by creating safer places.
How will this system be paid for?		The capital cost to build the system will be part of the City's Stage 2 LRT implementation and the City will be seeking funding partners with upper levels of government. On-going operating costs, including increases or decreases in service levels to meet ridership demands, will be part of OC Transpo's on-going budgetary process.
Why not extend the LRT to Trim Road and beyond?		Implementation of the LRT is based on the City of Ottawa's Affordable Plan to 2031 as well as projected ridership demand. The City's TMP currently identifies LRT implementation terminating at Place D'Orléans before 2031 with the extension to Trim Road post 2031. Beyond Trim Road, there is insufficient ridership to extend the LRT any further. We are protecting for a bus rapid transit corridor east of Trim to Frank Kenny to connect Eastern communities with the LRT.
How will the transit system operate when the LRT opens?		LRT trains will be completely segregated from other traffic, allowing them to move on a regular and predictable timetable, making trips fast and reliable. The bus network currently has a set of local, semi-express and express routes designed to take passengers downtown in the morning (and home in the evening) and serve the major node at Place d'Orléans during the midday and evening. The LRT will require a reorganization of the route network, including: <ul style="list-style-type: none"> • Bus routes will take peak period passengers to the nearest LRT station • Few buses will travel across the greenbelt and further west • Enhanced local service will meet increasing demand • Trains will stop at all stations allowing for passenger transfer • While bus routes will not be timed to the arrival or departure of trains these connections will be as direct as possible with climate-controlled areas CRT buses from Clarence-Rockland will have the ability to connect to the easternmost LRT station, allowing passengers to transfer to the LRT to travel into downtown
Where will there be Park and Ride or Kiss and Ride facilities?		Currently there are two Park and Ride locations at Trim Road and at Place d'Orléans. These will be maintained and expanded if possible. Kiss and Ride in some form will be considered at every station.

Question	Ans	WER
Widening of Highway 174 / County Road 17 Will the new highway lanes be for all drivers or only vehicles with more than one person in them?		The environmental assessment will identify the impacts of the largest footprint. This will include the additional lane width and buffer width that is needed to protect for High Occupancy Vehicle (HOV) lanes for vehicles with more than one person. If the City decides to make the lanes available to all drivers, the protected right-of-way will be wide enough and will not need additional approvals.
Won't the arterial roads that connect to the highway become overloaded if the highway is widened?		Our modelling work indicates that the arterial roads can adequately handle the additional traffic moving to and from the widened highway, although the level of congestion is likely to increase as travel demand increases
How is the widened highway being designed to make travel as safe as possible?		Current design standards are being applied. With the LRT being placed in the median (from Montreal Road to the eastern terminus), the road traffic will be separated from the trains by concrete barriers and safety fences. Visibility, design speed, lighting and safety measures are being designed into the plans.
Was a southern ring road considered		A ring road has been investigated by both the MTO and the City over the years and modelling work indicates that it would be poorly used. A ring road would be very expensive and currently neither the City nor MTO are carrying it forward for development
Was a three-lane highway with reversible lanes considered?		The City conducted a review and assessment of whether reversible lanes would be suitable in this context. Safety and cost considerations indicate that a reversible lane is not appropriate along Highway 174 and County Road 17. The costs of putting overhead signage that indicate the direction of traffic in the centre lane would be expensive. Furthermore, for safety reasons, we need a barrier between lanes of different directions. Using reversible lanes over long distances, with driveway accesses and at highway speeds is not a feasible option.
Is more development being allowed in the corridor?		Growth continues across the Region, however the City has placed a moratorium on lot severances and new access points to Highway 174. Existing lots that are not yet developed may be allowed a new access, but wherever feasible the City's preference is to consolidate these potential new entrances with existing entrances.
		A study similar to the Greater Toronto Area's "Places to Grow" is being considered for Eastern Ontario, which would look at where development should occur. There is a lot of good agricultural land in this area, and if we build a road, there would be pressure to develop the surrounding areas and we could lose this agricultural land.
Will property by expropriated to widen the 174/17?		There are many places along the corridor where we have sufficient right-of-way. For those portions where property is required to expand the right-of-way, then we will have a discussion with those residents who will be impacted.

Appendix E

Summary Report for April 2015 Consultation

- Presentations
- Display Material

- PRESENTATIONS

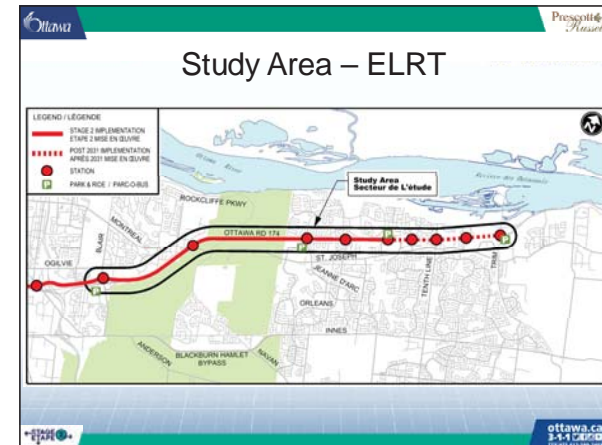
**Confederation Line East LRT
Extension & Hwy 174/17**

Environmental
Assessment Studies

STAGE 2

Consultation April 2015

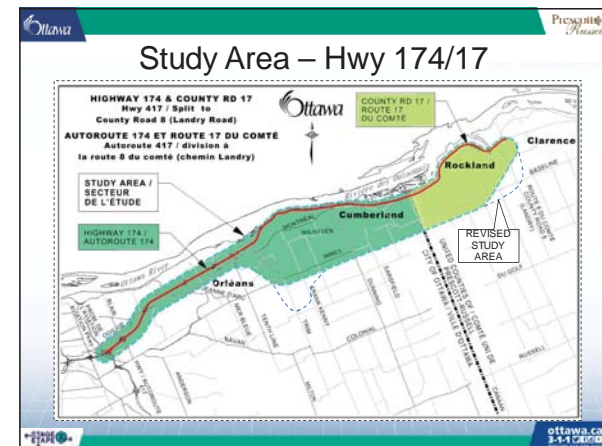
ottawa.ca
3-1-1



Presentation Overview

- Introduction and recap
- ELRT and Hwy 174 West of Trim
 - Comments received and actions taken
 - Functional design
 - Potential implementation staging (terminus and stations)
 - Impacts and mitigation measures
- Hwy 174-17 East of Trim
 - Comments received and actions taken
 - Functional design
 - Impacts and mitigation measures
- Next steps

ottawa.ca
3-1-1



Introduction and Recap

In February we reviewed:

- Integration of Municipal Class EA and Transit Project Assessment Process
- Alternative Routes/Designs and Technically Preferred Alternatives
- Potential impacts

West of Trim – Preferred alternative:

- LRT on north side from Blair to Montreal, transitioning to the median east of Montreal
- 174 widening protecting for a high occupancy vehicle (HOV) lane

East of Trim - Preferred road corridor:

- Widening of existing 174/17 with pedestrian and cycling facilities
- Need for speed management measures, u-turn possibilities

ottawa.ca
3-1-1 OTTAWA


East of Trim: Comments/Action

Comments received – actions taken:

- Concern for property impacts – impacts reduced where possible
- Concern for traffic congestion and high speeds – speed management included in design
- Concern for access to waterfront and road crossing – controlled pedestrian crossing opportunities included in design
- Concern for impact on the Ottawa river – alignment avoids filling into the river
- Concern for noise impacts – where warranted, noise barriers considered
- Concern for timeframe of 174/17 widening beyond 2031 – limited by affordability


East of Trim

- Functional design alternatives
- Functional design of preferred plan



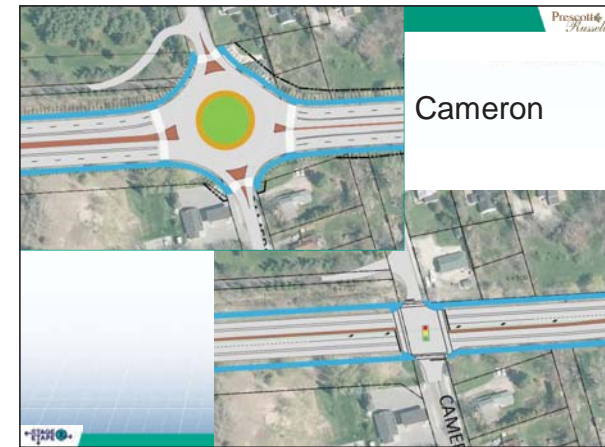
ottawa.ca
3-1-1 OTTAWA

East of Trim - Typical Rural Cross Section



Design considerations:

- Median barrier to improve safety (type to be determined in detail design)
- Median breaks
- Enhanced paved shoulders
- Multi-use pathway away from the roadway where possible



Analysis of Signals and Roundabouts

- Operations:
 - 2-lane roundabout can handle 1700-1800 veh/h (2100 veh/h is the projected 2031 volume). Therefore 3-lane roundabout is required

	Advantages	Trade-offs
Roundabouts	Access management/ Manoeuvrability	More property required
	Some speed management	Unbalanced vehicle demand results in delays during peak
		Less convenient for pedestrians and cyclists
		Requires pedestrian-actuated signals to cross 174
Signals	Better operational performance	Requires urban design elements to manage speed
	Less property required	
	Convenient for pedestrians and cyclists	



Ottawa *Prescriptive Russell*

East of Trim – Preferred Plan


Show roll plan

ottawa.ca
3-1-1 2260

Ottawa *Prescriptive Russell*

West of Trim

- Functional design alternatives
- Preliminary preferred plan



ottawa.ca
3-1-1 2260

Ottawa *Prescriptive Russell*

West of Trim: Comments/Action

Comments received – actions taken:

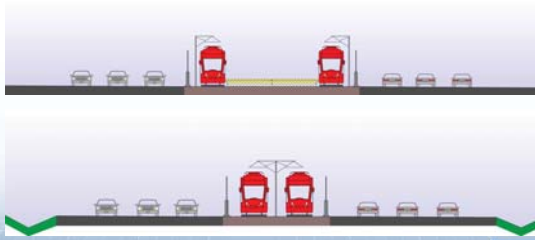
- LRT should extend to Trim station immediately and defer building some stations – **limited by affordability**
- Need to expand the park and rides – **design based on need**
- Concern for noise and vibration impacts – **mitigation considered where warranted**
- Concern about pedestrian and cycling access to stations – **stations designed for full accessibility**
- Concern about personal safety at stations – **design with personal safety considerations**
- Concern for timeframe of 174 widening beyond 2031 – **limited by affordability**

ottawa.ca
3-1-1 2260

Ottawa *Prescriptive Russell*

West of Trim – 174 and LRT Median Station

- Median station and runningway with road widening



ottawa.ca
3-1-1 2260

Ottawa

Prescott & Russell

West of Trim – Preferred Plan

Present roll plan

ottawa.ca
3-1-1

Ottawa

Prescott & Russell

Example of a Median Station



Copyright 2014, Ottawa Transit Commission. All rights reserved.

ottawa.ca
3-1-1

Ottawa

Prescott & Russell

Example of a Median Station



ottawa.ca
3-1-1

Ottawa

Prescott & Russell

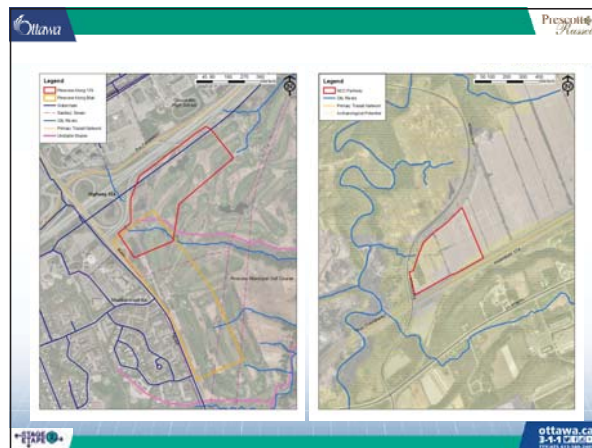
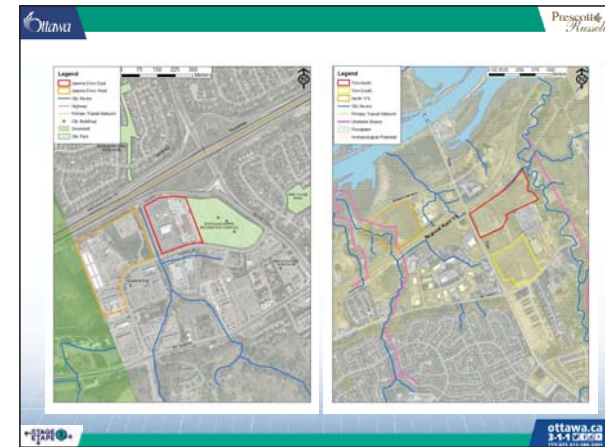
Example of a Median Station



ottawa.ca
3-1-1

Maintenance and Storage Facility

- Additional train storage and maintenance capacity needed for Stage 2
- Several locations identified in the East LRT study area
- Evaluation underway
 - Preferred location likely in the Western LRT Corridor to balance operational needs
 - Summary will be posted before review



Next Steps

- Based on feedback received:
 - Confirm/refine Hwy 174-ELRT functional design west of Trim
 - Confirm/refine Hwy 174-17 functional design refinements east of Trim
- Finalize Functional Designs
- Finalize mitigation measures
- West of Trim – June/July presentation to Ottawa Transportation Committee
- East of Trim – Fall presentation to Committees and Councils
- Public review of study documentation



Thank You

For project updates and Open House information:

www.prescott-russell.on.ca/en/public-works/major-projects
ottawa.ca/easternlt
ottawa.ca/hwy174and17study
ottawa.ca/stage2




Prolongement vers l'est de la Ligne de la Confédération du TLR et 174/17



Études d'évaluation
environnementale

Consultation avril 2015

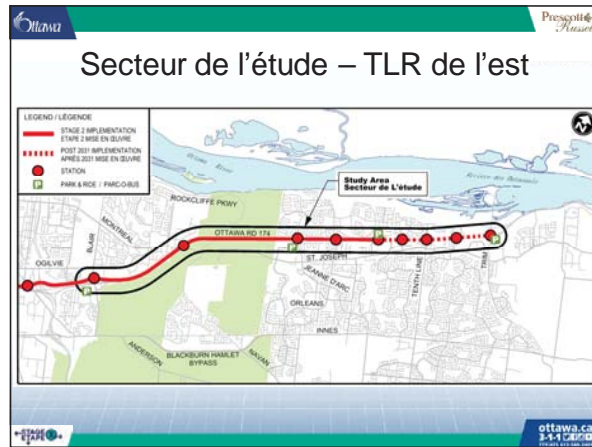
ottawa.ca
 3-1-1 3-1-1 3-1-1



Survol de la présentation

- Introduction et récapitulation
- TLR de l'est et route 174 dans le secteur à l'ouest du chemin Trim
 - Commentaires reçus et mesures prises
 - Conception fonctionnelle
 - Étapes potentielles de mise en œuvre (terminus et stations)
 - Répercussions et mesures d'atténuation
- Routes 174 et 17 dans le secteur à l'est du chemin Trim
 - Commentaires reçus et mesures prises
 - Conception fonctionnelle
 - Répercussions et mesures d'atténuation
- Prochaines étapes

ottawa.ca
 3-1-1 3-1-1 3-1-1



Introduction et récapitulation

En février, nous avons révisé :

- L'intégration du processus d'évaluation environnementale municipale de portée générale et du processus d'évaluation du projet de transport en commun
- Les tracés et les conceptions de rechange, et les solutions techniques privilégiées
- Les répercussions potentielles

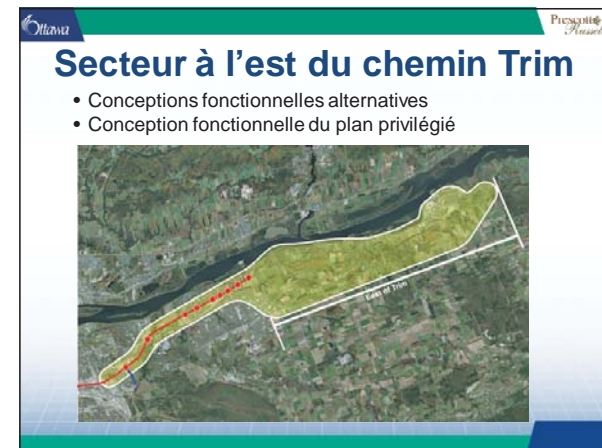
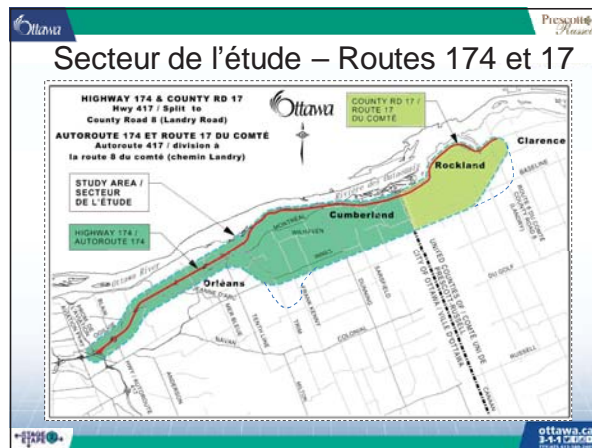
Secteur à l'ouest du chemin Trim – solution privilégiée :

- Le TLR du côté nord de Blair jusqu'au chemin Montréal, empruntant la voie médiane à l'est du chemin Montréal
- L'élargissement de la route 174 assurant la sécurité de la voie pour les véhicules à taux d'occupation élevée (VTOE)

Secteur à l'est du chemin Trim – couloir routier privilégié :

- L'élargissement des routes 174 et 17 et l'aménagement d'infrastructures pour piétons et cyclistes
- Le besoin de mesures de gestion de vitesse et de possibilités de demi-tours

ottawa.ca 3-1-1




Secteur à l'est du chemin Trim : commentaires et mesures

Commentaires reçus – mesures prises :

- Craintes des conséquences sur les propriétés – réduction des conséquences, lorsque possible.
- Craintes liées à la congestion de la circulation et à la vitesse élevée – mesures de gestion de vitesse prévues dans la conception.
- Craintes liées à l'accès aux berges et aux passages routiers – passages pour piétons commandés prévus dans la conception.
- Craintes des conséquences sur la rivière des Outaouais – le tracé choisi évite le déversement de remblai dans la rivière.
- Craintes liées au bruit – lorsque nécessaire, l'installation d'écrans antibruit est considérée.
- Craintes que le calendrier de l'élargissement des routes 174 et 17 s'étire au-delà de 2031 – limité pour cause d'abordabilité


Secteur à l'est du chemin Trim – section transversale rurale type



Considérations liées à la conception :

- Terre-plein central pour améliorer la sécurité (type à être déterminé dans le plan détaillé)
- Ouvertures du terre-plein central uniquement à des endroits contrôlés
- Accotements asphaltés améliorés
- Sentier polyvalent à distance de la route, lorsque possible

Secteur à l'est du chemin Trim – sections

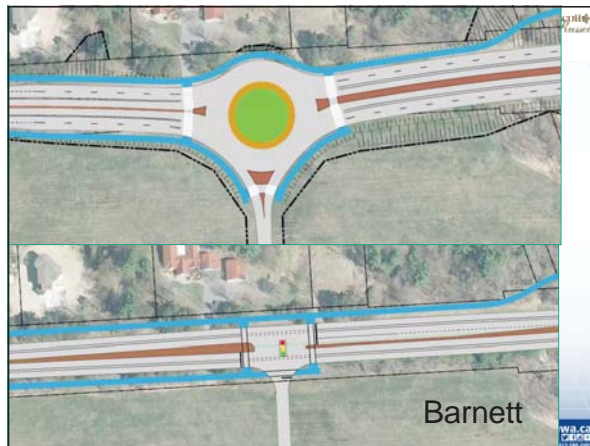
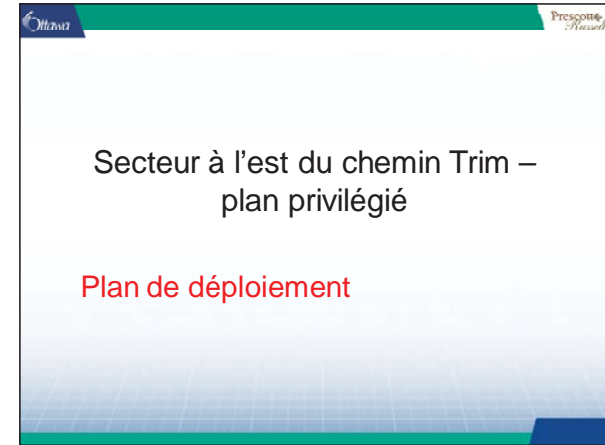
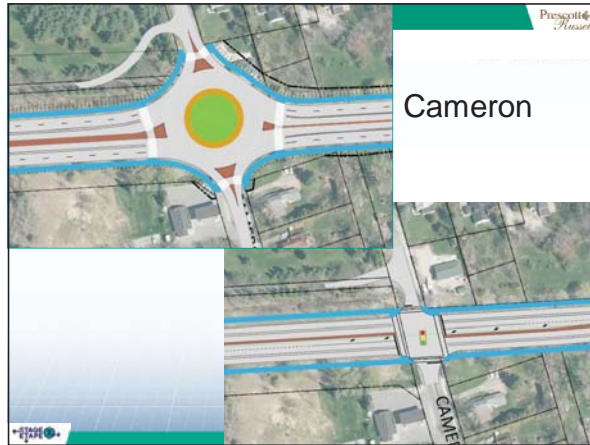


Analyse des feux de circulation et du carrefour giratoire

• Operations :

- Un carrefour giratoire à 2 voies peut gérer 1700-1800 véh/h (le volume projeté est de 2100 véh/h). Un carrefour giratoire à trois voies est nécessaire

	Avantages	Compromis
Carrefours giratoires	Gestion de l'accès/manœuvrabilité	Nécessite plus de terrain
	Certaine gestion de la vitesse	La circulation débalancée donne lieu à des délais lors des heures de pointe
		Moins pratique pour les piétons et cyclistes
		Requiert des signaux de passage piétonnier activés par les piétons afin de traverser la 174 Démesuré dans contexte villageois
Feux de circulation	Performance opérationnelle améliorée	Requiert des mesures d'atténuation de la circulation afin de gérer la vitesse
	Moins de terrain est requis	
	Pratique pour les piétons et cyclistes	



Secteur à l'ouest du chemin Trim: commentaires et mesures

Commentaires reçus – mesures prises :


- Le TLR devrait être immédiatement étendu à la station Trim et la construction de certaines stations devrait être reportée – **limité pour cause d'abordabilité.**
- Besoin d'augmenter les parcs-o-bus – **conception fondée sur les besoins.**
- Craintes liées au bruit et à la vibration – **mesures d'atténuation considérées lorsque nécessaires.**
- Craintes liées à l'accès des piétons et des cyclistes aux stations – les stations sont conçues pour être entièrement accessibles.
- Craintes pour la sécurité des personnes à l'intérieur des stations – les stations sont conçues dans le souci de la sécurité des personnes.
- Craintes que le calendrier des travaux d'élargissement de la route 174 s'étire au-delà de 2031 – **limité pour cause d'abordabilité.**

Secteur à l'ouest du chemin Trim – Plan privilégié

Plan de déploiement actuel

Secteur à l'ouest du chemin Trim – route 174 et station du TLR sur la voie médiane

- Station et voie de roulement sur la voie médiane et élargissement de la route



Exemple de station sur la voie médiane



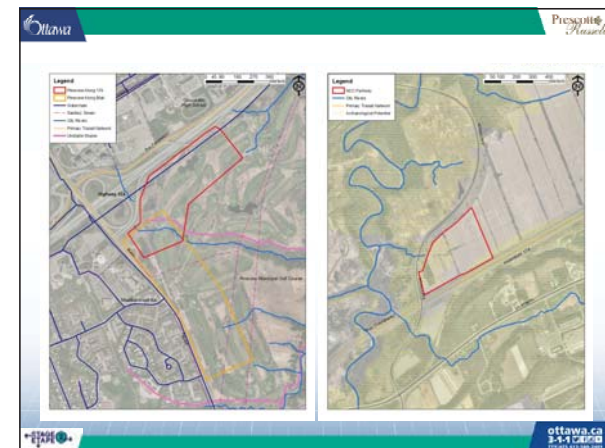


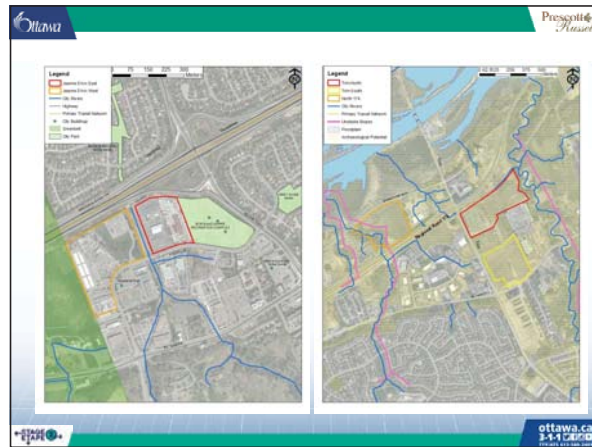
Ottawa *Prescott & Russell*

Installation d'entretien et de remisage

- La capacité d'entretien et de remisage des trains devra être accrue pour la phase 2.
- Plusieurs emplacements ont été répertoriés dans le secteur de l'étude du TLR de l'est.
- Évaluation en cours
 - L'emplacement privilégié sera sans doute dans le couloir du TLR de l'ouest afin de concilier les besoins opérationnels.
 - Le sommaire sera affiché avant l'examen.

ottawa.ca
3-1-1





Merci

Pour obtenir les mises à jour sur le projet et la documentation des réunions portes ouvertes :

www.prescott-russell.on.ca/fr/travaux-publics/projets-denvergure

ottawa.ca/TLRest

ottawa.ca/etuderoutes174-17

ottawa.ca/etape2

Prochaines étapes

- En fonction des commentaires reçus :
 - Confirmer/améliorer la conception fonctionnelle de la route 174 et du TLR de l'est dans le secteur à l'ouest du chemin Trim
 - Confirmer/améliorer la conception fonctionnelle des routes 174 et 17 dans le secteur à l'est du chemin Trim
- Mettre la dernière main aux conceptions fonctionnelles
- Mettre la dernière main aux mesures d'atténuation
- Secteur à l'ouest du chemin Trim – présentation en juin/juillet au Comité des transports d'Ottawa
- Secteur à l'est du chemin Trim – présentation à l'automne aux comités et conseils d'administration
- Examen public des documents afférents à l'étude

AECOM

United Counties of Prescott and Russell and
City of Ottawa

Summary Report for April 2015 Consultation
Confederation Line East LRT Extension/ Highway 174/
Prescott Russell County Road 17 Widening

- **DISPLAY MATERIAL**

Display Boards consisted of Functional Design Drawings for:

Eastern LRT Hybrid Alignment with HOV Widening

Hwy 174 Widening – Trim to Meadow Lane

Hwy 174 Widening – Quigley Hill to Cumberland Village

Hwy 174 Widening – Cumberland Roundabouts and Intersections

Hwy 174 Widening – Cumberland East to Canaan Road

Hwy 174 Widening – East of Canaan Road to East of Edwards

Hwy 174 Widening – East of Edwards to Landry

Copies of the display boards can be found here:

<http://ottawa.ca/en/city-hall/public-consultations/transit/open-houses-april-20-21-and-23-2015>

Public Open House #3 Text:

Open Houses - April 20, 21 and 23, 2015

Monday, April 20, 2015, Orléans
Tuesday, April 21, 2015, Cumberland
Thursday, April 23, 2015, Clarence Creek

Open House Display Boards

General Questions

Why are we studying the widening of Highway 174, CR 17 and the Eastern LRT?

The 2013 Transportation Master Plan (TMP) has identified a shortfall in transportation capacity for residents of the City of Ottawa and the United Counties of Prescott-Russell, primarily to access jobs and activities in the central area of Ottawa. In addition to providing additional transit capacity to meet the objectives of the TMP, additional road capacity is also required.

Through the corridor the transportation model indicates that one additional lane of road capacity is needed in each direction. This could be either a new two-lane roadway, or a widening of the existing roadway. The model also indicates that the majority of these trips want to access Highway 174 at Trim Road and Highway 417 west of the split.

Road safety is another concern on the existing road. Adding the additional lanes will allow for many of the safety issues to be addressed. Dividing the highway to prevent vehicles from crossing over the centre line and finding ways to consolidate driveway accesses will improve safety.

Why was the 174/17 corridor selected over the others considered?

There is less travel demand across the City by a southern route than travel demand to the downtown area and areas close to Highway 417 via Highway 174 and Highway 417. Routes that connect south of Highway 174 along Trim Road will require additional capacity to manage the downstream traffic volumes. Development continues in the east end of the City of Ottawa and in counties outside the City, placing pressure on the transportation network to accommodate more trips.

West of Trim Road, the implementation of LRT from Blair Station east to Place d'Orléans (and eventually to Trim Road) will provide additional travel capacity and connection to a widened 174/17 will help to improve ridership.

What is the timeline for the LRT extension and the widening of Highway 174 and CR 17?

The Confederation Line East LRT Extension is a Stage 2 project planned for 2023, subject to funding availability. The widening of 174/17 is not currently identified in the City of Ottawa's Affordable Plan to 2031 and will be dependant on funding availability.

Will noise be an issue?

The LRT will have a continuously welded track, is electrically driven, and will be generally quiet. The change in road proximity and traffic volumes will affect noise levels. Impacts are being investigated in greater detail, along the full length of the corridor and mitigation measures will be provided where warranted.

Eastern Light Rail Transit

How were the station sizes, design and locations determined?

Stations are located based on a number of criteria including connection to north-south bus routes (at arterial road crossings of the corridor), serving existing and projected ridership, and a spacing that maximizes the number of people who can access the station within a 600 metre walk zone.

There will be an emphasis on connectivity, accessibility and personal safety by designing stations that provide good connections to local bus routes, sidewalks, pedestrian pathways, and bicycle facilities. Stations will be fully accessible to passengers with disabilities through the use of elevators, logical arrangement of station elements and clear, concise signage. Creating logical stations with clear sight lines assist all members of the community by creating safer places.

How will this system be paid for?

The capital cost to build the system will be part of the City's Stage 2 LRT implementation and the City will be seeking funding partners with upper levels of government. On-going operating costs, including increases or decreases in service levels to meet ridership demands, will be part of OC Transpo's on-going budgetary process.

Why not extend the LRT to Trim Road and beyond?

Implementation of the LRT is based on the City of Ottawa's Affordable Plan to 2031 as well as projected ridership demand. The City's TMP currently identifies LRT implementation terminating at Place D'Orleans before 2031 with the extension to Trim Road post 2031. Beyond Trim Road, there is insufficient ridership to extend the LRT any further. We are protecting for a bus rapid transit corridor east of Trim to Frank Kenny to connect Eastern communities with the LRT.

How will the transit system operate when the LRT opens?

LRT trains will be completely segregated from other traffic, allowing them to move on a regular and predictable timetable, making trips fast and reliable.

The bus network currently has a set of local, semi-express and express routes designed to take passengers downtown in the morning (and home in the evening) and serve the major node at Place d'Orléans during the midday and evening. The LRT will require a reorganization of the route network, including:

- Bus routes will take peak period passengers to the nearest LRT station
- Few buses will travel across the greenbelt and further west
- Enhanced local service will meet increasing demand
- Trains will stop at all stations allowing for passenger transfer
- While bus routes will not be timed to the arrival or departure of trains these connections will be as direct as possible with climate-controlled areas

CRT buses from Clarence-Rockland will have the ability to connect to the easternmost LRT station, allowing passengers to transfer to the LRT to travel into downtown.

Where will there be Park and Ride or Kiss and Ride facilities?

Currently there are two Park and Ride locations at Trim Road and at Place d'Orleans. These will be maintained and expanded if possible. Kiss and Ride in some form will be considered at every station.

Confederation Line Eastern LRT

Blair Station

From Blair Station, the LRT will extend directly east and pass underneath Blair Road and the Blair interchange ramps, staying along the north side of the highway. Once under all of the ramps, the LRT comes back up to grade and runs at about the same elevation as Highway 174 (Hwy174) along the north side. A station adjacent to Gloucester High School will be protected for the future (post 2031). A continuous pathway on the north side of the LRT is proposed.

For the Hwy174 road widening to Blair Road, the design will match the lanes constructed as part of the Ontario Ministry of Transportation widening of Hwy 417 from Nicholas to Hwy174. From Blair Road easterly, Hwy174 will be three lanes in each direction with paved shoulders. The additional lane will be designed for a high occupancy vehicle lane in each direction. Where the existing median is narrower than current standards, it will be widened.

Montreal Road

The LRT continues along following the existing grade of the highway until Montreal Road. The LRT will rise slightly where Montreal Road crosses under the alignment and the highway. Ramps will be modified at the Montreal Road interchange to allow for the LRT to pass on an elevated structure across the northern half of the interchange. The station will be in the northwest quadrant with the stairs and elevators concentrated at the end of the platform connecting down to a lower level concourse which will tie into the west (or south) side of Montreal Road. The structures carrying OR 174 over Montreal Road will be replaced when Hwy174 is widened.

East of Montreal Road Station the land drops down into the Green's Creek valley. The LRT stays high and climbs up to span across the westbound lanes to transition into the median of the highway. The LRT will then drop down to grade as we move towards the Sir George-Étienne Cartier Parkway structure. The existing Parkway Bridge can be modified to accommodate the LRT and highway widening.

East through the Greenbelt and into Orleans there is sufficient space for both the median LRT and the highway widening within the existing right-of-way.

Jeanne D'Arc Station

The first station east of Montreal Road, and the Greenbelt, is at Jeanne D'Arc Boulevard, in the median of Hwy174. The track is widened out for the centre platform at the station. There would be a station house on either side of Jeanne D'Arc to allow direct access for passengers transferring to and from buses or

from the sidewalks of each side of the road. The interchange has been modified to remove the free flow ramp in the northeast quadrant and direct traffic through the intersection to accommodate bus operations in the curb lane across the bridge. This bridge also requires widening to 6 lanes to allow for 4 lanes of general traffic and a curb lane that will permit buses serving the station to transfer passengers.

Orléans Boulevard Station

The next station is at Orléans Boulevard. There is no access to OR 174 from the roadway, however bus service on the bridge will serve the station houses on the east and west sides of the bridge. This station is centered underneath the bridge.

Place D'Orléans Station

The next station is at Place D'Orléans and the current Phase 2 terminus for the Confederation Line East LRT program under the affordable plan. This station ties into the large bus loop in front of Place D'Orléans immediately south of the highway and to the Park and Ride lot to the north. Currently these are connected by a pedestrian bridge. This station is deliberately placed west of Champlain Street in part because the road bridge can accommodate the LRT and the existing highway in the short term but the structure needs to be rebuilt when the 174 is widened. It also aligns the platform more directly with the centre of the park and ride and the bus loop.

For the Hwy174 widening, minor modification to the ramps is required in the eastbound direction. There is also some conflict with the Hydro transmission line through the Place d'Orléans interchange that needs to be addressed. The LRT and widened highway encroach into the 15 metre wide maintenance space required by Hydro One. The preferred solution is to move the hydro lines to the south to provide Hydro One with maintenance access.

Orléans Town Centre

The next Station is a midblock station opposite the Orléans Town Centre. It is positioned to provide convenient access to the pathway to the north from connecting to the residential area. There would be a pedestrian bridge that spans the full width of the highway tying into the planned development on the south side. This station performs relatively well based on the development occurring in the town centre and with the amount of residential development it's within a reasonable walk or bicycle ride from the north of the station.

Tenth Line Station

The next station planned under the City's Transportation Master Plan was to be at Tenth Line Road. Based on the land development pattern in the future as well as the average spacing of the station, moving the station 300m to the east was examined. This would place it opposite two active development sites. As there is no bus service on Tenth Line today or planned for in the mid to long term, the transit oriented development potential of the future land uses warrant shifting the station to the east.

Taylor Creek Business Park Station

The next station identified in the City's Transportation Master Plan is Taylor Creek Business Park. This Station has extremely low ridership and is not recommended. When combined with the shift of the Tenth

Line Station 300 metres to the east, the station spacings are now too close between Tenth Line East, Taylor Creek, and Trim Road stations.

Trim Road Terminus

As the LRT approaches the planned terminus at Trim Road, there are double crossovers immediately in front of the Station to manage train operations. Trim Road and Hwy174 today is an at grade signalized intersection and grade separation is required. A roundabout at Taylor Creek Boulevard currently exists and a second roundabout to the north at Jeanne d'Arc Boulevard is proposed to facilitate u-turns. Ramps will be provided for all movements, but will be positioned to allow for good access to the LRT station and to permit transit-oriented development close to the station. The station will be placed under the Trim Road bridge with a pedestrian bridge connecting to the Park and Ride lot.

Currently the bus loop is at the south end of the Park and Ride lot. The bus Loop will be moved north closer to the LRT Station to shorten the walking distance.

[Eastern LRT Hybrid Alignment with HOV Widening \[PDF 10.203 MB \]](#)

Widening of Highway 174 / County Road 17

Will the new highway lanes be for all drivers or only vehicles with more than one person in them?
The environmental assessment will identify the impacts of the largest footprint. This will include the additional lane width and buffer width that is needed to protect for High Occupancy Vehicle (HOV) lanes for vehicles with more than one person. If the City decides to make the lanes available to all drivers, the protected right-of-way will be wide enough and will not need additional approvals.

Won't the arterial roads that connect to the highway become overloaded if the highway is widened?
Our modelling work indicates that the arterial roads can adequately handle the additional traffic moving to and from the widened highway, although the level of congestion is likely to increase as travel demand increases.
How is the widened highway being designed to make travel as safe as possible? Current design standards are being applied. With the LRT being placed in the median (from Montreal Road to the eastern terminus), the road traffic will be separated from the trains by concrete barriers and safety fences. Visibility, design speed, lighting and safety measures are being designed into the plans.

Was a southern ring road considered? A ring road has been investigated by both the MTO and the City over the years and modelling work indicates that it would be poorly used. A ring road would be very expensive and currently neither the City nor MTO are carrying it forward for development.

Was a three-lane highway with reversible lanes considered?
The City conducted a review and assessment of whether reversible lanes would be suitable in this context. Safety and cost considerations indicate that a reversible lane is not appropriate along Highway 174 and County Road 17. The costs of putting overhead signage that indicate the direction of traffic in the centre lane would be expensive. Furthermore, for safety reasons, we need to put a barrier between lanes of different directions. Using reversible lanes over long distances, with driveway accesses and at highway speeds is not a feasible option.

Is more development being allowed in the corridor?
Growth continues across the Region, however the City has placed a moratorium on lot severances and new access points to Highway 174. Existing lots that are not yet developed may be allowed a new access, but wherever feasible the City's preference is to consolidate these potential new entrances with existing entrances.

A study similar to the Greater Toronto Area's "Places to Grow" is being considered for Eastern Ontario, which would look at where development should occur. There is a lot of good agricultural land in this area, and if we build a road, there would be pressure to develop the surrounding areas and we could lose this agricultural land.

Will property be expropriated to widen the 174/17?
There are many places along the corridor where we have sufficient right-of-way. For those portions where property is required to expand the right-of-way, then we will have a discussion with those residents who will be impacted.

174/17 Widening

Trim to Meadow Lane
East of Trim Road, approaching Cardinal Creek Village, the alignment shifts to the south, away from the existing road with a 10 m multiuse pathway area along the river for a pedestrian boulevard. There is a new signalized intersection to access Cardinal Creek Village.

[Trim to Meadow Lane \[PDF 4.986 MB \]](#)

Quigley Hill to west of Cumberland Village

Further east, a new service road provides consolidated driveway access for a series of 18 properties along the north side. The service road intersection provides a turn around point between Cardinal Creek Village and Quigley Hill Road. Properties not served by the service road will have right in-right out driveways. At Quigley Hill Road there will be a signalized intersection and a new access to the launch and picnic area.

[Quigley Hill to Cumberland \[PDF 5.142 MB \]](#)

Cumberland Village

An urban cross-section with raised median, curbs and multi-use pathways is provided through Cumberland Village.

At the Cameron Street intersection a traffic signal is the technically preferred traffic control measure. A 3-lane roundabout was also examined for the Cameron intersection. Faubert Street and Peter Harkness Lane on the south side are to be closed at the 174. Driveways along the north side will have right in-right out access. A new signal at the extension of Barnett Drive is technically preferred over a 3-lane roundabout.

[Cumberland Roundabouts and Intersections \[PDF 1.233 MB \]](#)

Cumberland Village to Canaan Road

At the Cumberland Heritage Museum area there is a pedestrian signal for crossing Hwy174. On the north side, a multiuse pathway connects from the pedestrian signal to East Shore Road where a service road will connect East Shore Road and Morin Road to Kinsella Drive where a traffic signal is provided. There will be a traffic signal at Old Montreal Road. McTeer Road will be extended westerly to connect to the McSkimmings Outdoor Education Centre. Traffic along McTeer Road will use the signal at Canaan to make turns.

[Cumberland East to Caanen \[PDF 2.038 MB \]](#)

East of Canaan to East of Edwards

East of Canaan Road there will be a new service road to connect several farms and a traffic signal to cross between the farm buildings north of County Road 17 and the farm fields to the south. The next traffic signal is at Carmen Bergeron, which provides access to the Walmart store. Where County Road 17 (CR17) is close to the Ottawa River, retaining walls are provided as needed to avoid filling into the river. There is also a retaining wall between County Road 17 and existing service road west of Richelieu Street. An urban cross section with curbs and a multi-use pathway where space permits is incorporated from east of Laporte to east of Edwards Street. The existing traffic signals will be maintained at Chamberland and at Edwards. Past Edwards the urban cross-section changes back to a rural cross-section with a median barrier an roadside ditches.

[Caanen to Edwards \[PDF 9.247 MB \]](#)

East of Edwards to Landry

An unsignalized intersection with a median break will be provided at Pigeon Street and at the arena. Signals are proposed for the new intersection with the Caron extension. In areas of provincially-significant wetland retaining walls will be used to minimize the property required and environmental impacts. New development west of Laurier East will be accessed by an intersection that will also provide a connection to Laurier so that the existing intersection of Laurier East and CR17 can be closed due to ongoing safety concerns. A roundabout is illustrated, subject to discussions with the developer. At Laundry Road a roundabout is the technically preferred design.

[Edwards to Landry \[PDF 1.243 MB \]](#)

Next Steps

Based on feedback received:

- Confirm/refine Hwy 174-ELRT functional design west of Trim
- Confirm/refine Hwy 174-17 functional design refinements east of Trim
- Finalize Functional Designs

- Finalize mitigation measures
- West of Trim – June/July presentation to Ottawa Transportation Committee
- East of Trim – Fall presentation to Committees and Councils
- Public review of study documentation

For further information on this project, or to be added to our mailing list, contact:

East LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

OR174/CR17 Widening

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Funding for the OR 174/County Road 17 Widening EA Study is being provided by the Government of Ontario.

Notices



ottawa.ca
3-1-1    
TTY 613-580-2401

Notice of Commencement of Transit Project Assessment Process Confederation Line East Extension

The Project

The City of Ottawa is developing a plan to extend and expand the City's existing Light Rail Transit network. Specifically the plan includes extending the future Confederation Line farther east from Blair Station to an interim terminal at Place d'Orléans station and ultimately, to Trim Road.

The plan includes options to service the community of Orléans and adjacent lands. New stations will be located at Montreal Road, Jeanne d'Arc Boulevard, Orléans Boulevard and the terminal station at Place d'Orléans. Additional stations will ultimately be constructed at Brisebois Crescent/Mockingbird Drive (Orléans Town Centre), Tenth Line Road and the terminus of the line at Trim Road.

The Process

Planning efforts have been completed and the City is now formalizing the environmental assessment component in accordance with the Transit Project Assessment Process (TPAP) as prescribed in Ontario Regulation 231/08. As part of the TPAP, an Environmental Project Report (EPR) will be prepared to document the study process, a description of the planned project, its anticipated environmental impacts, as well as the project's consultation program.

Consultation

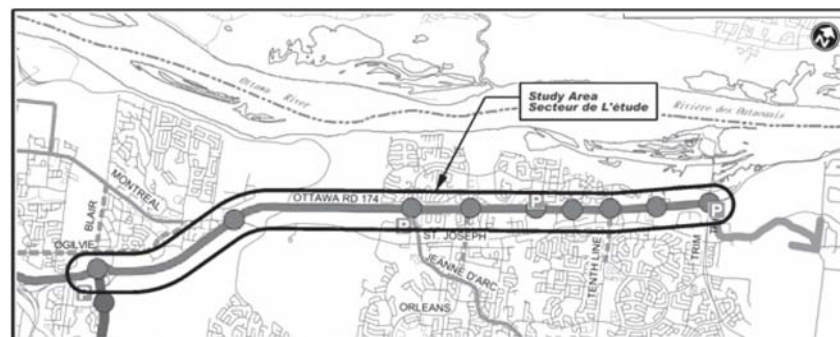
Members of the public, agencies and other interested persons are invited to review the work completed to-date, including previous consultation efforts and reports presented to City Council on this project during the planning phase. Information about the progress of the project to date is available on the City's website at ottawa.ca/easternLRT. In winter-spring 2016, the draft EPR will be made available for public review and notification will be issued.

If you would like to be added to our project mailing list, have project-related questions, or have any accessibility requirements in order to participate in this project, please contact:

Angela Taylor, P.Eng.
Senior Project Engineer
City of Ottawa
110 Laurier Avenue West, 4th Floor
Ottawa ON K1P 1J1
Tel: 613-580-2424 ext. 15210
Fax: 613-580-2578
E-mail: Angela.Taylor@ottawa.ca

Under the Freedom of Information and Protection of Privacy Act and the Environmental Assessment Act, unless otherwise stated in the submission, any personal information such as name, address, telephone number and property location included in a submission will become part of the public record files for this matter and will be released, if requested, to any person.

Notice first published on 26 November 2015.



R0013572660-1126

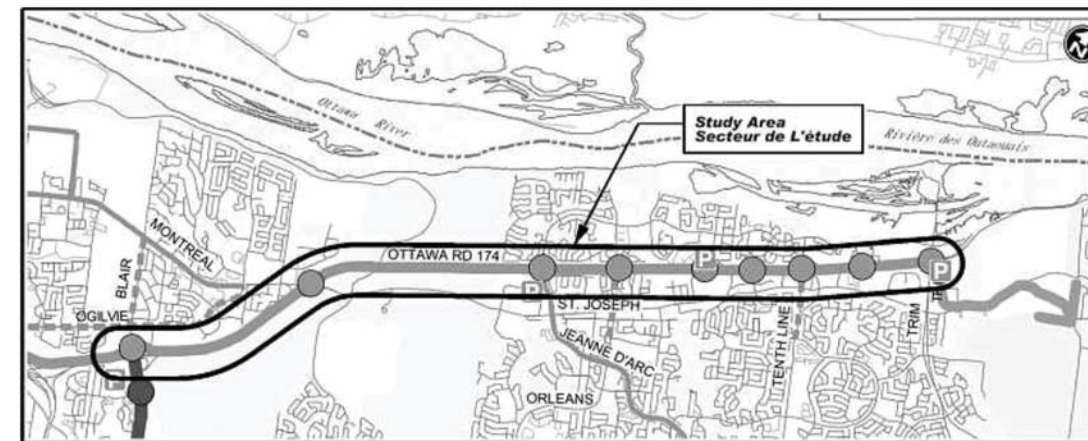
Avis du début du processus d'évaluation du projet de transport en commun visant à prolonger vers l'est la Ligne de la Confédération



Le projet

La Ville est à mettre au point un plan pour étendre et prolonger son réseau actuel de transport en commun par train. Plus précisément, le plan comporte le prolongement plus à l'est de la future Ligne de la Confédération depuis la station Blair jusqu'à une station intermédiaire à la station Place d'Orléans, pour finir au chemin Trim.

Le plan propose différentes options pour desservir la collectivité d'Orléans et les terrains adjacents. De nouvelles stations seront situées au chemin Montréal, aux boulevards Jeanne d'Arc et Orléans et à la station terminus de Place d'Orléans. D'autres stations seront construites à terme à la hauteur du croissant Brisebois et de la promenade Mockingbird (centre-ville d'Orléans), au chemin Tenth Line et au terminus du chemin Trim.



Le processus

Les travaux de planification sont maintenant terminés et la Ville s'emploie à officialiser le volet évaluation environnementale, conformément au Processus d'évaluation des projets de transport en commun en Ontario prescrit par le Règlement 231/08. Dans le cadre de ce processus, un rapport environnemental sur le projet sera produit pour documenter le processus de l'étude, décrire le projet prévu et ses répercussions attendues sur l'environnement, et présenter la démarche de consultation du projet.

Consultation

La population est invitée à prendre connaissance des travaux réalisés jusqu'à maintenant, y compris les diverses consultations qui ont eu lieu et les rapports qui ont été présentés au Conseil municipal sur ce projet au cours de l'étape de la planification. On peut obtenir de l'information sur le projet en consultant le site Web de la Ville, à l'adresse ottawa.ca/tlrest. Au cours de l'hiver-printemps 2016, le rapport environnemental préliminaire sur le projet sera publié aux fins d'examen par le public et un avis sera émis à cet effet. Un avis à ce sujet sera alors publié.

Si vous souhaitez figurer sur notre liste de diffusion relative au projet ou poser des questions ou formuler des commentaires, veuillez communiquer avec :


Angela Taylor
Ingénieure principale de projet Ville d'Ottawa
110, avenue Laurier Ouest, 4e étage
Ottawa (Ontario) K1P 1J1
613-580-2424, poste 15210
E-mail : Angela.Taylor@ottawa.ca


Aux termes de la Loi sur l'accès à l'information et la protection de la vie privée et de la Loi sur les évaluations environnementales, et sauf indication contraire dans la présentation, tous les renseignements personnels tels que le nom, l'adresse, le numéro de téléphone et l'emplacement de la propriété mentionnés feront partie des dossiers publics relatifs à cette question et pourront être communiqués à toute personne sur demande.
Avis publié le 26 novembre 2015


N° Pub 2015-133-s_27112015_fr

To the Owner/Occupant,
As a property owner residing in close proximity to the Confederation Line East LRT Extension project, you are receiving notification of this project in accordance with the Transit Project Assessment Process (TPAP) as prescribed in Ontario Regulation 231/08.

Au propriétaire/résident,
En tant que propriétaire ou résident d'une propriété à proximité du prolongement vers l'est de la Ligne de la Confédération du système de train léger rapide (TLR), vous recevez cet avis de projet conformément au processus d'évaluation des projets de transport (PEPT), tel que prescrit dans le règlement de l'Ontario 231/08.





ottawa.ca
3-1-1 
TTY 613-580-2401

Notice of Commencement of Transit Project Assessment Process Confederation Line East Extension

The Project

The City of Ottawa is developing a plan to extend and expand the City's existing Light Rail Transit network. Specifically the plan includes extending the future Confederation Line farther east from Blair Station to an interim terminal at Place d'Orléans station and ultimately, to Trim Road.

The plan includes options to service the community of Orléans and adjacent lands. New stations will be located at Montreal Road, Jeanne d'Arc Boulevard, Orléans Boulevard and the terminal station at Place d'Orléans. Additional stations will ultimately be constructed at Brisebois Crescent/Mockingbird Drive (Orléans Town Centre), Tenth Line Road and the terminus of the line at Trim Road.

The Process

Planning efforts have been completed and the City is now formalizing the environmental assessment component in accordance with the Transit Project Assessment Process (TPAP) as prescribed in Ontario Regulation 231/08. As part of the TPAP, an Environmental Project Report (EPR) will be prepared to document the study process, a description of the planned project, its anticipated environmental impacts, as well as the project's consultation program.

Consultation

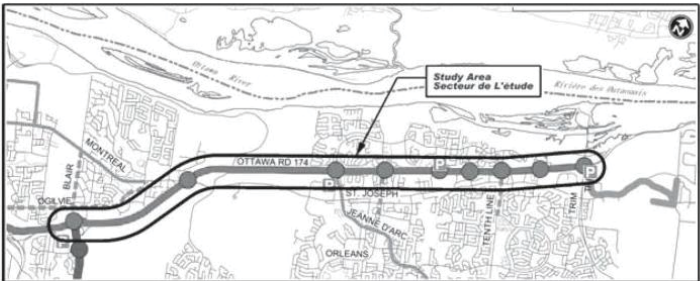
Members of the public, agencies and other interested persons are invited to review the work completed to-date, including previous consultation efforts and reports presented to City Council on this project during the planning phase. Information about the progress of the project to date is available on the City's website at ottawa.ca/easternLRT. In winter-spring 2016, the draft EPR will be made available for public review and notification will be issued.

If you would like to be added to our project mailing list, have project-related questions, or have any accessibility requirements in order to participate in this project, please contact:

Angela Taylor, P.Eng.
Senior Project Engineer
City of Ottawa
110 Laurier Avenue West, 4th Floor
Ottawa ON K1P 1J1
Tel: 613-580-2424 ext. 15210
Fax: 613-580-2578
E-mail: Angela.Taylor@ottawa.ca

Under the Freedom of Information and Protection of Privacy Act and the Environmental Assessment Act, unless otherwise stated in the submission, any personal information such as name, address, telephone number and property location included in a submission will become part of the public record files for this matter and will be released, if requested, to any person.

Notice first published on 26 November 2015.



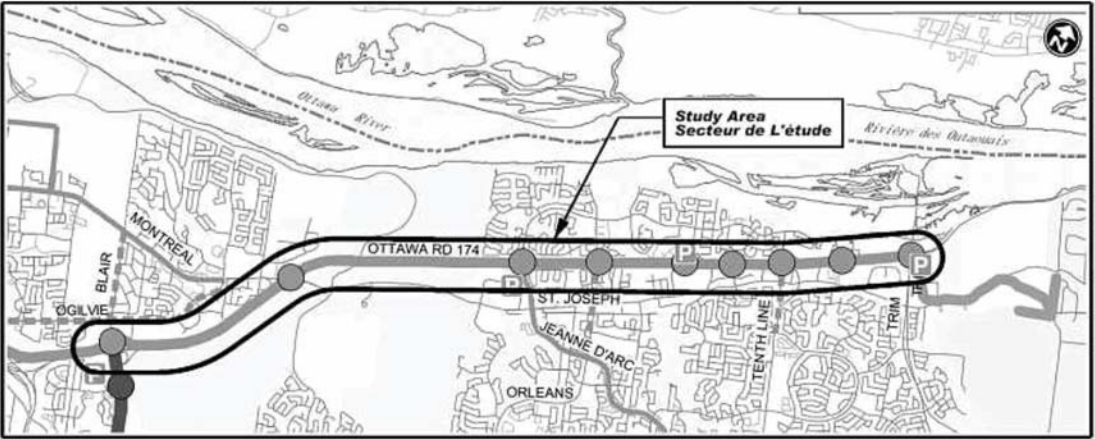
R0013572660-1126

Avis du début du processus d'évaluation du projet de transport en commun visant à prolonger vers l'est la Ligne de la Confédération



Le projet
La Ville est à mettre au point un plan pour étendre et prolonger son réseau actuel de transport en commun par train. Plus précisément, le plan comporte le prolongement plus à l'est de la future Ligne de la Confédération depuis la station Blair jusqu'à une station intermédiaire à la station Place d'Orléans, pour finir au chemin Trim.

Le plan propose différentes options pour desservir la collectivité d'Orléans et les terrains adjacents. De nouvelles stations seront situées au chemin Montréal, aux boulevards Jeanne d'Arc et Orléans et à la station terminus de Place d'Orléans. D'autres stations seront construites à terme à la hauteur du croissant Brisebois et de la promenade Mockingbird (centre-ville d'Orléans), au chemin Tenth Line et au terminus du chemin Trim.



Le processus
Les travaux de planification sont maintenant terminés et la Ville s'emploie à officialiser le volet évaluation environnementale, conformément au Processus d'évaluation des projets de transport en commun en Ontario prescrit par le Règlement 231/08. Dans le cadre de ce processus, un rapport environnemental sur le projet sera produit pour documenter le processus de l'étude, décrire le projet prévu et ses répercussions attendues sur l'environnement, et présenter la démarche de consultation du projet.

Consultation
La population est invitée à prendre connaissance des travaux réalisés jusqu'à maintenant, y compris les diverses consultations qui ont eu lieu et les rapports qui ont été présentés au Conseil municipal sur ce projet au cours de l'étape de la planification. On peut obtenir de l'information sur le projet en consultant le site Web de la Ville, à l'adresse ottawa.ca/tlrest. Au cours de l'hiver-printemps 2016, le rapport environnemental préliminaire sur le projet sera publié aux fins d'examen par le public et un avis sera émis à cet effet. Un avis à ce sujet sera alors publié.
Si vous souhaitez figurer sur notre liste de diffusion relative au projet ou poser des questions ou formuler des commentaires, veuillez communiquer avec :

Angela Taylor
Ingénieure principale de projet Ville d'Ottawa
110, avenue Laurier Ouest, 4e étage
Ottawa (Ontario) K1P 1J1
613-580-2424, poste 15210
E-mail : Angela.Taylor@ottawa.ca

Aux termes de la Loi sur l'accès à l'information et la protection de la vie privée et de la Loi sur les évaluations environnementales, et sauf indication contraire dans la présentation, tous les renseignements personnels tels que le nom, l'adresse, le numéro de téléphone et l'emplacement de la propriété mentionnés feront partie des dossiers publics relatifs à cette question et pourront être communiqués à toute personne sur demande.
Avis publié le 26 novembre 2015

Notice:

From: Taylor, Angela
Sent: Wednesday, November 25, 2015 10:38 AM
To: Monette, Bob; Mitic, Jody; Tierney, Timothy; Blais, Stephen; Egli, Keith
Cc: Landry, Lechelle; Kingston, Mike; Bureau, Mike; Brown, Stephanie; Ryan, Sara; Chi, Vivi; McKinney, Frank; Swail, Chris
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Dear Councillors,

You are receiving this email because the Stage 2 Confederation Line East LRT Extension Environmental Assessment (EA) Study includes parts of your ward and to inform you that the attached Notice of Commencement will be released this Thursday November 26, 2015. Issuance of this Notice is a requirement for the Ontario Transit Project Assessment Process (TPAP) and will be mailed via Canada Post to all property owners within 30m of the Confederation Line East LRT Extension corridor.

The Notice of Commencement will also be emailed to everyone on the Confederation Line East LRT Extension EA Study project contact list. Furthermore, it will be posted to the project website at www.ottawa.ca/easternlrt and published in the EMC and Le Droit newspapers on November 26 and December 3, 2015.

Regards,
 Angela

Angela Taylor, P.Eng.
 Senior Project Engineer
 Transportation Planning
 Gestionnaire principale de projet
 Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
 ☎ 613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

This e-mail originates from the City of Ottawa e-mail system. Any distribution, use or copying of this e-mail or the information it contains by other than the intended recipient(s) is unauthorized. Thank you.

Le présent courriel a été expédié par le système de courriels de la Ville d'Ottawa. Toute distribution, utilisation ou reproduction du courriel ou des renseignements qui s'y trouvent par une personne autre que son destinataire prévu est interdite. Je vous remercie de votre collaboration.

Notice:

From: Ghiourelotis, Catherine
Sent: Wednesday, December 2, 2015 11:17 AM
Bcc: [All Contacts on Study Contact List]
Cc: McGirr, Valerie <Valerie.McGirr@aecom.com>; angela.taylor@ottawa.ca
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Good morning,

Please find attached the Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension.

Any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email catherine.ghiourelotis@aecom.com.

Bonjour,

Veuillez trouver ci-joint l'avis du début du processus d'évaluation du projet de transport en commun visant à prolonger vers l'est la Ligne de la Confédération.

Veuillez faire parvenir vos questions à Angela Taylor, Ingénieure principale de projet à la Ville d'Ottawa. Ses coordonnées se trouvent dans l'avis ci-joint.

Si vous ne voulez plus recevoir de mises à jour par rapport à ce projet, veuillez envoyer un courriel à catherine.ghiourelotis@aecom.com.

Catherine Ghiourelotis
 Environmental Planner, Environment
 D +1-613-820-8282-ext-264
 M +1-613-276-4627
catherine.ghiourelotis@aecom.com

AECOM
 302-1150 Morrison Drive
 Ottawa, Ontario K2H 8S9, Canada
 T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

Aboriginal Consultation



September 3, 2014

Senior Claims Analyst
Aboriginal Affairs and Northern Development Canada
Environmental Unit - Environmental Assessment Coordination
25 St. Clair Avenue East, 8th Floor
Toronto, ON M4T 1M2
cau-uca@aadnc-aandc.gc.ca

Dear Sir/Madam,

Project No: 60323982

Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell County Road 17 Class Environmental Assessment

You are receiving this letter because you are included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project in Ottawa, Ontario, and we request confirmation regarding the communities included in our engagement efforts to date:

- Algonquins of Ontario Consultation Office
- Kitigan Zibi Anishinabeg First Nation
- Métis Nation Of Ontario
- Quebec Métis Nation

We have provided information below pertaining to the projects underway, as well as a key map identifying the project area. Comments will be invited for consideration.

About the Project:

On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment

City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

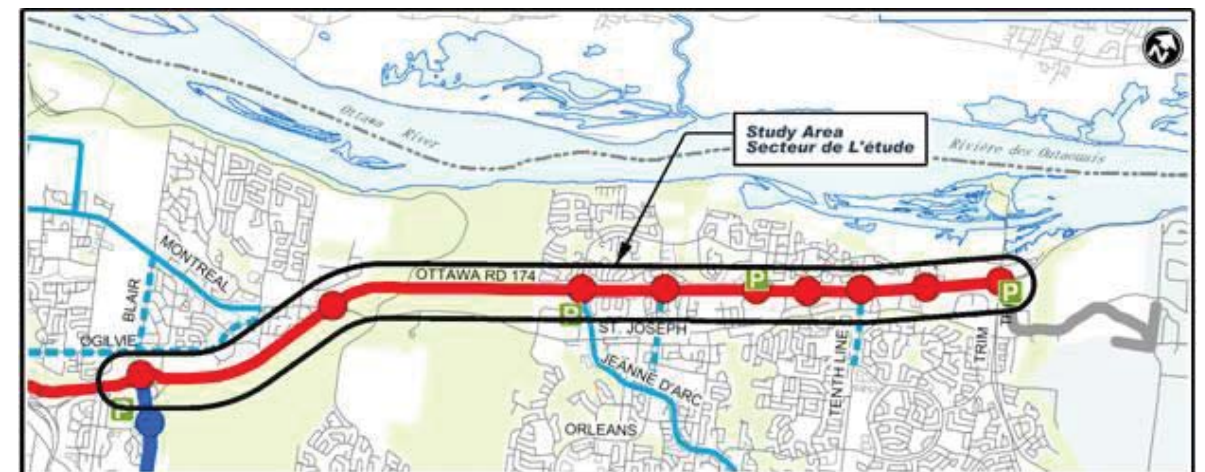
Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.

- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com

Sincerely,



Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme



September 3, 2014

Ms. Janet Stavinga
Executive Director
Algonquins of Ontario Consultation Office
31 Riverside Drive, Suite 301
Pembroke, ON K8A 8R6
E-mail: jstavinga@nrtco.net

Dear Ms. Stavinga:

Project No: 60323982

Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell County Road 17 Class Environmental Assessment

You are receiving this letter because you are included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project. We once again wish to invite a representative of your group to participate in the agency consultation group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us. Comments will be invited for consideration.

About the Project:

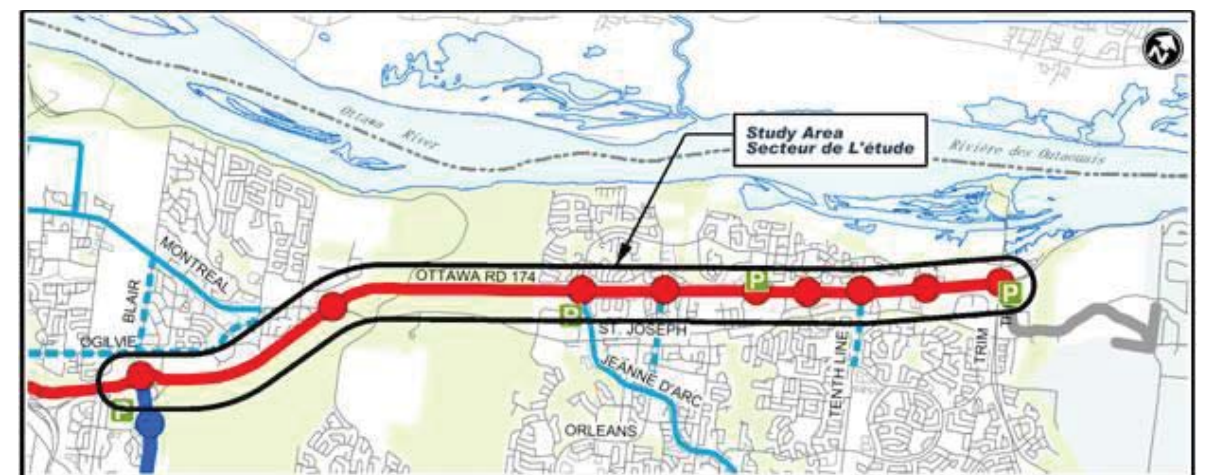
On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of

measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.
- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique
des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com

Sincerely,



Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme



September 3, 2014

Chief Gilbert Whiteduck
Kitigan Zibi Anishinabeg First Nation
P.O. Box 309
1 Paganakomin Mikan
Maniwaki, QC J9E 3C9

Dear Chief Whiteduck:

Project No: 60323982

Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell County Road 17 Class Environmental Assessment

You are receiving this letter because you are included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project. We once again wish to invite a representative of your group to participate in the agency consultation group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us. Comments will be invited for consideration.

About the Project:

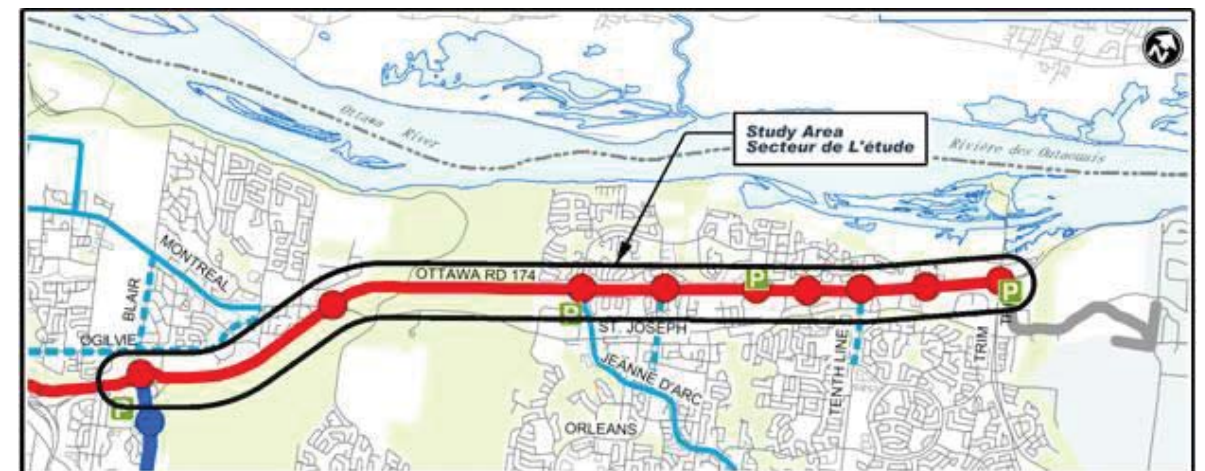
On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of

measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.
- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique
des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Sincerely,

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons

Survol de l'étude, 3 septembre 2014

Approche coordonnée : processus d'évaluation du couloir est du train léger sur rail et évaluation environnementale de portée générale du projet de la route 174 d'Ottawa et de la route de comté 17 de Prescott-Russell.

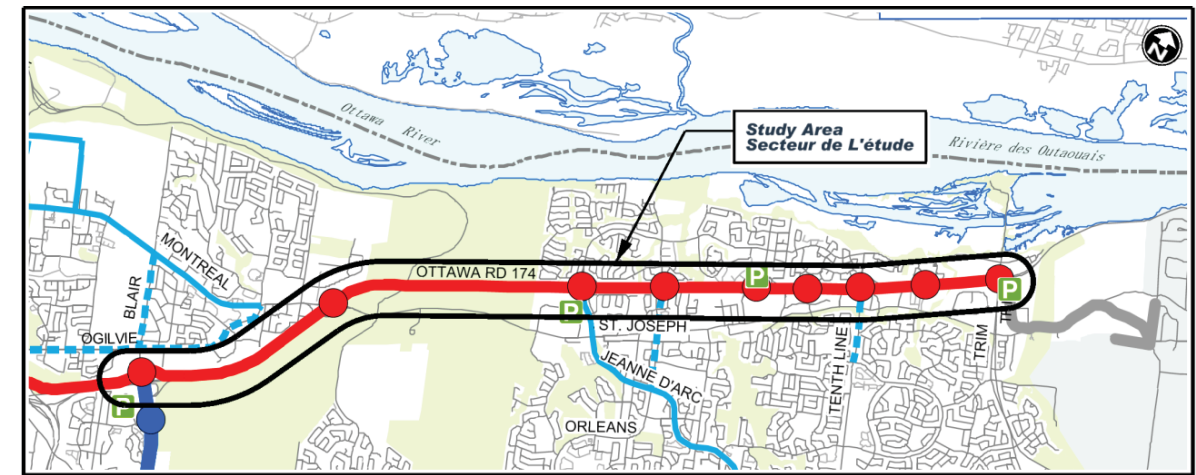
Vous recevez le présent courriel parce que vous figurez comme membre sur notre liste d'envoi pour l'étude d'évaluation environnementale de portée générale de la route 174 d'Ottawa et de la route de comté 17. Veuillez nous aviser si vous ne souhaitez plus recevoir de message au sujet des deux études mentionnées en objet.

Le 26 novembre 2013, le Conseil approuvait le Plan directeur des transports (PDT) de 2013, qui mentionne la proposition d'Étape 2 de prolongement à l'est, vers Orléans, de la Ligne de la Confédération du train léger, un tronçon appelé le couloir est du train léger sur rail (TLR). La zone délimitant le projet du couloir est du TLR va de la station Blair jusqu'au chemin Trim. La Ville a entrepris l'étude de planification et d'évaluation environnementale conformément au Processus d'évaluation des projets de transport en commun de la province d'Ontario (Règl. de l'Ont. 231/08). Le Processus d'évaluation des projets de transport en commun (PÉPTC) exige l'étude d'autres options, la consultation du public et des organismes, l'évaluation des répercussions éventuelles sur l'environnement et la prévision de mesures d'atténuation. Dans le cadre du PÉPTC, un rapport environnemental concernant le projet sera préparé et présenté pour consultation publique.

L'énoncé de travail de l'étude de planification et d'ÉE a été approuvé par le Comité des transports le 5 février 2014 et peut être consulté en suivant le lien suivant : [Statement of Work](#) (Énoncé des travaux)

Le PDT de 2013 prévoit que le couloir est du TLR soit parallèle à la route 174 d'Ottawa (OR174). À l'intérieur de ce couloir, une étude d'ÉE conjointe est présentement en cours au sujet de l'élargissement proposé de la route 174 se prolongeant jusqu'à la route de comté 17, à partir de la jonction de l'autoroute 417 et de la route 174 jusqu'aux limites de Clarence-Rockland dans les Comtés Unis de Prescott et Russell (Prescott-Russell). Étant donné que l'évaluation environnementale se rapportant aux deux routes traverse deux secteurs de compétence municipale, elle est effectuée conjointement par la Ville et par Prescott et Russell, avec un financement provincial. On peut trouver plus d'informations au sujet de cette étude à <http://ottawa.ca/fr/hotel-de-ville/consultations-publiques/etude-de-la-route-174-dottawa-et-la-route-de-comte-17> ou <http://ottawa.ca/fr/hotel-de-ville/consultations-publiques/etude-de-la-route-174-dottawa-et-la-route-de-comte-17>.

Étant donné que le TLR et l'élargissement de la route auront lieu dans le même couloir, il faut une approche coordonnée des deux études d'ÉE afin d'obtenir une solution optimale qui fera le meilleur usage du couloir de la route 174 tout en minimisant le total des coûts de construction. Le secteur de l'étude principale est illustré ci-dessous :



On procède au PÉPTC de façon parallèle à l'évaluation environnementale actuelle des deux routes, afin de coordonner le partage d'information, le travail de conception technique et les activités de consultation des parties intéressées. Les membres des organismes publics et les autres personnes intéressées sont encouragés à participer activement au projet en assistant aux séances de consultation offertes ou en s'adressant directement au personnel pour présenter des informations, des commentaires ou des questions. L'évaluation environnementale existante concernant les deux routes a compris un certain élément de planification initiale et de consultations préalables au sujet du couloir est du TLR.

L'étude coordonnée visera à :

- poursuivre le processus actuel d'ÉE des deux routes afin d'étudier l'élargissement possible des routes contenues dans ce couloir;
- étudier d'autres options de coordination et de conception du TLR en vue d'un prolongement du TLR vers l'est à partir de la station Blair, en plus des liens éventuels avec la route 174;
- maintenir l'engagement des parties intéressées envers les deux projets et les études communes tout en respectant les exigences du PÉPTC et des ÉE.

Des séances portes ouvertes combinées apporteront des informations pertinentes pour les deux études d'ÉE et offriront aux parties intéressées une occasion de connaître le contenu de l'étude et d'offrir leurs commentaires au sujet de n'importe quel aspect des études coordonnées. On annoncera les séances portes ouvertes dans les journaux locaux, par courriel selon la liste d'envoi de l'étude et au moyen d'avis affichés dans le site Web.

Le calendrier de l'étude se présente comme suit :

Étapes principales	Échéances
Planification préliminaire	Terminée en 2012 et 2013
1 ^{re} série de consultations publiques	Terminée en février 2013
Options de solutions intégrées entre le tracé du TLR et l'élargissement de la route 174	Automne 2014
2 ^e série de consultations publiques	Hiver 2015
Options de solutions de conception du TLR et des stations de transport en commun intégrées avec d'autres options de conception concernant l'élargissement de la route 174	Hiver/printemps 2015
3 ^e série de consultations publiques	Fin du printemps 2015
Concept fonctionnel du plan recommandé pour une solution intégrée concernant le corridor est du TLR et les routes 174 et 17	Été/automne 2015
4 ^e série de consultations publiques	Automne 2015
Rapport environnemental sur le projet (REP)	Automne 2015
Présentation au Comité des transports en commun	Automne 2015
Processus de PÉPTC de six mois	Hiver/printemps 2016
30 jours d'examen public du TLR	Printemps 2016

Les personnes intéressées peuvent offrir leurs commentaires dans le cadre des processus d'ÉE et de PÉPTC. Tous les commentaires reçus seront réunis conformément aux exigences de la *Loi sur les évaluations environnementales* et, sauf pour l'information personnelle, feront partie du dossier public. Pour obtenir plus d'informations au sujet de ce projet ou pour faire inscrire votre nom sur notre liste d'envoi, veuillez communiquer avec :

Angela Taylor, ing.
Gestionnaire principale de projet
Direction de la planification stratégique des transports

Valerie McGirr, ing.
Chargée de projet de firme de conseils
AECOM
613-820-8282, poste 243
Courriel : valerie.mcgirr@aecom.com



Ville d'Ottawa
613.580.2424 poste 15210
Courriel : Angela.Taylor@ottawa.ca
ottawa.ca/planning / ottawa.ca/urbanisme

Cordialement,

Angela Taylor, P.Eng.
Senior Project Engineer

Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports



September 3, 2014

Ministry of Aboriginal Affairs
Consultation Unit
4th Floor
160 Bloor Street East
Toronto, ON M7A 2E6
maa.ea.review@ontario.ca

Dear Sir/Madam,

Project No: 60323982

Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell County Road 17 Class Environmental Assessment

You are receiving this letter because you are included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project in Ottawa, Ontario, and we request confirmation regarding the communities included in our engagement efforts to date:

- Algonquins of Ontario Consultation Office
- Kitigan Zibi Anishinabeg First Nation
- Métis Nation Of Ontario
- Quebec Métis Nation

We have provided information below pertaining to the projects underway, as well as a key map identifying the project area. Comments will be invited for consideration.

About the Project:

On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment

City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

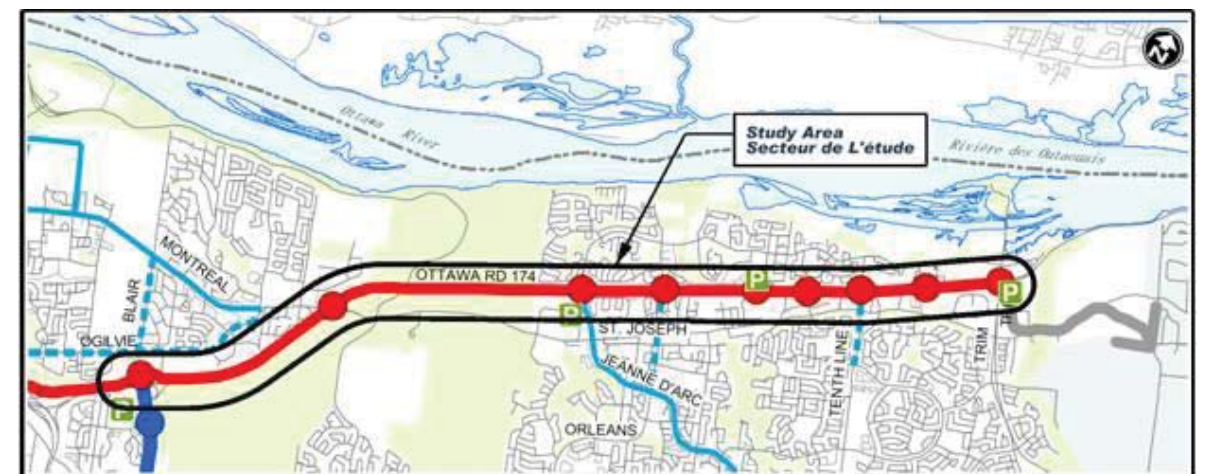
Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.

- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Sincerely,

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons



September 3, 2014

Ms. Janet Leader
Director of Communications
Metis Nation of Ontario
500 Old St. Patrick Street, Unit D
Ottawa, ON K1N 9G4

Dear Ms. Leader:

Project No: 60323982

Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell County Road 17 Class Environmental Assessment

You are receiving this letter because you are included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project. We once again wish to invite a representative of your group to participate in the agency consultation group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us. Comments will be invited for consideration.

About the Project:

On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of

City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

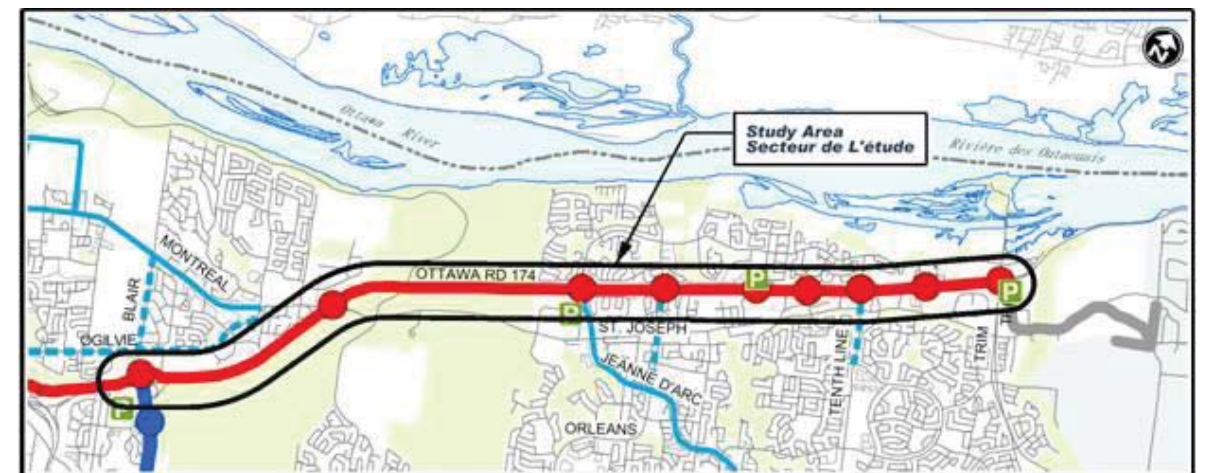
Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.
- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique
des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Sincerely,

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons



September 3, 2014

Québec Métis Nation

115 rue Lessard
St-Raymond, QC
G3L 2N1

Dear Sir/Madam:

Project No: 60270243

**Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project
Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell
County Road 17 Class Environmental Assessment**

You are receiving this letter because you are included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project. We once again wish to invite a representative of your group to participate in the agency consultation group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us. Comments will be invited for consideration.

About the Project:

On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

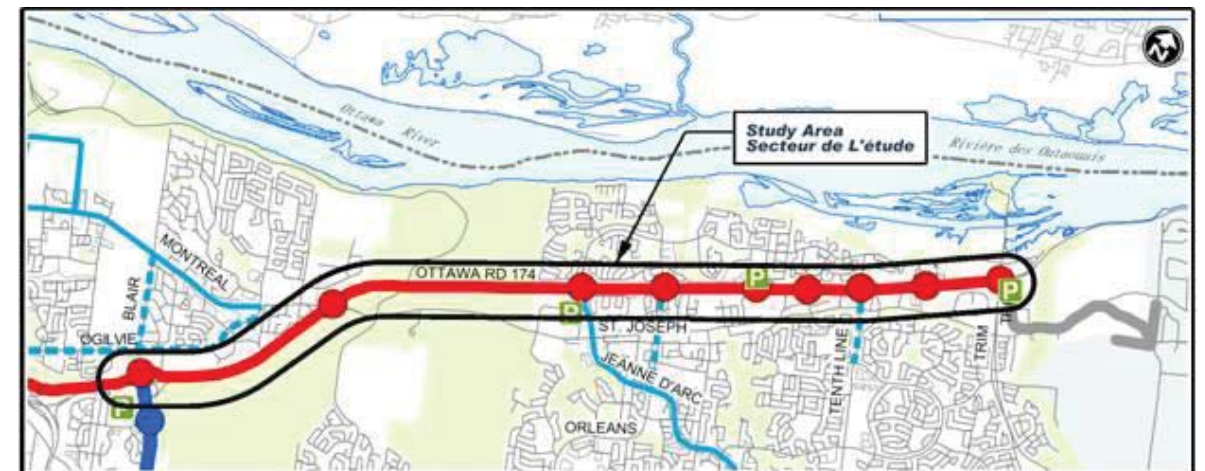
City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.
- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique
des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Sincerely,

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons

Survol de l'étude, 3 septembre 2014

Approche coordonnée : processus d'évaluation du couloir est du train léger sur rail et évaluation environnementale de portée générale du projet de la route 174 d'Ottawa et de la route de comté 17 de Prescott-Russell.

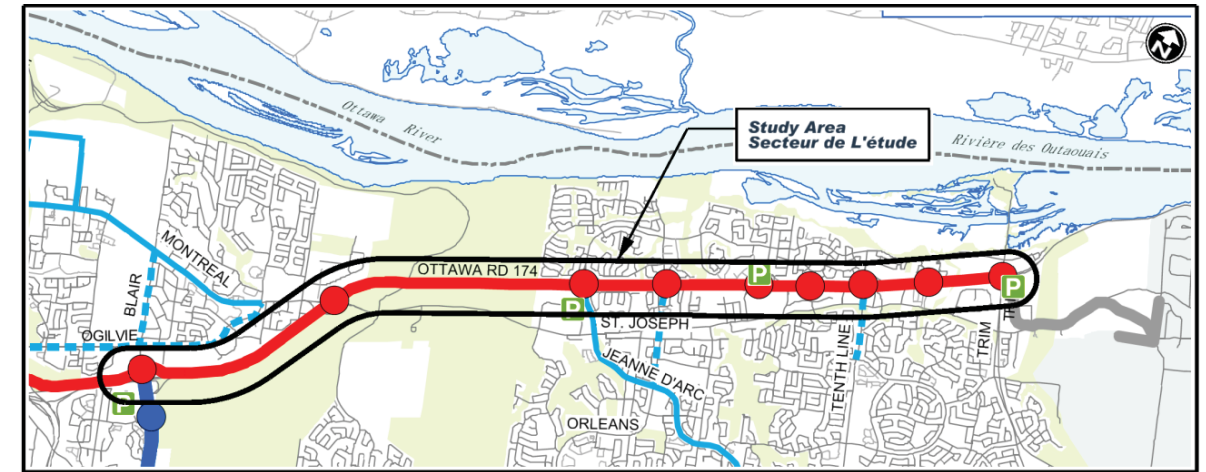
Vous recevez le présent courriel parce que vous figurez comme membre sur notre liste d'envoi pour l'étude d'évaluation environnementale de portée générale de la route 174 d'Ottawa et de la route de comté 17. Veuillez nous aviser si vous ne souhaitez plus recevoir de message au sujet des deux études mentionnées en objet.

Le 26 novembre 2013, le Conseil approuvait le Plan directeur des transports (PDT) de 2013, qui mentionne la proposition d'Étape 2 de prolongement à l'est, vers Orléans, de la Ligne de la Confédération du train léger, un tronçon appelé le couloir est du train léger sur rail (TLR). La zone délimitant le projet du couloir est du TLR va de la station Blair jusqu'au chemin Trim. La Ville a entrepris l'étude de planification et d'évaluation environnementale conformément au Processus d'évaluation des projets de transport en commun de la province d'Ontario (Règl. de l'Ont. 231/08). Le Processus d'évaluation des projets de transport en commun (PÉPTC) exige l'étude d'autres options, la consultation du public et des organismes, l'évaluation des répercussions éventuelles sur l'environnement et la prévision de mesures d'atténuation. Dans le cadre du PÉPTC, un rapport environnemental concernant le projet sera préparé et présenté pour consultation publique.

L'énoncé de travail de l'étude de planification et d'ÉE a été approuvé par le Comité des transports le 5 février 2014 et peut être consulté en suivant le lien suivant : [Statement of Work](#) (Énoncé des travaux)

Le PDT de 2013 prévoit que le couloir est du TLR soit parallèle à la route 174 d'Ottawa (OR174). À l'intérieur de ce couloir, une étude d'ÉE conjointe est présentement en cours au sujet de l'élargissement proposé de la route 174 se prolongeant jusqu'à la route de comté 17, à partir de la jonction de l'autoroute 417 et de la route 174 jusqu'aux limites de Clarence-Rockland dans les Comtés Unis de Prescott et Russell (Prescott-Russell). Étant donné que l'évaluation environnementale se rapportant aux deux routes traverse deux secteurs de compétence municipale, elle est effectuée conjointement par la Ville et par Prescott et Russell, avec un financement provincial. On peut trouver plus d'informations au sujet de cette étude à <http://ottawa.ca/fr/hotel-de-ville/consultations-publiques/etude-de-la-route-174-dottawa-et-la-route-de-comte-17> ou <http://ottawa.ca/fr/hotel-de-ville/consultations-publiques/etude-de-la-route-174-dottawa-et-la-route-de-comte-17>.

Étant donné que le TRL et l'élargissement de la route auront lieu dans le même couloir, il faut une approche coordonnée des deux études d'ÉE afin d'obtenir une solution optimale qui fera le meilleur usage du couloir de la route 174 tout en minimisant le total des coûts de construction. Le secteur de l'étude principale est illustré ci-dessous :



On procède au PÉPTC de façon parallèle à l'évaluation environnementale actuelle des deux routes, afin de coordonner le partage d'information, le travail de conception technique et les activités de consultation des parties intéressées. Les membres des organismes publics et les autres personnes intéressées sont encouragés à participer activement au projet en assistant aux séances de consultation offertes ou en s'adressant directement au personnel pour présenter des informations, des commentaires ou des questions. L'évaluation environnementale existante concernant les deux routes a compris un certain élément de planification initiale et de consultations préalables au sujet du couloir est du TLR.

L'étude coordonnée visera à :

- poursuivre le processus actuel d'ÉE des deux routes afin d'étudier l'élargissement possible des routes contenues dans ce couloir;
- étudier d'autres options de coordination et de conception du TLR en vue d'un prolongement du TLR vers l'est à partir de la station Blair, en plus des liens éventuels avec la route 174;
- maintenir l'engagement des parties intéressées envers les deux projets et les études communes tout en respectant les exigences du PÉPTC et des ÉE.

Des séances portes ouvertes combinées apporteront des informations pertinentes pour les deux études d'ÉE et offriront aux parties intéressées une occasion de connaître le contenu de l'étude et d'offrir leurs commentaires au sujet de n'importe quel aspect des études coordonnées. On annoncera les séances portes ouvertes dans les journaux locaux, par courriel selon la liste d'envoi de l'étude et au moyen d'avis affichés dans le site Web.

Le calendrier de l'étude se présente comme suit :

Étapes principales	Échéances
Planification préliminaire	Terminée en 2012 et 2013
1 ^{re} série de consultations publiques	Terminée en février 2013
Options de solutions intégrées entre le tracé du TLR et l'élargissement de la route 174	Automne 2014
2 ^e série de consultations publiques	Hiver 2015
Options de solutions de conception du TLR et des stations de transport en commun intégrées avec d'autres options de conception concernant l'élargissement de la route 174	Hiver/printemps 2015
3 ^e série de consultations publiques	Fin du printemps 2015
Concept fonctionnel du plan recommandé pour une solution intégrée concernant le corridor est du TLR et les routes 174 et 17	Été/automne 2015
4 ^e série de consultations publiques	Automne 2015
Rapport environnemental sur le projet (REP)	Automne 2015
Présentation au Comité des transports en commun	Automne 2015
Processus de PÉPTC de six mois	Hiver/printemps 2016
30 jours d'examen public du TLR	Printemps 2016

Les personnes intéressées peuvent offrir leurs commentaires dans le cadre des processus d'ÉE et de PÉPTC. Tous les commentaires reçus seront réunis conformément aux exigences de la *Loi sur les évaluations environnementales* et, sauf pour l'information personnelle, feront partie du dossier public. Pour obtenir plus d'informations au sujet de ce projet ou pour faire inscrire votre nom sur notre liste d'envoi, veuillez communiquer avec :

Angela Taylor, ing.
Gestionnaire principale de projet
Direction de la planification stratégique des transports

Valerie McGirr, ing.
Chargée de projet de firme de conseils
AECOM
613-820-8282, poste 243
Courriel : valerie.mcgirr@aecom.com



Ville d'Ottawa
613.580.2424 poste 15210
Courriel : Angela.Taylor@ottawa.ca
ottawa.ca/planning / ottawa.ca/urbanisme

Cordialement,

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning

Gestionnaire principal de project
Direction de la planification stratégique des transports



September 11, 2014

Aly Alibhai
Director of Lands, Resources and Consultations
Métis Nation of Ontario
75 Sherbourne Street,
Suite 311
Toronto, ON M5A 2P9
alya@metisnation.org

Dear Aly Alibhai:

Project No: 60323982

Regarding: Update 2014 - Coordinated Approach: Eastern LRT Transit Project Assessment Process (TPAP) and Ottawa Road 174/ Prescott-Russell County Road 17 Class Environmental Assessment

You are receiving this letter because the Métis Nation of Ontario is included on our mailing list for the OR 174/CR 17 Class EA Study. Please advise us if you no longer wish to receive communications or you have assigned another delegate on the two studies noted in the subject line.

We are interested in engaging with aboriginal groups that may have aboriginal interests regarding the proposed project. We once again wish to invite a representative of your group to participate in the agency consultation group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us. Comments will be invited for consideration.

About the Project:

On 26 November 2013, Ottawa City Council approved the 2013 Transportation Master Plan (TMP) that identifies the Stage 2 proposal to extend the Light Rail Confederation Line further east to Orléans, known as the Eastern Light Rail Transit (LRT). The Eastern LRT project limits extend from Blair Station to Trim Road. The City has initiated the Planning and Environmental Assessment (EA) study in accordance with the Province of Ontario's Transit Projects Assessment Process (O. Reg. 231/08). The Transit Projects Assessment

City Of Ottawa
Planning and Growth Management
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
Tel. : 613-580-2424 ext.: 15210
Email: Angela.Taylor@Ottawa.ca

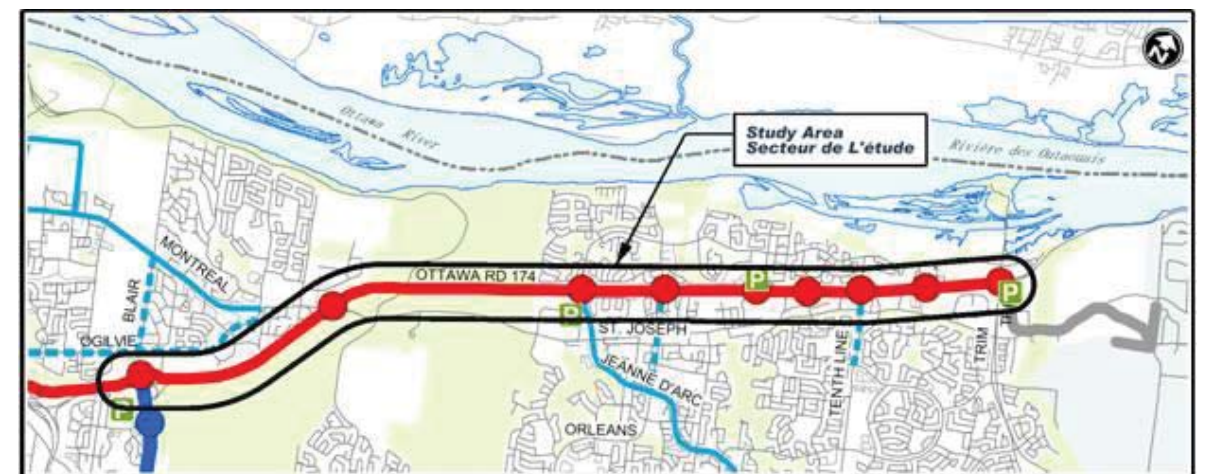
Ville d'Ottawa
Urbanisme et Gestion de la croissance
110 , avenue Laurier Ouest, 4ième étage
Ottawa (Ontario) K1P 1J1
Tél. : 613-580-2424 poste: 15210
Courriel : Angela.Taylor@Ottawa.ca

Process (TPAP) requires consideration of alternative designs, public and agency consultation, an assessment of potential environmental impacts and identification of measures to mitigate impacts. As part of the TPAP, an Environmental Project Report will be prepared and will be available for public review.

The Statement of Work for this Planning and EA study was approved at Transportation Committee on 5 February 2014 and is available at the following link: [Statement of Work](#)

The 2013 TMP envisions locating the Eastern LRT along the Ottawa Road 174 (OR174) road corridor. Within this corridor, a multi-jurisdictional Municipal Class EA Study is currently underway on a proposed widening of the OR174, extending to County Road 17 (CR17) from Highway 417/Split to the limits of Clarence-Rockland in the United Counties of Prescott Russell (Prescott-Russell). As the OR174/CR17 EA study crosses two municipal jurisdictions, it is being carried out jointly between the City and Prescott Russell with provincial funding. More information on this study is available at www.Ottawa.ca/hwy174and17study or www.Ottawa.ca/etuderoute174et17.

With the LRT and road widening sharing the same corridor, a coordinated approach to both EA studies is required to ensure an optimal solution that fully utilizes the OR174 road corridor is achieved, while minimizing overall construction costs. The primary study limits are illustrated below.



The TPAP is being conducted in parallel with the existing OR174/CR17 EA study to coordinate information sharing, technical design work, and stakeholder consultation activities. Members of the public, agencies, aboriginal groups and other interested persons are encouraged to participate actively in the project by attending consultation opportunities or contacting staff directly with information, comments or questions. Some initial planning and pre-consultation on the proposed Eastern LRT was undertaken in the existing OR174/CR17 EA study.

The coordinated study will seek to:

- Continue the existing OR174/CR17 EA process to examine possible road widening of this corridor.
- Consider LRT alignments and design alternatives for an eastward LRT extension from Blair Station, as well as potential interactions with the OR174.

- Maintain common stakeholder engagement and studies, while meeting TPAP and Class EA requirements.

Combined Open Houses will provide information relevant to both EA studies, and will give interested stakeholders an opportunity to learn about the study and comment on any aspect of the coordinated studies. Notification for the Open Houses will be provided in local newspaper notices, through emails to the study mailing list, and through website postings.

The study schedule is as follows:

Milestones	Timeframe
Preliminary Planning	Completed in 2012 and 2013
Round #1 Public Consultations	Completed in February 2013
Alternative integrated solutions to the LRT Alignment and OR174 road widening	Fall 2014
Round #2 Public Consultations	Winter 2015
Alternative LRT Designs and Transit Stations integrated with the OR174 road widening alternative designs	Winter/Spring 2015
Round #3 Public Consultations	Late Spring 2015
Functional Design of Recommended Plan for the Eastern LRT and OR174/CR17 integrated solution	Summer/Fall 2015
Round #4 Public Consultations	Fall 2015
Environmental Project Report (EPR)	Fall 2015
Presentation to Transportation Committee	Fall 2015
6 Month TPAP Process	Winter/Spring 2016
EPR 30 day Public Review	Spring 2016

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, or to be added to our mailing list, please contact:

Angela Taylor, P.Eng
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Valerie McGirr, P. Eng.
Consultant Project Manager
AECOM
Phone: 613-820-8282 ext. 243
E-mail: valerie.mcgirr@aecom.com

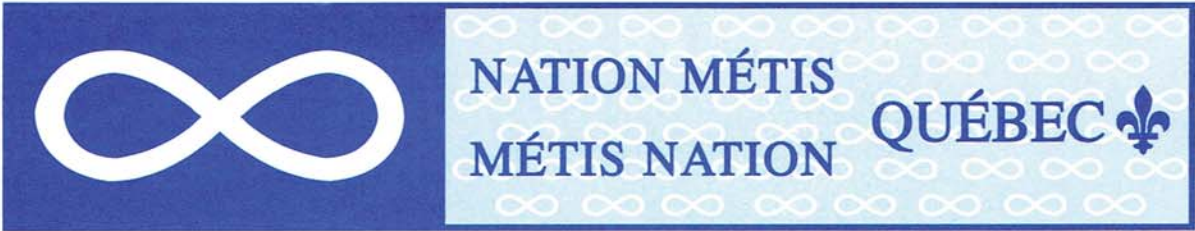


City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Sincerely,

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports

Encl.
cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons



Ghioureliotis, Catherine

From: McGirr, Valerie
Sent: February-13-15 4:39 PM
To: 'Janet Stavinga (Algonquins of Ontario)'; 'jhunton@jp2g.com'; 'Taylor, Angela'; 'Clermont, Marc'
Cc: 'Kelly Roberts'
Subject: FW: 174-17 notes of meeting Friday February 6, 2015 - for review

For your records and comment.

Attendees:
Janet Stavinga, Algonquins of Ontario Consultation Office
Jim Hunton, Jp2g
Angela Taylor, City of Ottawa
Marc Clermont, United Counties of Prescott and Russell
Kelly Roberts, Parsons
Valerie McGirr, AECOM

Discussion:

Angela described the status of the ongoing EA study. The widening of OR 174-CR 17 has been selected as the preferred alternative.	
The attached drawings illustrate the location of the Algonquin land parcel along OR 174. The property slopes to the Ottawa River.	
The City/consultant team will provide planning and engineering information available for the area such as topography and property fabric as well as the functional design for the road widening.	Valerie and the design team
In addition, the slide presentation from the Open Houses, the evaluation report and the archaeological report will be sent to Janet and Jim. Previous MTO studies will also be forwarded.	Valerie
Janet noted that they would like to review the Stage 1 report prior to submission to the Ministry and stressed the importance of archaeology to the Algonquins. The Algonquins of Ontario have hired a heritage planner who will be starting work soon.	Valerie
Throughout the land claim area, the Algonquins are interested in exploring opportunities to incorporate linkages to their history. This could be through interpretive pathways, identifying trees for their historical and/or spiritual importance. Access to the water is also important.	
Access to the Algonquin land parcel will be needed from OR 174. Should the parcel be sub-divided, a consolidated driveway would be considered.	All

Val
Valerie McGirr, P. Eng.
Manager, Ottawa Office
D 613.820.8282 ext 243

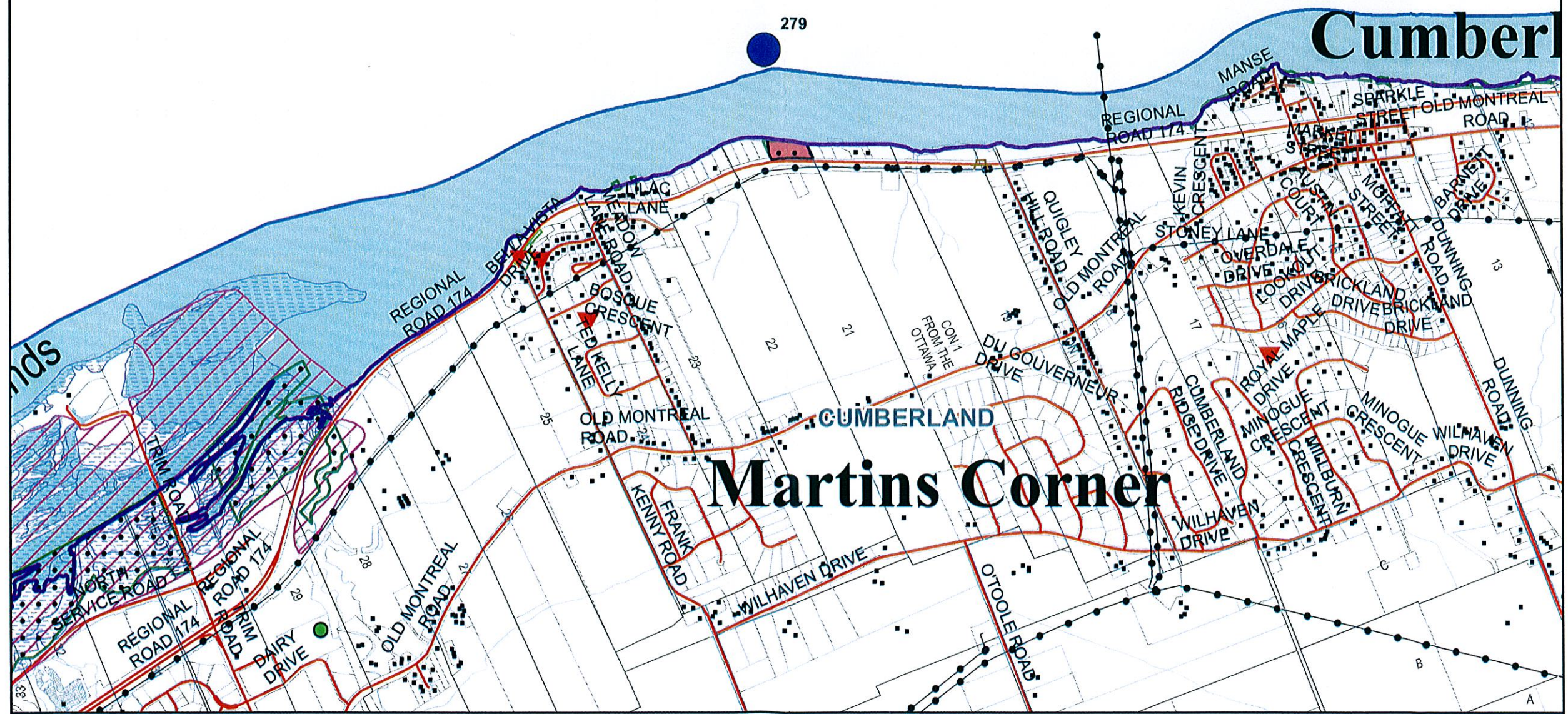
AECOM
302-1150 Morrison Drive, Ottawa ON K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

To whom it concern.

Hello,regarding your letter dated September 3,2014,we in Québec Metis Nation,are not affected by this project is happening out off our jurisdiction.

We want to thank you for your interest in indigenous communauty and, we appreciate your offer.

Best regards, Jean-Guy Gariépy,for Michel Lefebvre,Métis Nation Spokeman



Lands Proposed for Transfer to the
Algonquins of Ontario: R5 279:

0 250 500 750 1,000 m
1:30,000



This map is intended to be used for internal
purposes only and is not to be distributed.
Do not rely on it as being a precise indicator
of routes, locations of features, or as a guide
to navigation. This map may contain
cartographic errors or omissions.

Map data compiled from various sources.
Datum: North American 1983
Projection: NAD 1983 UTM Zone 18N
Published January 3, 2013



Ontario Review - Imagery Map:
R5 279

0 100 m
1:2,000



DRAFT
Confidential
to Negotiations

This map is intended to be used for internal purposes only and is not to be distributed. Do not rely on it as being a precise indicator of routes, locations of features, or as a guide to navigation. This map may contain cartographic errors or omissions.

Map data compiled from various sources.
Map Datum: NAD 83
Map Projection: UTM Zone 18
Published July 31, 2012

Ghioureliotis, Catherine

From: Edwards, Nelson <Nelson.Edwards@ottawa.ca>
Sent: June-23-15 2:41 PM
To: 'Janet Stavinga (Algonquins of Ontario)'; 'cluckasavitch@nrtco.net'
Cc: Stage2 - Etape2; Simpson, Colin; Taylor, Angela; Edwards, Nelson
Subject: City of Ottawa Stage 2 Environmental Assessment and Functional Design Report : Meeting Notice 29 June 2015

Greetings/Bonjour,

This note is to inform you that the Stage 2 Environmental Assessment and Functional Design Report went live yesterday with the Finance and Economic Development Committee agenda (FEDCO) for June 29. The link to the FEDCO agenda can be found here:
<http://app05.ottawa.ca/sirepub/mtgviewer.aspx?meetid=6594&doctype=AGENDA>

As you are aware, the Stage 2 LRT plan was a key piece of the 2013 Transportation Master Plan (TMP) unanimously approved by Council. This transformative \$3-billion plan includes three extensions of Ottawa’s O-Train system farther east, west and south of the City, including: the Confederation Line West extension to Baseline and Bayshore, Confederation Line East extension to Place d’Orleans, and the Trillium Line extension south to Bowesville/Riverside South.

The report released yesterday provides a summary of the functional design for each of these Stage 2 LRT extensions. The report also confirms land requirements and needs for future maintenance and storage capacity for the Confederation Line. Most importantly, the functional design work confirms that Stage 2 project estimates remain aligned with 2013 TMP figures. Subsequent to the approval of the report by Council, preliminary implementation activities will begin, and the City will continue to engage with federal and provincial partners on project funding.

Note that the City has also launched a new Stage 2 website: www.stage2LRT.ca. This website will provide you with information on Stage 2 in general as well as on the three extensions, and provides contact information, and important Stage 2 resources.

Should you have any questions, please do not hesitate to contact us.

La présente note a pour but de vous informer que le Rapport sur l’évaluation environnementale et la conception fonctionnelle de l’Étape 2 du train léger sur rail a été rendu public officiellement hier avec l’ordre du jour de la réunion du 29 juin du Comité des finances et du développement économique. Voici le lien pour l’ordre du jour pour la réunion du 29 juin :
<http://app05.ottawa.ca/sirepub/mtgviewer.aspx?meetid=6594&doctype=AGENDA>

Comme vous le savez, le plan pour l’Étape 2 du train léger sur rail était un élément important du Plan directeur des transports (PDT) de 2013 approuvé à l’unanimité par le Conseil municipal. Ce plan transformateur de 3 milliards de dollars prévoit trois prolongements du réseau de l’O-Train d’Ottawa, soit vers l’est, l’ouest et le sud : le prolongement ouest de la Ligne de la Confédération, jusqu’à Baseline et Bayshore; le prolongement est de la Ligne de la Confédération, jusqu’à Place d’Orléans, et le prolongement de la Ligne Trillium au sud jusqu’à Bowesville/Riverside-Sud.

Le rapport rendu public hier résume la conception fonctionnelle de chacun de ces prolongements de l’Étape 2. De plus, le rapport confirme les besoins en matière de terrains et de capacité future d’entreposage et d’entretien pour la Ligne de la Confédération. Par ailleurs, les travaux de la conception fonctionnelle ont permis de valider les estimations de coûts pour le projet de l’Étape 2, énoncées dans le PDT 2013. Après l’approbation du rapport par le Conseil, les activités de mise en œuvre préliminaires vont commencer; la Ville va poursuivre ses pourparlers avec ses partenaires fédéraux et provinciaux relativement au financement du projet.

Veuillez aussi prendre note que la Ville a lancé un nouveau site Web pour l’Étape 2 : www.etape2TLR.ca. Ce site Web contiendra de l’information générale sur l’Étape 2, ainsi que des renseignements particuliers sur les trois prolongements; vous y trouverez les coordonnées des personnes-ressources et des renseignements sur les ressources importantes du projet de l’Étape 2.

Si vous avez des questions, veuillez ne pas hésiter à communiquer avec nous.

Nelson Edwards
Senior Project Manager,
Transportation Planning Branch -
Environmental Assessment Unit
Gestionnaire principal de projet;
Planification de transport - Étude
environnementale



City of Ottawa | Ville d’Ottawa
110 Laurier Avenue West,
Ottawa, Ontario K1P 1J1
☎ 613.580.2424 ext./poste
21290
ottawa.ca/planning/ /
ottawa.ca/urbanisme

Colin Simpson, MCIP RPP
Senior Project Manager,
Transportation - Strategic
Planning Unit
Transportation Planning
Branch
Gestionnaire principal de
projet
Division de planification de
transport



City of Ottawa | Ville d’Ottawa
☎ 613.580.2424 ext./poste
27881
ottawa.ca/planning/ /
ottawa.ca/urbanisme

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de
projet
Direction de la planification
stratégique des transports



City of Ottawa | Ville d’Ottawa
☎ 613.580.2424 ext./poste
15210
ottawa.ca/planning/ /
ottawa.ca/urbanisme

Ghioureliotis, Catherine

From: Kielstra, David
Sent: April-08-15 12:39 PM
To: 'jstavinga@nrtco.net'
Cc: 'Taylor, Angela'; 'Clermont, Marc'; 'Kelly Roberts'; McGirr, Valerie
Subject: Invitation to the third Agency Consultation Group (ACG) meeting for Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the second ACG meeting for the Confederation Line Eastern LRT extension
Attachments: ELRT-Hwy 174 POH April FINAL eng-french.pdf

Dear Ms. Stavinga:

On March 30, 2015 we extended an invitation to the Algonquins of Ontario regarding the upcoming Open Houses for the Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies. I am once again providing you with the details if you wish to send a representative or encourage members of your community to attend to learn more about the project. The open house information is attached and included below.

We also wish to once again invite a representative of your group to participate in the Agency Consultation Group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. We are interested in engaging with Aboriginal groups that may have Aboriginal interests regarding the proposed project. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

The next ACG meeting to be held on:

Monday April 13, 2015
9:00 a.m. to 11:30 a.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West, Ottawa, Ontario

At this ACG meeting, we will present:

- Functional design of the preferred integrated LRT and OR 174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

A teleconference number is also included if you would like to participate by phone. Teleconference: Toll-free: 1-866-500-7930; Local: 613-244-1308; Conference ID: 4380617.

If you are unable to attend the ACG meeting, you may wish to join us at one of the public open houses being held as follows:

Monday, April 20, 2015 Community Pentecostal Church 1825 St. Joseph Boulevard Orléans	Tuesday, April 21, 2015 R. J. Kennedy Community Centre Hall A & B, 1115 Dunning Road Cumberland	Thursday, April 23, 2015 Clarence Creek Community Hall 418 Lemay Street Clarence Creek
---	--	--

Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.
---	---	---

Information about the study can be found on the City of Ottawa web site at ottawa.ca/easternlrt or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study.

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, please contact:

Eastern LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

OR174/CR17 Widening

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,

David Kielstra on behalf of

Angela Taylor, P.Eng.

Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons
Marc Clermont, Director of Public Works, United Counties of Prescott and Russell

Ghioureliotis, Catherine

From: Kielstra, David
Sent: April-08-15 12:39 PM
To: 'gwhiteduck@hotmail.com'
Cc: 'Taylor, Angela'; 'Clermont, Marc'; 'Kelly Roberts'; McGirr, Valerie
Subject: Invitation to the third Agency Consultation Group (ACG) meeting for Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the second ACG meeting for the Confederation Line Eastern LRT extension
Attachments: ELRT-Hwy 174 POH April FINAL eng-french.pdf

Dear Chief Whiteduck:

I would like to inform you and your community about upcoming an upcoming set of Open Houses for the Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies. I am providing you with the details if you wish to send a representative or encourage members of your community to attend to learn more about the project. The open house notice is attached and included below.

We also wish to once again invite a representative of your group to participate in the Agency Consultation Group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. We are interested in engaging with Aboriginal groups that may have Aboriginal interests regarding the proposed project. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

The next ACG meeting to be held on:

Monday April 13, 2015
9:00 a.m. to 11:30 a.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West, Ottawa, Ontario

At this ACG meeting, we will present:

- Functional design of the preferred integrated LRT and OR 174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

A teleconference number is also included if you would like to participate by phone. Teleconference: Toll-free: 1-866-500-7930; Local: 613-244-1308; Conference ID: 4380617.

If you are unable to attend the ACG meeting, you may wish to join us at one of the public open houses being held as follows:

Monday, April 20, 2015 Community Pentecostal Church 1825 St. Joseph Boulevard Orléans	Tuesday, April 21, 2015 R. J. Kennedy Community Centre Hall A & B, 1115 Dunning Road Cumberland	Thursday, April 23, 2015 Clarence Creek Community Hall 418 Lemay Street Clarence Creek
---	--	--

Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.
---	---	---

Information about the study can be found on the City of Ottawa web site at ottawa.ca/easternlrt or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study.

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, please contact:

Eastern LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

OR174/CR17 Widening

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,

David Kielstra on behalf of

Angela Taylor, P.Eng.

Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
☎ 613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons
Marc Clermont, Director of Public Works, United Counties of Prescott and Russell

Ghioureliotis, Catherine

From: Kielstra, David
Sent: April-08-15 12:39 PM
To: 'alya@metisnation.org'
Cc: 'Taylor, Angela'; 'Clermont, Marc'; 'Kelly Roberts'; McGirr, Valerie
Subject: Invitation to the third Agency Consultation Group (ACG) meeting for Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the second ACG meeting for the Confederation Line Eastern LRT extension
Attachments: ELRT-Hwy 174 POH April FINAL eng-french.pdf

Dear Ms. Aly Alibhai,

On March 30, 2015 we extended an invitation to the Métis Nation of Ontario regarding the upcoming Open Houses for the Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174 / Prescott-Russell County Road 17 Widening Environmental Assessment Studies. I am once again providing you with the details if you wish to send a representative or encourage members of your community to attend to learn more about the project. The open house information is attached and included below.

We also wish to once again invite a representative of your group to participate in the Agency Consultation Group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. We are interested in engaging with Aboriginal groups that may have Aboriginal interests regarding the proposed project. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

The next ACG meeting to be held on:

Monday April 13, 2015
9:00 a.m. to 11:30 a.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West, Ottawa, Ontario

At this ACG meeting, we will present:

- Functional design of the preferred integrated LRT and OR 174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

A teleconference number is also included if you would like to participate by phone. Teleconference: Toll-free: 1-866-500-7930; Local: 613-244-1308; Conference ID: 4380617.

If you are unable to attend the ACG meeting, you may wish to join us at one of the public open houses being held as follows:

Monday, April 20, 2015 Community Pentecostal Church 1825 St. Joseph Boulevard Orléans	Tuesday, April 21, 2015 R. J. Kennedy Community Centre Hall A & B, 1115 Dunning Road Cumberland	Thursday, April 23, 2015 Clarence Creek Community Hall 418 Lemay Street Clarence Creek
---	--	--

Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.
---	---	---

Information about the study can be found on the City of Ottawa web site at ottawa.ca/easternlrt or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study.

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, please contact:

Eastern LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

OR174/CR17 Widening

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,

David Kielstra on behalf of

Angela Taylor, P.Eng.

Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons
Marc Clermont, Director of Public Works, United Counties of Prescott and Russell

Ghioureliotis, Catherine

From: Kielstra, David
Sent: April-08-15 12:38 PM
To: 'chiefcouncil@pikwakanagan.ca'
Cc: 'Taylor, Angela'; 'Clermont, Marc'; 'Kelly Roberts'; McGirr, Valerie
Subject: Invitation to the third Agency Consultation Group (ACG) meeting for Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the second ACG meeting for the Confederation Line Eastern LRT extension
Attachments: ELRT-Hwy 174 POH April FINAL eng-french.pdf

Dear Chief Whiteduck:

On March 30, 2015 we extended an invitation to the Algonquins of Pikwàkanagàn regarding the upcoming Open Houses for the Confederation Line East Light Rail Transit (LRT) Extension Ottawa Road 174/ Prescott-Russell County Road 17 Widening Environmental Assessment Studies. I am once again providing you with the details if you wish to send a representative or encourage members of your community to attend to learn more about the project. The open house information is attached and included below.

We also wish to once again invite a representative of your group to participate in the Agency Consultation Group (ACG) which was formed for the OR 174/CR 17 Class EA Study and will also be considering the Eastern LRT. The group meets in advance of each Public Open House and receives a presentation on the progress of the study. We are interested in engaging with Aboriginal groups that may have Aboriginal interests regarding the proposed project. If you are interested in participating in the ACG or would like to discuss an alternative consultation format that better suits your needs please contact us.

The next ACG meeting to be held on:

Monday April 13, 2015
9:00 a.m. to 11:30 a.m.
Ottawa City Hall, Honeywell Room
110 Laurier Avenue West, Ottawa, Ontario

At this ACG meeting, we will present:

- Functional design of the preferred integrated LRT and OR 174 widening west of Trim Road
- Functional design of the widening of OR 174 and CR 17 east of Trim Road
- Summary of comments and responses from the February 2015 consultation
- Next steps

A teleconference number is also included if you would like to participate by phone. Teleconference: Toll-free: 1-866-500-7930; Local: 613-244-1308; Conference ID: 4380617.

If you are unable to attend the ACG meeting, you may wish to join us at one of the public open houses being held as follows:

Monday, April 20, 2015 Community Pentecostal Church 1825 St. Joseph Boulevard Orléans	Tuesday, April 21, 2015 R. J. Kennedy Community Centre Hall A & B, 1115 Dunning Road Cumberland	Thursday, April 23, 2015 Clarence Creek Community Hall 418 Lemay Street Clarence Creek
---	--	--

Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.	Open House: 6 to 9 p.m. Presentation: 7 p.m.
---	---	---

Information about the study can be found on the City of Ottawa web site at ottawa.ca/easternlrt or at www.prescott-russell.on.ca/en/public-works/major-projects or ottawa.ca/hwy174and17study.

Interested persons can provide comments as part of the EA and TPAP processes. Any comments received will be collected under the *Environmental Assessment Act* and, with the exception of personal information, will become part of the public record. For further information on this project, please contact:

Eastern LRT and OR174/CR17 Widening

Angela Taylor, P. Eng.
Senior Project Engineer
City of Ottawa
613-580-2424, ext. 15210
E-mail: Angela.Taylor@ottawa.ca

OR174/CR17 Widening

Marc R. Clermont, P. Eng.
Director of Public Works
United Counties of Prescott and Russell
613-675-4661, ext. 3100
E-mail: MClermont@prescott-russell.on.ca

Sincerely,

David Kielstra on behalf of

Angela Taylor, P.Eng.

Senior Project Engineer
Transportation Planning
Gestionnaire principal de project
Direction de la planification stratégique des transports



City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

cc: Valerie McGirr, Consultant Project Manager, AECOM
Kelly Roberts, Environmental Planner, Parsons
Marc Clermont, Director of Public Works, United Counties of Prescott and Russell

Ghioureliotis, Catherine

From: Kielstra, David
Sent: June-09-15 4:33 PM
To: 'jstavinga@nrtco.net'
Subject: Agency Consultation Group (ACG) minutes - Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and Confederation Line Eastern LRT extension
Attachments: PRES-2015-04-13- ELRT 174-17 ACG.PDF; ACG_MeetingMinutes_Apr13_2015_Final.pdf

Dear Ms. Stavinga,

Please see the minutes and presentation from the April Agency Consultation Group meeting. The meeting provided an update on progress regarding the Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the Confederation Line Eastern LRT extension project.

If you have any comments or questions, please contact the following members of the project team:

Marc R. Clermont, P. Eng. Director of Public Works United Counties of Prescott and Russell Phone: 613-675-4661 ext. 3100 E-mail: MClermont@prescott-russell.on.ca	Angela Taylor, P. Eng. Senior Project Engineer City of Ottawa Phone: 613-580-2424 ext. 15210 E-mail: Angela.Taylor@ottawa.ca
---	---

Dave Kielstra, M.A
Consultation & Communications Specialist
Impact Assessment & Planning
D: 613.634.2830 M: 226.203.7334
David.Kielstra@aecom.com

AECOM
654 Norris Court
Kingston, ON K7P 2R9
T: 519.650.5313 F: 613.389.6729

Ghioureliotis, Catherine

From: Kielstra, David
Sent: June-09-15 4:32 PM
To: 'gwhiteduck@hotmail.com'
Subject: Agency Consultation Group (ACG) minutes - Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and Confederation Line Eastern LRT extension
Attachments: PRES-2015-04-13- ELRT 174-17 ACG.pdf; ACG_MeetingMinutes_Apr13_2015_Final.pdf

Dear Chief Whiteduck,

Please see the minutes and presentation from the April Agency Consultation Group meeting. The meeting provided an update on progress regarding the Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the Confederation Line Eastern LRT extension project.

If you have any comments or questions, please contact the following members of the project team:

Marc R. Clermont, P. Eng. Director of Public Works United Counties of Prescott and Russell Phone: 613-675-4661 ext. 3100 E-mail: MClermont@prescott-russell.on.ca	Angela Taylor, P. Eng. Senior Project Engineer City of Ottawa Phone: 613-580-2424 ext. 15210 E-mail: Angela.Taylor@ottawa.ca
---	---

Dave Kielstra, M.A
Consultation & Communications Specialist
Impact Assessment & Planning
D: 613.634.2830 M: 226.203.7334
David.Kielstra@aecom.com

AECOM
654 Norris Court
Kingston, ON K7P 2R9
T: 519.650.5313 F: 613.389.6729

Ghioureliotis, Catherine

From: Kielstra, David
Sent: June-09-15 4:32 PM
To: 'alya@metisnation.org'
Subject: Agency Consultation Group (ACG) minutes - Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and Confederation Line Eastern LRT extension
Attachments: PRES-2015-04-13- ELRT 174-17 ACG.PDF; ACG_MeetingMinutes_Apr13_2015_Final.pdf

Dear Ms. Aly Alibhai,

Please see the minutes and presentation from the April Agency Consultation Group meeting. The meeting provided an update on progress regarding the Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the Confederation Line Eastern LRT extension project.

If you have any comments or questions, please contact the following members of the project team:

Marc R. Clermont, P. Eng. Director of Public Works United Counties of Prescott and Russell Phone: 613-675-4661 ext. 3100 E-mail: MClermont@prescott-russell.on.ca	Angela Taylor, P. Eng. Senior Project Engineer City of Ottawa Phone: 613-580-2424 ext. 15210 E-mail: Angela.Taylor@ottawa.ca
--	---

Dave Kielstra, M.A
Consultation & Communications Specialist
Impact Assessment & Planning
D: 613.634.2830 M: 226.203.7334
David.Kielstra@aecom.com

AECOM
654 Norris Court
Kingston, ON K7P 2R9
T: 519.650.5313 F: 613.389.6729

Ghioureliotis, Catherine

From: Kielstra, David
Sent: June-09-15 4:33 PM
To: 'chiefcouncil@pikwakanagan.ca'
Subject: Agency Consultation Group (ACG) minutes - Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and Confederation Line Eastern LRT extension
Attachments: PRES-2015-04-13- ELRT 174-17 ACG.PDF; ACG_MeetingMinutes_Apr13_2015_Final.pdf

Dear Chief Whiteduck,

Please see the minutes and presentation from the April Agency Consultation Group meeting. The meeting provided an update on progress regarding the Ottawa Road 174 / Prescott-Russell County Road 17 Class EA Study and the Confederation Line Eastern LRT extension project.

If you have any comments or questions, please contact the following members of the project team:

Marc R. Clermont, P. Eng. Director of Public Works United Counties of Prescott and Russell Phone: 613-675-4661 ext. 3100 E-mail: MClermont@prescott-russell.on.ca	Angela Taylor, P. Eng. Senior Project Engineer City of Ottawa Phone: 613-580-2424 ext. 15210 E-mail: Angela.Taylor@ottawa.ca
--	---

Dave Kielstra, M.A
Consultation & Communications Specialist
Impact Assessment & Planning
D: 613.634.2830 M: 226.203.7334
David.Kielstra@aecom.com

AECOM
654 Norris Court
Kingston, ON K7P 2R9
T: 519.650.5313 F: 613.389.6729

Ghioureliotis, Catherine

From: Ghioureliotis, Catherine
Sent: September-10-15 11:18 AM
Cc: angela.taylor@ottawa.ca; McGirr, Valerie; 'kelly.robbers@parsons.com'
Subject: Confederation Line East Extension - Draft EPR Review

Bcc: 'jstavinga@nrtco.net'; 'chiefcouncil@pikwakanagan.ca'; 'gwhiteduck@hotmail.com'; 'alya@metisnation.org'

Good morning,

The City of Ottawa will soon initiate the Transit Project Assessment Process (TPAP) to examine and seek approval for the Confederation Line East Extension (Blair Station to Trim Road). The Confederation Line East Extension is a key component of the recommended interconnected system of rapid transit corridors in the City. The corridor offers significant opportunities for compact, mixed-used development in the vicinity of stations.

This project will follow the provincial TPAP as set out in *Ontario Regulation 231/08 – Transit Project Regulation*. Coming into effect in June 2008, this regulation provides a framework for the self-assessment for focused consultation and the assessment of potential environmental effects and decision-making to be completed within a regulated six-month timeframe. Once the Notice of Commencement for this project is issued, there will be up a 120-day comment period.

The intent of this email is to provide an opportunity to review the draft Environmental Project Report (EPR) for this project prior to the Notice of Commencement. As such, please indicate whether you would:

- Would you like to receive a digital or print version of the draft EPR?
- If you would prefer receiving a paper version, how many copies do you require?

Please do not hesitate to contact me if you require any additional information.

Sincerely,

Catherine Ghioureliotis, B.Soc.Sc.
Environmental Planner, Environment
D 613.820.7728 x 264
catherine.ghioureliotis@aecom.com

AECOM
302 - 1150 Morrison Drive, Ottawa, Ontario K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

Ghioureliotis, Catherine

From: Ghioureliotis, Catherine
Sent: September-21-15 1:20 PM
Cc: angela.taylor@ottawa.ca; McGirr, Valerie; kelly.robbers@parsons.com; 'stephen.fitzpatrick@parsons.com'
Subject: Follow-up - Confederation Line East Extension - Draft EPR Review

Bcc: 'jstavinga@nrtco.net'; 'chiefcouncil@pikwakanagan.ca'; 'gwhiteduck@hotmail.com'; 'alya@metisnation.org'

Good Afternoon,

This is a follow-up to our initial email. The City of Ottawa will soon initiate the Transit Project Assessment Process (TPAP) to examine and seek approval for the Confederation Line East LRT Extension (Blair Station to Trim Road). The Confederation Line East Extension is a key component of the recommended interconnected system of rapid transit corridors in the City. The corridor offers significant opportunities for compact, mixed-used development in the vicinity of stations.

The intent of this email is to provide an opportunity to review the draft Environmental Project Report (EPR) for this project prior to the Notice of Commencement. As such, please indicate whether you would:

- Would you like to receive a digital or print version of the draft EPR?
- If you would prefer receiving a paper version, how many copies do you require?

If no response is received by Wednesday, September 23, 2015 we will forward a digital (CD) copy to your office.

Sincerely,

Catherine Ghioureliotis, B.Soc.Sc.
Environmental Planner, Environment
D 613.820.7728 x 264
catherine.ghioureliotis@aeacom.com

AECOM
302 - 1150 Morrison Drive, Ottawa, Ontario K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

From: Ghioureliotis, Catherine
Sent: September-10-15 11:18 AM
Cc: angela.taylor@ottawa.ca; McGirr, Valerie; 'kelly.robbers@parsons.com'
Subject: Confederation Line East Extension - Draft EPR Review

Good morning,

The City of Ottawa will soon initiate the Transit Project Assessment Process (TPAP) to examine and seek approval for the Confederation Line East Extension (Blair Station to Trim Road). The Confederation Line East Extension is a key component of the recommended interconnected system of rapid transit corridors in the City. The corridor offers significant opportunities for compact, mixed-used development in the vicinity of stations.

This project will follow the provincial TPAP as set out in *Ontario Regulation 231/08 – Transit Project Regulation*. Coming into effect in June 2008, this regulation provides a framework for the self-assessment for focused consultation and the assessment of potential environmental effects and decision-making to be completed within a regulated six-month timeframe. Once the Notice of Commencement for this project is issued, there will be up a 120-day comment period.

The intent of this email is to provide an opportunity to review the draft Environmental Project Report (EPR) for this project prior to the Notice of Commencement. As such, please indicate whether you would:

- Would you like to receive a digital or print version of the draft EPR?
- If you would prefer receiving a paper version, how many copies do you require?

Please do not hesitate to contact me if you require any additional information.

Sincerely,

Catherine Ghioureliotis, B.Soc.Sc.
Environmental Planner, Environment
D 613.820.7728 x 264
catherine.ghioureliotis@aeacom.com

AECOM
302 - 1150 Morrison Drive, Ottawa, Ontario K2H 8S9
T 613.820.8282 F 613.820.8338
www.aecom.com

Ghioureliotis, Catherine

From: Ghioureliotis, Catherine
Sent: December-22-15 3:15 PM
To: 'jstavinga@nrtco.net'
Cc: angela.taylor@ottawa.ca; McGirr, Valerie
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement - Follow Up
Attachments: ELRT-Notice-Of-Commencement-Ad.pdf; French Notice of Commencement.pdf

Good afternoon Ms. Stavinga

This email is to confirm that you received our Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension sent to you on December 2nd.

As noted in our previous email, any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email me directly at catherine.ghioureliotis@aecom.com. Otherwise, we will continue to keep you updated as this study progresses.

Thank you,

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

From: Ghioureliotis, Catherine
Sent: December-02-15 11:17 AM
Cc: McGirr, Valerie; angela.taylor@ottawa.ca
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Good morning,

Please find attached the Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension.

Any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email catherine.ghioureliotis@aecom.com.

Bonjour,

Veuillez trouver ci-joint l’avis du début du processus d’évaluation du projet de transport en commun visant à prolonger vers l’est la Ligne de la Confédération.

Veuillez faire parvenir vos questions à Angela Taylor, Ingénieure principale de projet à la Ville d’Ottawa. Ses coordonnées se trouvent dans l’avis ci-joint.

Si vous ne voulez plus recevoir de mises à jour par rapport à ce projet, veuillez envoyer un courriel à catherine.ghioureliotis@aecom.com.

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

Ghioureliotis, Catherine

From: Ghioureliotis, Catherine
Sent: December-22-15 3:20 PM
To: gwhiteduck@hotmail.com
Cc: angela.taylor@ottawa.ca; McGirr, Valerie
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement - Follow Up
Attachments: ELRT-Notice-Of-Commencement-Ad.pdf; French Notice of Commencement.pdf

Good afternoon Chief Whiteduck,

This email is to confirm that you received our Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension sent to you on December 2nd.

As noted in our previous email, any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email me directly at catherine.ghioureliotis@aecom.com. Otherwise, we will continue to keep you updated as this study progresses.

Thank you,

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

From: Ghioureliotis, Catherine
Sent: December-02-15 11:17 AM
Cc: McGirr, Valerie; angela.taylor@ottawa.ca
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Good morning,

Please find attached the Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension.

Any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email catherine.ghioureliotis@aecom.com.

Bonjour,

Veuillez trouver ci-joint l’avis du début du processus d’évaluation du projet de transport en commun visant à prolonger vers l’est la Ligne de la Confédération.

Veuillez faire parvenir vos questions à Angela Taylor, Ingénieure principale de projet à la Ville d’Ottawa. Ses coordonnées se trouvent dans l’avis ci-joint.

Si vous ne voulez plus recevoir de mises à jour par rapport à ce projet, veuillez envoyer un courriel à catherine.ghioureliotis@aecom.com.

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

Ghioureliotis, Catherine

From: Ghioureliotis, Catherine
Sent: December-22-15 3:15 PM
To: 'alya@metisnation.org'
Cc: McGirr, Valerie; angela.taylor@ottawa.ca
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement - Follow Up
Attachments: ELRT-Notice-Of-Commencement-Ad.pdf; French Notice of Commencement.pdf

Good afternoon Mr. Alibhai,

This email is to confirm that you received our Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension sent to you on December 2nd.

As noted in our previous email, any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email me directly at catherine.ghioureliotis@aecom.com. Otherwise, we will continue to keep you updated as this study progresses.

Thank you,

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

From: Ghioureliotis, Catherine
Sent: December-02-15 11:17 AM
Cc: McGirr, Valerie; angela.taylor@ottawa.ca
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Good morning,

Please find attached the Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension.

Any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email catherine.ghioureliotis@aecom.com.

Bonjour,

Veuillez trouver ci-joint l’avis du début du processus d’évaluation du projet de transport en commun visant à prolonger vers l’est la Ligne de la Confédération.

Veuillez faire parvenir vos questions à Angela Taylor, Ingénieure principale de projet à la Ville d’Ottawa. Ses coordonnées se trouvent dans l’avis ci-joint.

Si vous ne voulez plus recevoir de mises à jour par rapport à ce projet, veuillez envoyer un courriel à catherine.ghioureliotis@aecom.com.

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

Ghioureliotis, Catherine

From: Ghioureliotis, Catherine
Sent: December-22-15 3:18 PM
To: 'chiefcouncil@pikwakanagan.ca'
Cc: angela.taylor@ottawa.ca; McGirr, Valerie
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement - Follow Up
Attachments: ELRT-Notice-Of-Commencement-Ad.pdf; French Notice of Commencement.pdf

Good afternoon Chief Whiteduck,

This email is to confirm that you received our Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension sent to you on December 2nd.

As noted in our previous email, any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email me directly at catherine.ghioureliotis@aecom.com. Otherwise, we will continue to keep you updated as this study progresses.

Thank you,

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

From: Ghioureliotis, Catherine
Sent: December-02-15 11:17 AM
Cc: McGirr, Valerie; angela.taylor@ottawa.ca
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Good morning,

Please find attached the Notice of Commencement of Transit Project Assessment Process for the Confederation Line East Extension.

Any questions can be directed to Angela Taylor, Senior Project Engineer with the City of Ottawa. Her contact information can be found on the attached notice.

Should you no longer wish to receive updates about this project, please email catherine.ghioureliotis@aecom.com.

Bonjour,

Veuillez trouver ci-joint l’avis du début du processus d’évaluation du projet de transport en commun visant à prolonger vers l’est la Ligne de la Confédération.

Veuillez faire parvenir vos questions à Angela Taylor, Ingénieure principale de projet à la Ville d’Ottawa. Ses coordonnées se trouvent dans l’avis ci-joint.

Si vous ne voulez plus recevoir de mises à jour par rapport à ce projet, veuillez envoyer un courriel à catherine.ghioureliotis@aecom.com.

Catherine Ghioureliotis
Environmental Planner, Environment
D +1-613-820-8282-ext-264
M +1-613-276-4627
catherine.ghioureliotis@aecom.com

AECOM
302-1150 Morrison Drive
Ottawa, Ontario K2H 8S9, Canada
T +1-613-820-8282
aecom.com

Built to deliver a better world

[LinkedIn](#) [Twitter](#) [Facebook](#) [Instagram](#)

Comments Received Following Consultation Report #3

Comment:

From: [REDACTED]
Sent: Friday, August 14, 2015 2:08 PM
To: Taylor, Angela
Subject: RE: Stage 2 Confederation Line East Report

Hello,

I am writing back to determine where the sound barriers will be placed along the 174. I live at [REDACTED] which is on the south side of that road between Jeanne D'Arc and Orleans Blvd. The increase in noise over the last decade has had an effect on our real estate valuations and I would like some sort of assurance that the increase in noise from adding a train and two extra lanes of traffic will be mitigated.

I am curious about the time line for the proposed sound barriers. For instance, would the barriers be installed before the construction begins?

Regards,

[REDACTED]

Response:

From: Taylor, Angela
Sent: Monday, August 17, 2015 11:52 AM
To: [REDACTED]
Subject: RE: Stage 2 Confederation Line East Report

Hello [REDACTED], the study has completed a separate noise assessment of the Highway 174 road corridor for the Eastern LRT as well as the future widening of Highway 174. For your property, when either project or both projects are implemented, the noise assessment indicated no increase to the existing sound levels. Furthermore, the existing and future noise levels are at acceptable levels in accordance with the City's Environmental Noise Control Guidelines. The LRT vehicles will be electrically powered and will therefore be much quieter than the diesel powered busses currently in operation and will have minimal impact on noise levels. On the other hand, while the future road widening (beyond 2031) will generate additional highway traffic, the existing berm between your property and Highway 174 effectively acts as a sound barrier today and shields your property from the traffic noise today and in the future. As a result of this berm, no additional noise attenuation is required for your property.

If you require further clarification, please let me know.

Regards,
Angela Taylor

Angela Taylor, P.Eng.
Senior Project Engineer

Comment:

From: [REDACTED]
Sent: Friday, June 26, 2015 1:08 PM
To: Taylor, Angela
Cc: [REDACTED]
Subject: RE: ELRT/Hwy174-CR17 EA Study comments received

I recognize that the guidelines are to serve a general purpose but my property is very much unique and is strung out along the river resulting in backyard that narrow in depth with limited room for our leisure. The area directly behind the house does not make it feasible to descend there to get to a dock unfortunately, with a 10 meter cliff. Resulting in our dock being located at the only reasonable location and also very near the highway. I ask that I can meet with you on site so I can best demonstrate the uniqueness of the property so you can witness that the guidelines do not properly achieve its purpose on my property. Please get back to me.

[REDACTED]

Response:

From: Taylor, Angela [mailto:Angela.Taylor@ottawa.ca]
Sent: July-07-15 2:09 PM
To: [REDACTED]
Subject: RE: ELRT/Hwy174-CR17 EA Study comments received

Hi [REDACTED], unfortunately there are no provisions in the City's Guidelines for these types of exceptions and the City is required to adhere to the guidelines to ensure it is being applied consistently throughout the City. Note that as the City's plan to widen Highway 174 is beyond 2031, there will likely be policy changes prior to construction and there may be opportunities at that time. At the very least, a noise study will be revisited or conducted during the implementation phase to either reconfirm the results or revise the assumptions if enough time has elapsed.

Regards,
Angela

Comment:

From: [REDACTED]
Sent: July 13, 2015 10:29 AM
To: Taylor, Angela <Angela.Taylor@ottawa.ca>
Subject: RE: ELRT/Hwy174-CR17 EA Study comments received

Hi Angela
I think we can agree Guidelines are that, guidelines and that they are set up to address the norms and typical situation. I still would like to have the opportunity for the conditions of my home be seen and properly considered for its uniqueness.
Please get back to me.
Thank you.

[REDACTED]

Response:

From: Taylor, Angela
Sent: Wednesday, September 02, 2015 12:30 PM
To: [REDACTED]
Subject: RE: ELRT/Hwy174-CR17 EA Study comments received

Hi [REDACTED], while I recognize that your property is unique, the City's guidelines specifically defines noise sensitive areas to be outdoor living areas (OLA). As per the City's Environmental Noise Control Guidelines definition of an OLA:

"Outdoor Living Area is the part of an outdoor area provided for the quiet enjoyment of the outdoor environment. The OLA is typically a backyard area at ground level accommodating outdoor living activities. For sound level calculation purposes, the usual distance from the dwelling unit wall is 3m. The vertical height is 1.5m above the existing corridor surface."

Based on this definition, your dock does not qualify as an OLA and as a City employee, I do not have the authority to go against the City's guidelines.

Regards,
Angela

Comment:

From: [REDACTED]
Sent: Sunday, September 06, 2015 7:20 PM
To: Stage2 - Etape2
Subject: Question regarding the extension from Blair to Place D'Orleans

We would like to know the approximate location of the light rail between Montreal road and Jeanne D'arc. Will it us the existing bus lane, or take land from the north of the bus lane? We recently purchased a home backing onto the north side of the 174 (Vineyard Drive).Will the light rail will be much higher than the highway, and if yes, how high will it be? There is currently a large sound berm at the end of our backyard and our backyard is very private. Do you anticipate that light rail will affect our privacy? Is light rail noisier than regular highway traffic? When will construction start?

Thank you,

[REDACTED]
[REDACTED]

Response:

From: Taylor, Angela
Sent: Tuesday, September 08, 2015 3:46 PM
To: [REDACTED]
Subject: FW: Question regarding the extension from Blair to Place D'Orleans

Dear [REDACTED],

Thank you for your interest in this project. The Confederation Line East Light Rail Transit (LRT) Extension will extend east from Blair Station, which is the existing terminus for the Confederation LRT Line currently under construction. The City's Stage 2 LRT Extension East is planned to Place D'Orleans and is anticipated to be operational by 2023 subject to federal and provincial funding with a possible further extension to Trim Road if additional funding is secured. There are four (4) transit stations planned under the Stage 2 LRT Extension East: Montreal Road, Jeanne D'Arc Boulevard, Orleans Boulevard, and Place D'Orleans. Should additional funding be secured to Trim Road, another three (3) transit stations are planned: Orleans Town Centre, Tenth Line east, and Trim Road Station and Park N Ride. For this LRT to be operational by 2023, we estimate construction to start in 2018, shortly after the current LRT is operational.

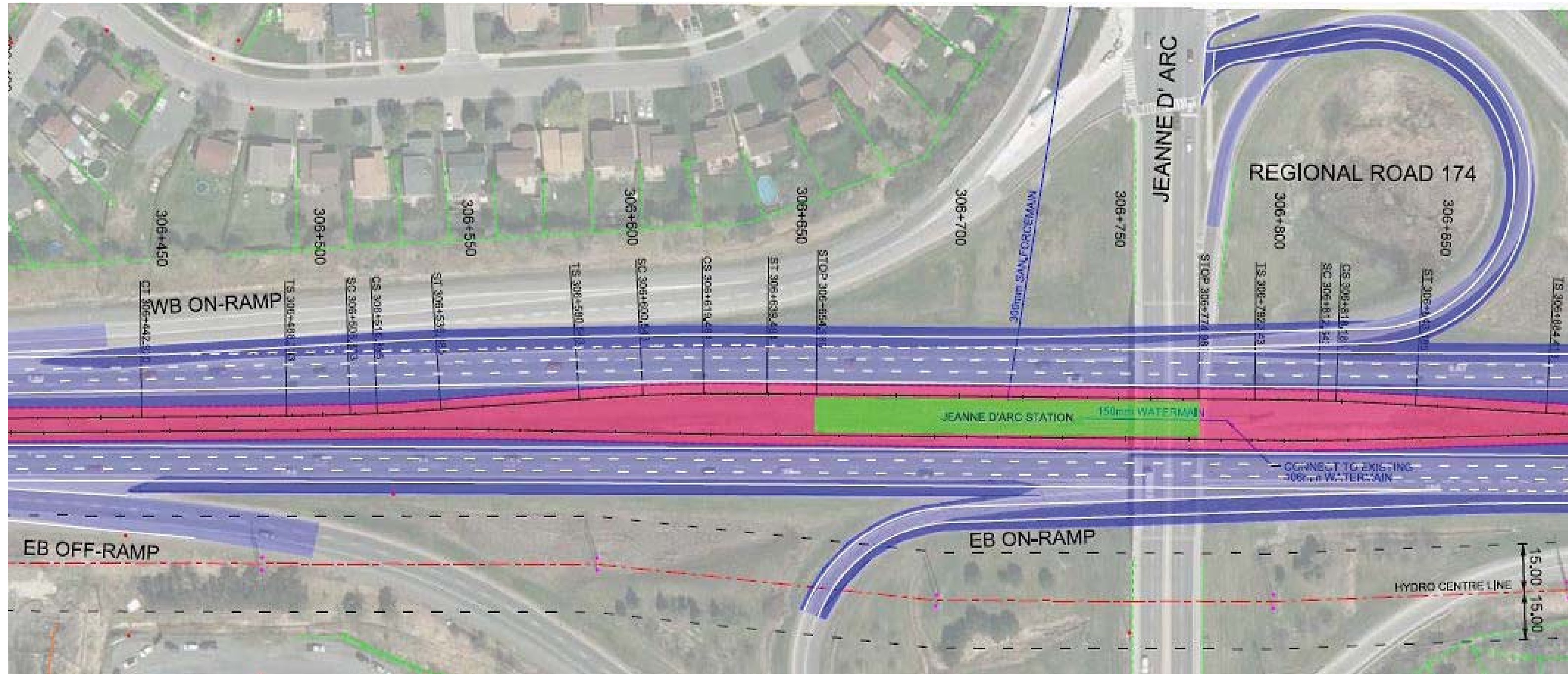
The East LRT will be co-located within the Highway 174 (Hwy174) road corridor. From Blair Station, the LRT will travel on the north side of Hwy174 with a grade separated station at the southwest quadrant of Montreal Road and Hwy174. Just beyond Montreal Road, the grade separated LRT will travel over Hwy174 and transition into the median at the same grade as Hwy174 and will continue in the median at grade to the ultimate terminus at Trim Road Station. I attach a plan with the LRT alignment near Vineyard Drive in the median as well as a future Hwy174 road widening to 6 lanes, planned post 2031. Both the road widening and LRT is well within the road corridor and no property is required. This design will allow the buses to continue using the existing shoulder bus lanes during construction of the LRT.

With respect to noise levels, the LRT vehicles will be electrically powered and will therefore be much quieter than the diesel powered busses currently in operation and will have minimal impact on noise levels. For further information on this project, please refer to the project webpage as noted below. Please don't hesitate to contact me know if you require more information on this project.

www.Ottawa.ca/easternlrt

Regards,
Angela Taylor

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports



Comment:

From: [redacted]
Sent: Friday, August 21, 2015 9:04 AM
To: Taylor, Angela
Cc: [redacted]
Subject: Re: Images of new 174

Hi Angela
Hope you are having a great summer. [redacted]
[redacted]

I was really disturbed to see the planned expansion of the 174 intends to completely destroy the berm that we have spent more than a decade developing as a visual/sound/privacy barrier between our house and the road, as [redacted] mentioned.

Have you had a chance to determine whether the road could be shifted a few meters to the south to ensure that the berm can remain. From your plan there is certainly enough room between our house and the escarpment to do this. Looking forward to hearing from you in this regard.

Another question. Is there a plan to put a cycle lane along the road so people can reach the cycle path at Trim and Petrie Island? It would make sense to have that path be a two direction one on the north side so that residents of the 174 and the village can safely ride to Trim without having to cross the road 4 lanes and a center barrier. I recall that the Taggart plan for Cardinal Creek Village was an offer to shift the road south to provide a pathway along the river. I can't find any information on this and it would seem that it would make sense that it be part of this plan. Not sure if you have any info on this.

Can you also let me know what the next steps in the timeline of finalizing the 174 expansion plan are.

Kind regards
[redacted]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
To: [redacted]
Cc: [redacted]
Sent: Thursday, August 27, 2015 9:12 AM
Subject: RE: Images of new 174

Hi [redacted]
[redacted] I hope you had a chance to review our widening design, in which we have incorporated a pedestrian and cycling facility throughout the corridor from Trim Road to Rockland. This facility consists of multi-use pathways where right-of-way exists with

linkages to service roads and paved shoulders. The 3m multi-use pathways (MUP) are located on the north side beginning at Trim Road and extending to the 10m waterfront boulevard fronting the Cardinal Creek Village community. This MUP then connects to a paved shoulder linking to the first service road provided for the residential community on the north side. From there it reconnects to the paved shoulders until it approaches the Cumberland Village where 3m MUP are provided on both sides of the road extending to the area north of Cumberland Museum. This is a sampling of our continuous facility and I encourage you to check out the project website www.ottawa.ca/hwy174and17study for further information.

With regards to your berm and armor wall, we have shifted the road centreline to the south to preserve your wall. I attach below the planned widening design illustrating the road widening primarily on the south side, although notice that some grading on your property will be required for road drainage. We are in the process of finalizing the EA study and anticipate presenting the functional design to Transportation Committee on November 4, 2015, followed by Council approval on November 12. As this study is being coordinated with the Eastern LRT EA study, the 30 day public review period will be held together in early 2016. You will be notified of these activities as they arise as well as the general public on our distribution list.

If you have any questions or concerns, please don't hesitate to contact me.

Regards,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports

City of Ottawa | Ville d'Ottawa
613.580.2424 ext./poste 15210
ottawa.ca/planning / ottawa.ca/urbanisme

Response:

From: [redacted]
Sent: Tuesday, September 01, 2015 4:38 PM
To: Taylor, Angela
Cc: [redacted]
Subject: Re: Images of new 174

Hi Angela
Thanks for getting back to me so quickly and sorry for my delay in getting back to you [redacted]. I did attend the public meetings that were held in February but the only diagrams and details at the time were those of the road widening in the village.

I am wondering if you could send us a copy of the planned section of road that is in front of our whole property [REDACTED]. From your diagram, it appears that the road is only shifted in front of the berm, west of our driveway way. The tiny bit in your diagram east of our driveway looks like the road will shift north or it this just an illusion of the picture. It would be really helpful if we could see the whole section that borders our property.

I'm really happy to hear that there is a multi-use pathway planned along the northside of the road but it is so disappointing that it will stop at CCV. Have you given thought to the section that continues on the shoulder being two directional as it will be impossible to cross 4 lanes of traffic and a central barrier without serious risk and I can't see residents (who all live on the north side, except for 1) cycling extra kilometers to access the U turn points. As well, people cycling to and from the village would enjoy riding along the water for the view in both directions. It would be great to have the cycling shoulders identified by more than just the road line. Have you seen the extra wide line they have painted on the road from Old Chelsea to the parkway parking lots on the road to Old Chelsea to separate the cycling shoulder from the car lane? It is really visible and a good deterrent to prevent drivers from straying into the cycling lane even though the cycling lane is just the paved shoulder. It would be great to consider for the 174 to make cyclists feel safer.

You may be interested to know that since the road was upgraded last year with paved shoulders (which are great to cycle on) when I indicate that I am going to make a left hand turn into my driveway, cars now speed up and pass on the right as if it was a passing lane. Despite the hundreds of signs along the 174 from Trim to the village there are no signs that say do not pass on the shoulder. Interesting behaviour. I can only imagine how crazy it will be when there are two lanes and everyone will be racing to get to Trim.

If you could send an updated pic of the road along the full length of our property that would be great, thanks. I'm keeping my fingers crossed that there will be no money in the city budget for this project for a long time.

Best regards
[REDACTED]

Response:

From: Taylor, Angela
Sent: Wednesday, September 09, 2015 8:59 AM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Images of new 174

Hi [REDACTED],

As requested, a plan of the functional design of the Hwy174 widening fronting your property is attached, along with a series of cross-sections so that you can see at various locations how the road ties into your existing topography, ditch and berm. Good drainage is a critical part of road design for infrastructure longevity. The design preserves your berm as we have added a subdrain to allow water to drain out of the granular materials that form the base of the road. The

subdrain design reduces the extent of ditching required in front of your property and we have shown a ditch across the treed lot east of your driveway.

Hwy174 has a straight alignment from the end of the curve west of your property to the curve starting just west of Quigley Hill Road. There is no shifting of the alignment east of your driveway. With respect to elevation, Hwy174 has a gentle grade of about 0.35% across your property to match the existing road elevation. About half way through the treed lot east of your driveway, the road grade increases to 1%. Across from your property on the south side of Hwy174, the plan shows retaining walls where needed to avoid undercutting the power transmission poles.

The functional design illustrated on this plan will be refined through the preliminary and detailed design process as additional data is gathered regarding the topography (including your existing berm), drainage channels, slopes, characteristics of soils/rock, depth to rock and any other features that influence the design.

Multi-use pathways are being added to this project where feasible, in consultation with City staff responsible for pedestrian and cycling facility planning. Service roads will provide another off-highway location for pedestrians and cyclists to use. Where there are gaps in off-road facilities, the City will be looking at how these might be addressed, separately from this road design. The Cumberland Village vision, for example, encourages riverfront pathways that could be explored in some areas as well as facilities on Old Montreal Road where feasible. During the design process, the City will consider any updated policies and designs for pedestrian and cycling facilities based on the experience that is current at the time.

As always, please don't hesitate if you need further clarification.

Regards,
Angela

Comments Received Following Notice of Commencement

Comment:

From: [REDACTED]
Sent: Friday, November 27, 2015 11:19 AM
To: Taylor, Angela
Subject: Stage 2 conf In assess project

Good day. I'm an Orleans resident interested in receiving project details of the extension of the conf In LRT to Orleans. My info:

[REDACTED]
[REDACTED]
[REDACTED]

Thanks

Response:

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Monday, November 30, 2015 9:44 AM
To: [REDACTED]
Cc: McGirr, Valerie
Subject: RE: Stage 2 conf In assess project

Hello [REDACTED]

Thank you for your interest in this exciting LRT project. Information on this project is available on the project website at www.ottawa.ca/easternlrt. Furthermore, I attach a summary sheet highlighting both the coordinated projects of the LRT as well as the Highway 174 widening study as both projects share the same corridor.

In summary, the Confederation Line East LRT Extension to Orleans is located within the Highway 174 road corridor beginning at Blair Station and continuing on the north side of the highway and just beyond the existing road pavement until it reaches Montreal Road, the first station east of Blair station. This station is an elevated station located at the northwest quadrant of the interchange with a pedestrian plaza at grade to facilitate pedestrian and cycling access. The LRT continues east and stays elevated over the westbound lanes, then transitions into the median of the highway (between the east and westbound lanes) at grade just beyond Montreal Road, and continues in the median at grade until Trim Road station. All remaining stations are located in the median at grade. The stations are located at Jeanne D'Arc Boulevard, Orleans Boulevard, Place D'Orleans, Orleans Towns Centre, Tenth Line east, and Trim Park and Ride.

The Stage 2 Extension of LRT to Orleans extends to Place D'Orleans and is anticipated to be operational by the year 2023, while further extension to Trim Park and Ride is anticipated post 2031. The concept LRT alignment and station locations are available on the project website. We will also add your name to the project distribution list for updates to the project. If there is specific information you require, please let me know.

Regards,
Angela

Comment:

From: [REDACTED]
Sent: Thursday, December 03, 2015 11:52 AM
To: Ghioureliotis, Catherine
Cc: McGirr, Valerie; angela.taylor@ottawa.ca
Subject: RE: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Catherine, et all,

Please keep me in the loop on this project. I have reviewed the report to finance and have a number of questions related to the text and the revised configuration at Montreal Road. I would be more than willing to sit down with you and go through the comments/questions/concerns I have. Give me a call and we can discuss.

Thanks

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Response:

- Valerie McGirr conducted a phone call with [REDACTED] on December 3, 2015. [REDACTED] noted:
- There are existing slope stability issues in the vicinity of the Montreal Road interchange and at the 174 crossing of Green's Creek.
 - Suggested getting the LRT into the median (under Hwy 174) before reaching the poor soils at Montreal Road. This provides an opportunity for staging of bridge replacement with new westbound bridge. Have you checked single point interchange for suitability at Montreal?
 - Crown removal done on Highway 417 from Nicholas to the split by increasing the cross-fall to 3% for lane 3 (i.e. with crossfall of 2 % for lanes 1 and 2) to improve runoff
 - Highway settlement west of Trim does not seem to have affected the barrier
 - Check south side pier of pedestrian bridge at Place d'Orléans. It may have less clearance than the Champlain bridge. Check the elevations of the station to the existing pedestrian bridge.

Follow-up was made via email the same day :

From: McGirr, Valerie
Sent: December-03-15 1:45 PM
To: [REDACTED]; Ghioureliotis, Catherine
Cc: angela.taylor@ottawa.ca
Subject: RE: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Thanks for the chat [REDACTED]. I will tell the folks working on the alternatives your thoughts and your offer for future discussions!

Comment:

From: [REDACTED]
Sent: Friday, December 04, 2015 11:25 AM
Cc: Taylor, Angela
Subject: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Angela,

I am making a request to be included in the project mailing list for any future communications regarding the Confederation Line East LRT Extension.

I am also specifically concerned with any noise mitigation as per the MOE/MTO to meet current City of Ottawa Environmental Noise Control Guidelines.

The issue is of my concern is noise level criterion definitions for residential outdoor living areas and the inclusion of noise barrier walls in the LRT project.

Regards,
[REDACTED]

Response:

From: Taylor, Angela
Sent: Tuesday, December 08, 2015 4:02 PM
To: [REDACTED]
Subject: RE: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Hello [REDACTED], thank you for your email and interest in this exciting LRT project to Orleans. Your name has been added to our distribution list for updates to the study. Please note that the LRT study will be addressing noise mitigation as a result of traffic noise generated by the existing Highway 174 lanes. While the LRT itself will not increase the noise levels as it will be electrically powered, noise barriers have been recommended in specific locations where warranted under existing conditions of Highway 174. If you would like further information on specific noise barrier locations, please let me know.

Regards,
Angela

Response:

From: [REDACTED]
Sent: Wednesday, December 09, 2015 11:38 AM
To: Taylor, Angela

Subject: Re: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Angela,

Thank you for your reply.
I am indeed interested in all noise assessment studies regarding noise barriers for Highway 174 traffic in Orleans.

Given that the existing lanes of Highway 174 are to be moved approximately 3 meters outward, towards the shoulder, this brings the traffic noise even closer to outdoor living areas of single family homes in the LRT corridor.

I have a particular concern regarding future noise barriers on the North side of Highway 174 between Jeanne d'Arc and Orleans blvds.

As the province funded the new noise barrier installed West of Jeanne d'Arc blvd. from left over budget funds, I wonder why the barrier was not logically extended to at least Orleans blvd., or beyond.

Would you be able to inform me if only noise modeling calculations will be used or if actual field measurements will be used for accurate Leq noise assessments?

Thank for any help or information on this matter.

Regards,

[Redacted Signature]

Response:

From: Taylor, Angela
Sent: Tuesday, December 15, 2015 11:26 AM
To: [Redacted]
Subject: RE: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Hello [Redacted]

Between Jeanne d'Arc and Orleans Boulevard on the north side of Highway 174, the study is recommending a minimum 3 metre noise barrier to be placed on top of the existing berm for additional noise protection. This is to be implemented with the LRT construction, anticipated before 2023, when the LRT is planned to be operational. While the City will generally undertake modelling to determine noise levels for the ultimate widening of Highway 174, these details will

be considered during detailed design. As for the provincial funding received for the recent construction of sound barriers, the City was not consulted on the extent of the sound barriers.

Regards,
Angela

Comment:

From: [REDACTED]
Sent: Monday, December 07, 2015 2:24 PM
To: Taylor, Angela
Subject: Email List

Hi Angela,

I got a letter today, "Notice of Commencement of Transit Project Assessment Process Confederation Line East Extention". Could I please be added to the project mailing list? My address is:

[REDACTED]
[REDACTED]
[REDACTED]

Thank you very much,

[REDACTED]

Response:

From: Taylor, Angela
Sent: Tuesday, December 08, 2015 4:07 PM
To: [REDACTED]
Subject: RE: Email List

Hi [REDACTED], hope all is well and am happy to hear that you are interested in this exciting LRT project to Orleans. You should note that this project and all LRT projects will be improving cycling connectivity along the LRT facilities and into the communities as the City is prioritizing LRT, pedestrian and cycling infrastructure over roads. We have added your email to our distribution list unless you prefer being notified by mail. Please let me know and if you have any further questions, don't hesitate.

Regards and have a great Christmas,

Angela

Response:

From: [REDACTED]
Sent: Tuesday, December 08, 2015 4:12 PM
To: Taylor, Angela
Subject: Re: Email List

Thanks Angela,

I just realized who you were! Sorry for the formal e-mail at first.

Thanks for adding me to the e-mail list - that's perfect. And thanks for adding your notes about cycling infrastructure. Very exciting stuff!

Say hi to the gang for me,

[REDACTED]

Response:

From: Taylor, Angela
Sent: Wednesday, December 9, 2015 3:01 PM
To: [REDACTED]
Subject: RE: Email List

Hey [REDACTED], I didn't notice that you didn't know who I was so no worries. If you are not already plugged in, I attach a link to the City's cycling program and projects. The City is working on a winter cycling network to encourage cycling all year round. You can also sign up for the regular newsletter under the "Cycling in the City" link. Don't hesitate if you need more info on this. Happy to help.

<http://ottawa.ca/en/residents/transportation-and-parking/cycling>

Angela

Comment:

From: [REDACTED]
Sent: Monday, December 07, 2015 8:25 PM
To: Taylor, Angela
Subject: LRT Transit Project Assessment -East Expansion

Hello Angela

Could you please add me to the project mailing list? We are located on [REDACTED] and are very keen to learn more about the project. In particular, we are eager to have news about when we can expect construction of noise barriers to begin. We back onto the 174 and barriers would go a long way to reduce sound in this area. I also look forward to seeing plans for the Orleans Blvd station... this is good news for those of us in Convent Glen!

Thanks for adding us to your list.

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Response:

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Wednesday, December 09, 2015 12:28 PM
To: [REDACTED]
Subject: RE: LRT Transit Project Assessment -East Expansion

Hello M [REDACTED], thank you for your email and we have included your name on the distribution list for updates to the study. You may already have heard that the LRT study will be addressing noise mitigation as a result of traffic noise generated by the existing Highway 174 lanes. While the LRT itself will not increase the noise levels as it will be electrically powered, noise barriers have been recommended in specific locations where warranted under existing conditions on Highway 174.

More specifically for your property on [REDACTED] and your neighbours with backyards facing Highway 174, we are recommending 3 metre high noise barriers to be constructed on top of the existing berm within the City's road corridor. This will be implemented during the East LRT construction, anticipated to be operational by 2023. I hope this is great news for you and your nearby neighbours.

Regards,
Angela Taylor

Response:

From: [REDACTED]
Sent: Wednesday, December 09, 2015 12:52 PM

To: Taylor, Angela
Subject: RE: LRT Transit Project Assessment -East Expansion

Thanks Angela for taking the time to reply – we appreciate it.

Certainly the addition of 3 meter high noise barriers will be welcome... I trust their construction will be sequenced to ensure they are fully in place well BEFORE construction actually starts. Arguably, there is more than a proven need to have them in place today, let alone in the future. It's always been a mystery to me why our section of the 174 has not had any barriers. Nevertheless, good news indeed.

I think it's great that you are highlighting the fact LRT will not increase noise... hopefully with increased ridership we get more cars off the roads and reduce noise even further.

Looking forward to further updates as they become available.

[REDACTED]

Comment:

From: [REDACTED]
Sent: Monday, December 07, 2015 8:46 PM
To: Taylor, Angela
Subject: Stage 2: Transit Project: Confederation Line East Extension

Dear Angela Taylor:

I received today via mail the above notification for the proposed LRT extension from Blair Road to Place D’Orleans and eventually Trim Road.

Residing for the past thirty two years on [REDACTED] in Convent Glen I am keenly interested in following this project as it develops, especially as how it will impact, benefit, or be of concern to myself and residents of our immediate area.

I do have a question as to the proposed route placement. The insert map seems to show the LRT running Parallel or along Highway 174. Do you know at this point in time where it is tended to specifically be located on that stretch of highway i.e. on the north or south side, or centre median?

As per your information sheet, yes please put me on your project mailing list as I would like to follow this project.

Sincerely,

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Response:

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Wednesday, December 09, 2015 2:09 PM
To: [REDACTED]
Subject: RE: Stage 2: Transit Project: Confederation Line East Extension

Hello [REDACTED], thank you for your email and we have included your name on the distribution list for updates to the study. For information on our study, I encourage you to visit the project webpage at www.ottawa.ca/easternlrt that includes the LRT alignment from Blair Station to Trim Road as well as the concept station locations. A PDF of the LRT alignment is available under the “Open Houses – April 2015” link.

To summarize the alignment, from Blair Station, the LRT continues on the north side until it reaches Montreal Road Station with an elevated station at the northwest quadrant of the interchange. Heading east, the LRT stays elevated and swings over the westbound lanes into the median at grade. From here, it continues in the median until it reaches Trim Station, in which

the at grade station will be integrated with a proposed interchange at Trim Road and Highway 174. Please visit the project website for further information.

Regards,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports

Response:

From: [REDACTED]
Sent: Wednesday, December 09, 2015 3:10 PM
To: Taylor, Angela
Subject: RE: Stage 2: Transit Project: Confederation Line East Extension

Hi Angela:

Thanks for your prompt response and information. I will follow the updates on the webpage and the link that you have indicated. Much appreciated.

Regards,

[REDACTED]

Comment:

From: [redacted]
Sent: Tuesday, December 08, 2015 2:53 PM
To: Taylor, Angela
Subject: Eastern light rail

Hello Angela,

I was wondering when or if there is another open house coming up for this project?

If not can we find out if the rail is going to be installed on the highway and if so will it affect the condominiums currently on the side of the highway?

Thank you in advance,

[redacted]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-09 2:41 PM (GMT-05:00)
To: [redacted]
Subject: RE: Eastern light rail

Hello [redacted], thank you for your email. There were two open houses held in February and April of 2015 to present the study findings and there are no more open houses planned for the remainder of the study period. The LRT facility has been designed completely within the Highway 174 road corridor and no property is required. Between Blair Station and Montreal Road, the LRT facility is located on the north side of Highway 174 and closer to the community, however additional property is not needed. If you would kindly provide your address, I can provide you with the LRT design in front of your property.

Regards,

Angela

Response:

From: [redacted]
Sent: Wednesday, December 09, 2015 2:44 PM
To: Taylor, Angela
Subject: Re: Eastern light rail

Hello again Angela,

Thank you for your response.

I will let everyone here at the condo complex know that we're safe.

The address is [redacted].

Thank you again,

[redacted]

Response:

From: Taylor, Angela
Sent: Tuesday, December 15, 2015 11:40 AM
To: [redacted]
Subject: RE: Eastern light rail

Hello [redacted],

To reassure you that your property will not be affected, I attach a plan view of the LRT alignment (shown in pink) located on the north side of Highway 174 and far away from your buildings. The LRT may need a little sliver of the parking lot to the west. The red 250 mm sanitary sewer line is underground and is shown as infrastructure to avoid during construction.

Regards and happy holidays,
Angela

Comment:

From: [redacted]
Sent: Tuesday, December 08, 2015 8:45 AM
To: Taylor, Angela
Subject: Notification of transit assessment process

Good morning Angela.

Being so close to the proposed site plan for the LRT I have many concerns. Over the past year we have been bothered by the construction on the 174 culvert.....our home shaking. I question what effect the LRT will have on our home. I would like to discuss these issues and be added to the project mailing list.

[redacted]
[redacted]
[redacted]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-10 3:10 PM (GMT-05:00)
To: [redacted]
Subject: RE: Notification of transit assessment process

Hello [redacted], thank you for your email and in response to your request, we have added your name to the distribution list for project updates. I also encourage you to visit the project webpage at www.ottawa.ca/easternlrt for information on the project to date. The study reviewed vibration impacts in the surrounding area and to mitigate vibration, rail track treatments will include floating slabs, ballast mats, and resilient rail fasteners, among others.

Regards,
Angela

Comment:

From: [redacted]
Sent: Tuesday, December 08, 2015 8:17 PM
To: Taylor, Angela
Cc: [redacted]
Subject: LRT extension to Orleans and widening of highway 17/174

Hi,

I own a property at [redacted] which is just north of the park & ride at Place D Orleans Shopping Center. I just received your letter giving a simple overview of the widening of highway 17/174 and the changes at Place D' Orleans and the associated park & ride. Unfortunately, when I go on the Ottawa.ca/eastern LRT website the document does not clearly state what is being done at each of stations or the park & rides.

It would be beneficial if any of the proposed drawings or layouts provided at the open houses would be available at this website. Also, the PDF referenced in this site which is supposed to better show the route being selected is not expandable which makes it unreadable.

Would you please provide me with copies of any slides, drawings or layouts that were presented at the open house and a copy of the PDF which is expandable so that I can see how the widening might impact the parking and my property.

If you have any questions or comments, please contact me directly.

[redacted]
[redacted]
[redacted]
[redacted]
[redacted]
[redacted]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-14 10:12 AM (GMT-05:00)
To: [redacted]
Subject: RE: LRT extension to Orleans and widening of highway 17/174

Hello [redacted], thank you for your email and your interest in this exciting project. It is unfortunate that you were unable to properly access the project information on line. I have attached a link to the project information that includes the Confederation Line East LRT alignment. This link will open a PDF document of the LRT alignment and once open, a toolbar will appear at the bottom centre of the page to enlarge the document, print, save the document etc. The PDF illustrates the general footprint of the LRT alignment and concept LRT station locations, while station designs will be carried out during detailed design, commencing next year. As the LRT alignment is in the median at Place D'Orleans, there are no changes planned for the park and Ride on the north side.

<http://ottawa.ca/en/city-hall/public-consultations/transit/open-houses-april-20-21-and-23-2015>

Regards,
Angela

Comment:

From: [REDACTED]
Sent: Wednesday, December 09, 2015 1:17 PM
To: Taylor, Angela
Subject: Confederation Line Going East - Stage 2

Good morning,

I just received in the mail a notice, as I reside in close proximity to the Confederation Line East LRT Extension Project.

- 1- I would like to be added to your project mailing list;
- 2- It appears that the line will be near or adjacent to the 174. Is this correct? The noise from the 174 is already a nuisance and has reduced property values. With the LRT on the top of it, our property values will certainly continue to go down. Last year, I signed a petition to have a wall erected to reduce noise pollution from the 174, but to no avail. Can you advise if there is proposed compensation for home owners living near the proposed line or at least if there will be steps taken to reduce noise and vibration?
- 3- Will property taxes be revised downward? What other measures will be in place to diminish the impact on homeowners living in proximity.

My wife and I are retired and our home is our largest asset. I fear what will happen once the LRT is in place. Will we even be able to sell the house?

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-14 12:24 PM (GMT-05:00)
To: [REDACTED]
Subject: RE: Confederation Line Going East - Stage 2

Hello [REDACTED],

Thank you for your email and your interest in this exciting project. As requested, we have added your name to the project mailing list for updates.

The Confederation Line East LRT alignment is located in the median from Montreal Road to Trim Road and will be electrically powered and therefore very quiet when it is operational. As such, there will be no increase to the noise levels from the LRT. However, as part of the LRT project the City will be constructing sound barriers along Highway 174 where warranted as a result of existing traffic noise. This is in recognition of the recent sound barriers that were

constructed along Highway 174 west of Jeanne D'Arc Boulevard that was funded by the province.

Specifically for your property at [REDACTED] indicated below, a 3 metre sound barrier on top of the existing berm is recommended near your property to mitigate noise levels from the existing highway traffic. Construction of these barriers will be concurrent with the LRT, which is expected to be operational by 2023. This project will have no effect on your property taxes since the LRT facility will be located within the existing transportation corridor and will not impact on your lands.

Regards,
Angela

Comment:

From: [REDACTED]
Sent: Saturday, December 12, 2015 9:49 PM
To: Taylor, Angela
Subject: Route plans or map

I would like to see the proposed route plan of the rail extention to Trim rd. Also would like to know if any owners of houses in the trailer parc of Tera Nova Estates are going to be expropriated for this project. I am a new owner of [REDACTED]

THANKS.

[REDACTED]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-15 12:01 PM (GMT-05:00)
To: [REDACTED]
Subject: RE: Route plans or map

Hello [REDACTED],

Thank you for your email and your interest in this exciting study. The Confederation Line East LRT Extension is recommended in the median from east of Montreal Road to Trim Road and station. As such, the LRT will have no impact on Terra Nova Estates. Note that a future LRT station is planned 300 metres east of Tenth Line with a pedestrian overpass and within convenient walking distance from your area.

To view the LRT alignment, I have attached a link to the project information. Under this link, a PDF document of the LRT alignment titled "Eastern LRT Hybrid Alignment with HOV Widening" is available for viewing. Once open, a toolbar will appear at the bottom centre of the page to enlarge the document, print, save the document etc. The PDF illustrates the general footprint of the LRT alignment and concept LRT station locations.

<http://ottawa.ca/en/city-hall/public-consultations/transit/open-houses-april-20-21-and-23-2015>

Regards and happy holidays,
Angela

Response:

From: [REDACTED]
Sent: Wednesday, December 16, 2015 11:51 AM
To: Taylor, Angela
Subject: Re: Route plans or map

Thank you very much for your prompt reply. Quite informative and reassuring. [REDACTED]

Comment:

From: [REDACTED]
Sent: Tuesday, December 15, 2015 1:59 PM
To: Taylor, Angela
Subject: Transit Project

Good afternoon Angela

Can you please register [REDACTED] on the project mailing list as we would like to be invited to any public meetings and to know when the deadlines are for submissions.

You may use my address [REDACTED] or mail directly to [REDACTED].

Can you also please provide a website that we will be able to research information

Thanks [REDACTED]
 Property Manager

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-21 11:46 AM (GMT-05:00)
To: [REDACTED]
Subject: RE: Transit Project

Hello [REDACTED], thank you for your email and interest in this exciting project. We have included your email on our distribution list for project updates. For further project information, please refer to the project webpage at www.ottawa.ca/easternlrt. I wish to note that while the Confederation Line East LRT Extension is located on the north side of Highway 174 and closer to [REDACTED], there are no impacts [REDACTED] as no additional land or property is required for this project.

All the best for Christmas and the New Year.

Regards,
 Angela

Comment:

From: [REDACTED]
Sent: Tuesday, December 22, 2015 8:13 AM
To: Taylor, Angela
Subject: RE: Transit Project

Thank you for your reply Angela. I think what we are more concerned about will be the increase in noise [REDACTED]. Will there be any sound barriers or anything like that to prevent this.

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2015-12-23 11:19 AM (GMT-05:00)
To: [REDACTED]
Subject: RE: Transit Project

Hello [REDACTED], the LRT vehicles will be electrically powered and will therefore be much quieter than the diesel powered busses currently in operation and will have minimal impact on noise levels. As such, noise barriers are not warranted for the LRT project.

Regards,
 Angela

Comment:

From: Chris Mahood <Chris.Mahood@ontario.ca>
Sent: Tuesday, December 29, 2015 4:02 PM
To: Taylor, Angela
Subject: RE: Stage 2 Confederation Line East LRT Extension - Transit Project Assessment Process (TPAP) Notice of Commencement

Hello Angela,

MTCS comments on the Confederation Line East LRT Extension draft Environmental Project Report / Notice of Commencement are attached to this email.

Regards,
Chris

Chris Mahood, MCIP, RPP
Heritage Planner
Ministry of Tourism, Culture and Sport
416-314-5424
chris.mahood@ontario.ca

**Ministry of Tourism,
Culture and Sport**

Culture Services Unit
Programs and Services Branch
401 Bay Street, Suite 1700
Toronto ON M7A 0A7
Tel: 416 314 5424
Fax: 416 212 1802

**Ministère du Tourisme,
de la Culture et du Sport**

Unité des services culturels
Direction des programmes et des services
401, rue Bay, Bureau 1700
Toronto ON M7A 0A7
Tél: 416 314 5424
Télé: 416 212 1802



December 29, 2015 (EMAIL ONLY)

Angela Taylor
Senior Project Engineer
City of Ottawa
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
E: angela.taylor@ottawa.ca

RE: MTCS file #: 0006311
Proponent: City of Ottawa
Subject: Draft Environmental Project Report/Notice of Commencement
Confederation Line East LRT Extension
Location: Ottawa, Ontario

Dear Angela Taylor:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the draft Environmental Project Report/Notice of Commencement for your project. MTCS's interest in this EA project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- Archaeological resources, both land-based and marine;
- Built heritage resources, including bridges and monuments; and,
- Cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources.

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation. Aboriginal communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Aboriginal communities includes a discussion about known or potential cultural heritage resources that are of value to these communities. Municipal heritage committees, historical societies and other local heritage organizations may also have knowledge that contributes to the identification of cultural heritage resources.

Archaeological Resources

MTCS notes that a Stage 1 archaeological assessment has been completed for the study area and that the study area includes areas of archaeological potential. MTCS also notes that a commitment has been made in the draft Environmental Project Report to complete further archaeological assessment, prior to construction, of those areas of archaeological potential that may be impacted by construction activities. MTCS recommends that archaeological assessment take place *as early as possible in the planning process and prior to the completion*

of preliminary design, to ensure that options for avoidance and protection of archaeological sites remain open. The avoidance and protection of archaeological sites, through project redesign or incorporation of an archaeological site into a project design, is the preferred option for the mitigation of impacts.

Archaeological assessments are undertaken by archaeologists licenced under the Ontario Heritage Act, who are responsible for submitting reports directly to MTCS for review.

Built Heritage Resources and Cultural Heritage Landscapes

MTCS notes that maps 11, 12 and 13 in ‘Annex B-1. Stage 1 Archaeological Assessment’ of the Existing Conditions Report identify numerous listed and designated heritage properties in the study area. A Heritage Impact Assessment (HIA) will need to be prepared by a qualified consultant, as early as possible in the planning process and prior to the completion of preliminary design, to assess potential project impacts of the preferred alternative on built heritage resources and cultural heritage landscapes. An HIA considers how a project may impact a cultural heritage resource and, where impacts are identified, also considers conservation options, mitigative measures and alternatives (see MTCS Info Sheet #5: Heritage Impact Assessments and Conservation Plans for further information on the scope of HIAs).

The study area should also be screened for cultural heritage resources that may not have been formally identified using the MTCS Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes. A Cultural Heritage Evaluation Report (CHER) is used to determine the cultural heritage value or interest of a potential cultural heritage resource.

Please send CHERs and HIAs to MTCS, and make them available to local organizations or individuals who have expressed interest in heritage.

Environmental Assessment Reporting

All technical heritage studies and their recommendations are to be addressed and incorporated into EA reports. Please advise MTCS on additional technical heritage studies that will be completed, and provide them to MTCS before issuing a Notice of Completion.

Thank-you for circulating MTCS on this project. Please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Chris Mahood, MCIP, RPP
Heritage Planner
chris.mahood@ontario.ca

Copied to: Steven Fitzpatrick, Parsons

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MTCS makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MTCS be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MTCS if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately and the local police as well as the Cemeteries Regulation Unit of the Ministry of Government and Consumer Services must be contacted. In situations where human remains are associated with archaeological resources, MTCS should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.

Comment:

From: [redacted]
Sent: Sunday, December 27, 2015 12:08 PM
To: Taylor, Angela
Subject: Project Mailing List - Stage 2 Light Rail

Good morning Ms. Taylor,

Would it be possible to add me to your project mailing list for the stage 2 light rail project?

I am also wondering if you know whether the City plans to address the noise concerns of those residents whose properties back on to the tracks and 174 where noise and vibration is currently an issue.

Kind regards,

[redacted]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2016-01-04 10:23 AM (GMT-05:00)
To: [redacted]
Subject: RE: Project Mailing List - Stage 2 Light Rail

Dear [redacted],

Thank you for your email and your interest in this exciting project. As requested, we have added your name to the project mailing list for updates.

The Confederation Line East LRT alignment is located on the north side of Highway 174 and adjacent to your property at [redacted]. While the LRT facility is close to your property, it will also be electrically powered and therefore very quiet when it is operational. As such, there will be no increase to the noise levels from the LRT. However, as part of the LRT project the City will be constructing sound barriers along Highway 174 where warranted as a result of existing traffic noise. This is in recognition of the recent sound barriers that were constructed along Highway 174 between the Highway 416/Split and Jeanne D'Arc Boulevard that was funded by the province.

According to the noise study undertaken during this Environmental Assessment study and specifically for your property at [redacted], a 5 metre sound barrier is recommended to mitigate noise levels from the existing highway traffic. Construction of these barriers will be concurrent with the LRT, which is expected to be operational by 2023. To mitigate vibration, rail track treatments will include floating slabs, ballast mats, and resilient rail fasteners, among others.

Best regards and Happy New Year,
Angela

Comment:

From: [REDACTED]
Sent: Tuesday, January 05, 2016 3:46 PM
To: Taylor, Angela
Subject: Re: Project Mailing List - Stage 2 Light Rail

Thank you very much for your response.

Just to make sure I understood you correctly - the current metal wall behind my house will be replaced by a new sound barrier?

Best,

[REDACTED]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2016-01-07 8:53 AM (GMT-05:00)
To: [REDACTED]
Subject: RE: Project Mailing List - Stage 2 Light Rail

Hello [REDACTED], the noise barriers will be designed during implementation and at that time, the exact location will be determined but note that it will be located within the City's right of way (City property). In previous noise barrier construction projects where existing barriers/fences were in place, the City have installed noise barriers on City property adjacent to the existing barriers. As such, it is not known at this time where the exact alignment of the barriers will be.

Regards,
Angela

Comment:

From: [REDACTED]
Sent: Wednesday, December 30, 2015 2:06 PM
To: Taylor, Angela
Subject: Confédération Line Extension - Stage 2

Angela,

I am requesting to be added to the project mailing list as a resident living along the 174! Any info will be well appreciated.

I am very much interested in the progress of this.

Many thanks

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

[REDACTED]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2016-01-04 11:45 AM (GMT-05:00)
To: [REDACTED]
Subject: RE: Confédération Line Extension - Stage 2

Dear [REDACTED],

Thank you for your email and your interest in this exciting project. As requested, we have added your name to the project mailing list for updates.

The Confederation Line East LRT facility will be located on the north side of Highway 174 from Blair Station to Montreal Road Station, which is the first station for the east extension. From Montreal Road Station the LRT will travel overtop of the Highway 174 westbound lanes and will then transition to the median at grade to the terminus at Trim Road Park and Ride Station. While the LRT facility is in the median in the vicinity of your property, it will also be electrically powered and therefore very quiet when it is operational. As such, there will be no increase to the noise levels from the LRT. However, as part of the LRT project the City will be constructing sound barriers along Highway 174 where warranted as a result of existing traffic noise. This is in recognition of the recent sound barriers that were constructed along Highway 174 between the Highway 416/Split and Jeanne D'Arc Boulevard that was funded by the province.

According to the noise study undertaken during this Environmental Assessment study and specifically for your property at 6448 Sugar Creek Way, a 3 metre sound barrier to be constructed on top of the existing berm is recommended to mitigate noise levels from the existing highway traffic. Construction of these barriers will be concurrent with the LRT, which is

expected to be operational by 2023. Further information on the concept LRT alignment and station locations is available on the project webpage at www.ottawa.ca/easternlrt.

Best regards and Happy New Year,
Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports

Record of Phone Conversation:

From: Taylor, Angela [<mailto:Angela.Taylor@ottawa.ca>]
Sent: Wednesday, December 16, 2015 3:58 PM
To: McGirr, Valerie
Subject: ELRT need call back in french

Val, [REDACTED] called and though I spoke to her in English, she had great difficulty understanding as she is French. Please have someone call her tomorrow at [REDACTED]. She was concerned about having to move because of the LRT even though I reassured her that her property [REDACTED] will not be affected. She also did not understand the median/centre alignment. I think she understood that she did not have to move but an explanation in French will help her understanding of the project.

This TPAP process and the 30m notification has caused alarm for some residents. I have had calls from alarmed residents thinking they were affected and had to move.

Angela

Angela Taylor, P.Eng.
Senior Project Engineer
Transportation Planning
Gestionnaire principale de projet
Direction de la planification stratégique des transports

From: Gilbert, Elizabeth
Sent: Thursday, December 17, 2015 4:13 PM
To: McGirr, Valerie
Subject: RE: ELRT need call back in french

Hi Val,

I just spoke to [REDACTED] and she was very relieved to hear that she is well out of any proposed land acquisition limits.

Thanks,

Elizabeth Gilbert, P.Eng., ing.
Civil Engineer, Water
Ingénieure en génie civil, Eau
D +1-613-820-8282-ext-271
M +1-613-222-2240
elizabeth.gilbert@aecom.com

Comment:

From: [REDACTED]
Sent: Monday, January 04, 2016 1:36 PM
To: Taylor, Angela
Subject: Transit project East line extension

Good afternoon Angela,

I received a noticed in the mail recently about the Stage 2 transit project and wondered if you are able to tell me if the city plans to include noise barriers up to Place D'Orleans?

My condo is very close to the highway so I am concerned about additional noise.

Any information would be appreciated.

Thank you

[REDACTED]

Response:

From: "Taylor, Angela" <Angela.Taylor@ottawa.ca>
Date: 2016-01-07 8:23 AM (GMT-05:00)
To: [REDACTED]
Subject: RE: Transit project East line extension

Hi [REDACTED], thank you for your email. The Confederation Line East LRT Extension study assessed noise levels for the LRT facility and concluded that since the LRT will be electrically powered, it will be very quiet when operational. As such, there will be no increase to the noise levels from the LRT. However, this study noted an increase in noise levels due to the existing highway traffic and as part of the LRT project, the City will be constructing sound barriers along Highway 174 in areas where warranted as a result of existing traffic noise. This is in recognition of the recent sound barriers that were constructed along Highway 174 between the Highway 416/Split and Jeanne D'Arc Boulevard that was funded by the province.

If you would provide your address, I can determine if a noise barrier is warranted at your location.

Regards,
 Angela

Comment:

From: [REDACTED]
Sent: Wednesday, February 24, 2016 1:17 PM
To: Taylor, Angela
Subject: Question re LRT in Orleans Ontario
 Good afternoon Angela,

To: Angela Taylor

After reading the information being provided by the city regarding the implementation of the LRT in orleans I would like information on a few concerns I have.

Last year a petition was sent to councilors with multiple signatures of home owners on either side of the 174 between jeanne d'arc and champlain requesting a sound barrier wall to be erected . We have yet to receive any information of this status.

Questions

- 1)Will a wall be put into place and when???
- 2)The widening of the highway will it be on both sides or just one?
- 3)Is the rails being built in the centre of highway or on both sides?

I live at [REDACTED] and already the highway is so close that from my kitchen window I can see the people in the cars police giving traffic ticket etc. If the highway is widening any more they will be in my back yard.

The sound and vibration is already high how much more will it increase with the rails going through?

I also would like to be added to the information mailing this regarding this project.

If you can answer some of these questions it would be greatly appreciated.

Thank you

[REDACTED]

Response:

From: Taylor, Angela
Sent: Thursday, February 25, 2016 10:16 AM
To: [REDACTED]
Subject: RE: Question re LRT in Orleans Ontario

Hello [REDACTED], thank you for your email and I hope to alleviate some of your concerns. I can offer some answers to your questions as follows:

1)Will a wall be put into place and when???

While the LRT operations will be much quieter than vehicle traffic and will not increase the existing noise levels due to vehicular traffic, the City will be implementing noise barriers where warranted along the Highway 174 corridor. For your property at [REDACTED], a 3 metre high noise barrier placed on top of the existing berm is being recommended and will be constructed sometime during the LRT construction, anticipated to be operational by 2023.

2)The widening of the highway will it be on both sides or just one?

Widening of Hwy174 will be equally on both sides of the existing road and will be contained within the City's right of way corridor and no additional property is required.

3)Is the rails being built in the centre of highway or on both sides?

The LRT will be located in the median in your area and is in the median between Montreal Road and Trim Road. From Blair Station, the LRT is located on the north side of Hwy 174 to Montreal Road. Since the LRT is in the median, vibration will not be an issue.

Best regards,
Angela Taylor